

**WORTHINGTON CITY COUNCIL
REGULAR MEETING**

AGENDA

**5:30 P.M. - Tuesday, May 27, 2025
City Hall Council Chambers**

A. CALL TO ORDER AND PLEDGE OF ALLEGIANCE

B. INTRODUCTIONS AND OPENING REMARKS

C. AGENDA ADDITIONS/CHANGES AND CLOSURE

1. Additions/Changes
2. Closure

D. CONSENT AGENDA

1. CITY COUNCIL MINUTES (WHITE)
 - a. Regular City Council Meeting Minutes of May 12, 2025
2. MINUTES OF BOARDS AND COMMISSIONS (PINK)
 - a. Water Light Commission Meeting Minutes of May 19, 2025
 - b. Law Enforcement Center Joint Powers Board Meeting Minutes of May 14, 2025
 - c. Water & Light Commission Meeting Minutes of May 5, 2025
 - d. Board of Review Meeting Minutes of May 1, 2025
 - e. Heron Lake Watershed Board Meeting Minutes of April 16, 2025

3. CITY COUNCIL BUSINESS - ADMINISTRATION (WHITE)

Case Item(s)

1. Application for Parade Permit or to Block Streets - Independent School District 518
2. Application for Parade Permit or to Block Streets - Solid Rock Church
4. FINANCIAL STATEMENTS (LAVENDER)
 - a. Municipal Liquor Store Income Statement for the Period of January

- 1, 2025 through April 30, 2025
- b. Olson Park Statement of Revenue and Expenditures - Budget and Actual for the Period of January 1, 2025 through April 30, 2025
- c. Field House Statement of Revenue and Expenditures - Budget and Actual for the Period of January 1, 2025 through April 30, 2025

5. **BILLS PAYABLE (WHITE)**

PLEASE NOTE: All utility expenditures are listed as 601, 602, and 604, and are approved by the Water and Light Commission

E. CITY COUNCIL BUSINESS - ADMINISTRATION (WHITE)

Case Item(s)

- 1. PFA Loan Extension
- 2. Nominating Committee Appointments / Reappointments
- 3. Commitment of Spending Plan Funds for Wetland Mitigation at the South Shore Apartments Development

F. CITY COUNCIL BUSINESS - ENGINEERING (BLUE)

Case Item(s)

- 1. Resolution Pertaining to Intent to Bond for Cost Incurred in Making Public Improvements
- 2. Approval of AT Grant Agreement and Supporting Documents for East Okabena Lake Trail Project
- 3. Approve Plans for East Flower Lane Street and Utility Improvements and Authorize Advertisement for Bids
- 4. Amended License Agreement with Minnesota Energy Resources Corporation (MERC) for East Okabena Trail
- 5. Consideration of Entering into Negotiation to Acquire a Section of Parcel 31-3497-250

G. COUNCIL COMMITTEE REPORTS

1. Mayor Von Holdt
2. Council Member Janssen
3. Council Member Ernst
4. Council Member Weber
5. Council Member Kuhle
6. Council Member Kielblock

H. CITY ADMINISTRATOR REPORT

I. ADJOURNMENT

**WORTHINGTON CITY COUNCIL
REGULAR MEETING
MAY 12, 2025**

The meeting was called to order at 5:30 p.m., in City Hall Council Chambers by Mayor Rick Von Holdt with the following Council Members present: Chris Kielblock, Dennis Weber, Mike Kuhle, Amy Ernst. Larry Janssen (excused).

Staff present: Hyunmyeong Goo, City Engineer; Matt Selof, Community Development Director; Todd Wietzema, Public Works Director; Jorge Lopez, Rental Inspector; Cristina Adame, Community Relations & Communications; Mindy Eggers, City Clerk.

Others Present: Sam Martin, *The Globe*; Ben Sieve, Fertilizer Dealer Supply; Clair Williams, Priscilla Williams, Loreena Luetgers, Jaime Salinas, FORWARD Worthington; Dan Kruger, IDEAL Construction; Al Drost, IDEAL Construction; Justin Olson, Bolton & Menk; Efrain Patino, Rich Besel, Gary Brandt, Paulette Sjogren.

The Pledge of Allegiance was recited.

AGENDA APPROVED WITH ADDITIONS/CHANGES

The Mayor stated *Item H.8. Approval of AT Grant Agreement and Supporting Documents for East Okabena Lake Trail Project* from the agenda.

A motion was made by Council Member Kielblock, seconded by Council Member Kuhle and unanimously carried to approve the agenda with the noted change.

**PUBLIC HEARING AND RESOLUTION NO. 2025-05-28 ORDERING IMPROVEMENT
AND PREPARATION OF PLANS AND SPECIFICATIONS - EAST FLOWER LANE
EXTENSION**

Pursuant to published notice, this was the time and date set for a public hearing on the East Flower Lane Extension.

The motion was made by Council Member Kielblock, seconded by Council Member Ernst and unanimously carried to open the hearing.

Hyunmyeong Goo, City Engineer, gave a presentation on the project objectives of the street improvement project. The proposed improvement would provide infrastructure to support the South Shore Apartments development, realign East Flower Lane to serve residential and future commercial/industrial uses, provide base reconstruction, sidewalks, driveways, and a permanent concrete pavement surface. Assessment costs include curb & gutter, driveways, sidewalk, outside of center 24-foot pavement. The rate is based on average fixed cost per foot of property frontage.

The estimated cost summary is as follows:

- Project Cost \$350,000.00
- Total Assessable Costs \$250,000.00
- Assessment Rate \$375.00
- City Cost \$100,000.00

Mayor Von Holdt asked if there was any testimony. None was received.

The motion was made by Council Member Kuhle, seconded by Council Member Weber and unanimously carried to close the hearing.

The motion was made by Council Member Ernst, seconded by Council Member Weber and unanimously carried to adopt the following resolution:

RESOLUTION NO. 2025-05-28

A RESOLUTION ORDERING IMPROVEMENT AND PREPARATION OF PLANS AND SPECIFICATIONS

(Refer to Resolution File for complete copy of Resolution)

CONSENT AGENDA

A motion was made by Council Member Ernst, seconded by Council Member Kielblock and unanimously carried to approve the consent agenda as presented.

- Regular City Council Meeting Minutes of April 28, 2025
- Special City Council Meeting Minutes of April 23, 2025
- Application for Temporary On-Sale Liquor License - Worthington Okabena Windsurfers
- Application for Parade Permit or to Block Streets - Worthington Okabena Windsurfers
- Girl's Softball Association Agreement
- Worthington Soccer League Agreement
- 2025 Private Dock Applications
 - Terry Bourassa - 518 W. Lake Avenue
 - Eric Winch - 117 Lake Avenue
- Bills Payable Totaling \$1,066,798.19

RESOLUTION NO. 2025 05-29 ADOPTED ACCEPTING A DONATION OF PERSONAL PROPERTY - THE EARLY RISERS KIWANIS

Todd Wietzema, Public Works Director, said staff was approached by members of the Worthington Early Risers Kiwanis group, wondering if the City would have any interest in placing a permanent cornhole game in Chautauqua Park.

The cornhole boards are made of concrete and would replace two of the existing horseshoe pits.

The idea was presented to the Park and Recreation Advisory Committee and was unanimously approved.

The Early Riser Kiwanis Club has raised enough donations to purchase 2 cornhole games and they have been delivered. Staff will install the boards as soon as time permits.

Clair Williams, Early Riser Kiwanis Club, said the boards were purchased by the membership as a thank you to the City of Worthington for the help in funding the improvements that have been made in Chautauqua Park.

The motion was made by Council Member Kuhle, seconded by Council Member Weber and unanimously carried to approve the feasibility report and adopt the following resolution:

RESOLUTION NO. 2025-05-29

A RESOLUTION ACCEPTING A DONATION OF PERSONAL PROPERTY

(Refer to Resolution File for complete copy of Resolution)

ORDINANCE NO. 1214 ADOPTED AMENDING TITLE XV OF THE CITY OF WORTHINGTON, NOBLES COUNTY, MINNESOTA, TO REZONE PROPERTY FROM "TZ" (TRANSITION ZONE DISTRICT) AND "B-3" (GENERAL BUSINESS DISTRICT) TO "M-1" (LIGHT MANUFACTURING DISTRICT)

Matt Selof, Community Development Director, said Fertilizer Dealer Supply, Inc has requested a change of zone from 'TZ' Transition Zone and 'B-3' General Business District to 'M-1' Light Manufacturing District for property located South of Gateway Drive and immediately adjacent to State Highway 59/60. The area encompasses parcels 31-0981-000 and a portion of 31-0980-000.

A public hearing was held at the Planning Commission meeting on May 6, 2025 with the Commission voting unanimously to recommend approval of the requested change of zone. The decision was based on the following:

1. The applicant is purchasing parcel 31-0981-000 and a portion of parcel 31-0980-000 (the empty farm ground portion of the parcel) with plans to construct a new business on the

property. The subject property is currently zoned 'TZ' Transition Zone and 'B-3' General Business. The applicant is seeking to change this to 'M-1' Light Manufacturing District.

2. The City's 2045 Comprehensive Plan designates the subject property as future light industrial which aligns with the requested change. The Plan specifies light industrial as "areas that support a range of industrial commerce activities with minimal disturbance to the immediate surrounding community...". The Plan also identifies support for new businesses, growth of the community, and siting development in areas easy to serve with utilities.

3. With any land use requests, the surrounding zoning and land uses should be considered. The surrounding properties zoning and uses are as follows:

North: B-3 and M-1 are located to the north in the immediate vicinity. An auto body shop, storage units, and some residential are located on the adjacent properties.

East: Highway 59/60 separates the subject property for land outside City limits. South: Farmland and a single-family home located outside City limits. West: To the west is a metal recycling operation zoned as a PUD.

4. The subject property has utility service around the north/west sides of the property and will have access from Gateway Drive after completion of the purchase of a portion of parcel 31-0980-000 along with access from South Lake Street on the south side (currently gravel).

Mr. Selof said this would be a parts dealer and would not have fertilizer or chemicals on site.

He also noted the ordinance will only require one reading.

The motion was made by Council Member Weber, seconded by Council Member Kuhle and unanimously carried to adopt the proposed ordinance:

ORDINANCE NO. 1214

AN ORDINANCE AMENDING TITLE XV OF THE CITY OF WORTHINGTON, NOBLES COUNTY, MINNESOTA, TO REZONE PROPERTY FROM "TZ" (TRANSITION ZONE DISTRICT) AND "B-3" (GENERAL BUSINESS DISTRICT) TO "M-1" (LIGHT MANUFACTURING DISTRICT)

(Refer to Ordinance File for complete copy of Ordinance)

APPROVED PURCHASE AGREEMENT - PARCELS#31-3786-583, 31-3786-585, 31-3786-587, AND 31-3786-589

Mr. Selof said Council accepted a letter of intent from Efrain Patino of Mexicano Properties, Inc. to purchase approximately 8.7 acres of land in the City's Bioscience Industrial Park for a new warehouse center with office and storage space in September 2024. Mr. Patino has now submitted a purchase agreement for Council's consideration. The proposed offer includes four parcels 31-3786-583, 31-3786-585, 31-3786-587, and 31-3786-589 along Research Lane at the asking price of \$247,034.000.

The acquisition will also be subject a conditional use permit and other necessary steps to fulfill the City's obligations in selling property.

The motion was made by Council Member Kuhle, seconded by Council Member Weber and unanimously carried to accept the full price offer of \$274,034.00 for the following parcels 31-3786-583, 31-3786-585, 31-378-587, and 31-3786-589.

RESOLUTION NO. 2025-05-30 ADOPTED APPROVING PLANS AND SPECIFICATIONS AND ORDERING ADVERTISEMENT FOR BIDS - RAY DRIVE

Hyunmyeong Goo, City Engineer, said staff have prepared the plans and specifications for the North Ray Drive Extension Project, which is part of the 2025 Capital Improvement Plan. The project is scheduled for construction during the 2025 season. Staff is requesting the approval of the plans and specifications and authorize the advertisement for bids. Bids would be scheduled to be received until 2:00 p.m. on June 4, 2025, and would be brought forward for consideration of award at the June 9, 2025, City Council meeting.

A motion was made by Council Member Ernst, seconded by Council Member Weber and unanimously carried to approve the following resolution:

RESOLUTION NO. 2025-05-30

A RESOLUTION APPROVING PLANS AND SPECIFICATIONS AND ORDERING ADVERTISEMENT FOR BIDS - RAY DRIVE

(Refer to Resolution File for complete copy of Resolution)

AWARDED THE CONTRACT FOR SERVICE DRIVE & SERVICE ROAD IMPROVEMENT

Mr. Goo said the bid opening for the 2025 Service Drive & Service Road Improvement project was held on Wednesday, May 7, 2025, at 2:00 p.m. Once Council awards the contract to the lowest responsible bidder the results will be made public.

The lowest bid for the bituminous option was submitted by Duininck Inc., in the amount of \$1,662,700.80. The lowest bid for the concrete option was submitted by Ideal Construction Group Inc., in the amount of \$1,936,468.75.

Staff recommends that the council award the contract to the lowest responsible bidder for the bituminous option.

Mr. Goo explained the difference in price between the two options is \$273,767.95, which would be an additional assessable amount to the seven businesses that are in the project area.

Council discussed how much traffic the street will have. Mr. Goo said that the street is a right in and right out so does not expect it to be very much. He also explained that the bituminous would be 5 inches which is stronger than the 3 inch that has been used in the past on projects. The concrete option would be 7 inches.

Council Member Kuhle said he did not think adding the additional amount to the assessments was the way to go. Council Member Weber said it would save the City money in the end, Council Member Ernst agreed. The Mayor added there will be more traffic than they think.

The motion was made by Council Member Weber, seconded by Council Member Ernst to award the bid to IDEAL Construction for the concrete option in the amount of \$1,936,468.75. Those voting in favor of the motion were: Kielblock, Ernst, and Weber. Those voting against the same motion: Kuhle. Motion passed.

RESOLUTION NOS. 2025-05-31 AND 2025-05-32 ADOPTED RELATING TO FINANCING OF CERTAIN PUBLIC IMPROVEMENTS BY THE CITY OF WORTHINGTON; ESTABLISHING COMPLIANCE WITH REIMBURSEMENTS BOND REGULATIONS UNDER THE INTERNAL REVENUE CODE

Mr. Goo said to comply with federal tax regulations and Minnesota Statutes Section 429, the City must adopt a resolution stating its intent to reimburse itself with bond proceeds for project costs incurred before bonds are issued.

Common practice is for the City to cover early project expenses using internal funds and issue bonds later when project costs justify it.

Staff recommends Council approve the resolutions that would allow for reimbursements for the Service Drive & Service Road Improvement Project and for the North Ray Drive Extension Project. To allow for cost adjustments the amounts in each of the resolutions are approximately 10% higher than the current project estimates.

The motion was made by Council Member Kuhle, seconded by Council Member Weber and unanimously carried to adopt the following resolution:

RESOLUTION NO. 2025-05-31

A RESOLUTION RELATING TO FINANCING OF CERTAIN PUBLIC IMPROVEMENTS BY THE CITY OF WORTHINGTON; ESTABLISHING COMPLIANCE WITH REIMBURSEMENTS BOND REGULATIONS UNDER THE INTERNAL REVENUE CODE - SERVICE DRIVE & SERVICE ROAD

(Refer to Resolution File for complete copy of Resolution)

A motion was made by Council Member Kuhle, seconded by Council Member Weber and unanimously carried to adopt the following resolution:

RESOLUTION NO. 2025-05-32

A RESOLUTION RELATING TO FINANCING OF CERTAIN PUBLIC IMPROVEMENTS BY THE CITY OF WORTHINGTON; ESTABLISHING COMPLIANCE WITH REIMBURSEMENTS BOND REGULATIONS UNDER THE INTERNAL REVENUE CODE - NORTH RAY DRIVE

(Refer to Resolution File for complete copy of Resolution)

APPROVED PUBLIC MEETING NOTICE FOR REVIEW OF STORM WATER POLLUTION PREVENTION PLAN

Mr. Goo explained the City of Worthington is regulated under Phase II of the federal stormwater program for Municipal Separate Storm Sewer Systems (MS4s). As part of this program, the City is required to maintain coverage under a statewide NPDES permit and implement a Storm Water Pollution Prevention Plan (SWPPP).

The SWPPP must address six key areas:

- Public education and outreach
- Public involvement and participation
- Illicit discharge detection and elimination
- Construction site stormwater runoff control
- Post-construction stormwater management
- Pollution prevention and good housekeeping for municipal operations

One of the permit requirements is an annual public meeting must be held to provide residents an opportunity to comment on the SWPPP. Staff recommends scheduling the public meeting for Monday, June 23, 2025, 5:30 p.m., during the regular City Council meeting.

The meeting will include a presentation of the draft annual report, a summary of activities completed over the past year, and an outline of goals moving forward. Residents may submit written or verbal

comments on the SWPPP through June 13, 2025. All comments will be reviewed, and any necessary adjustments will be considered for inclusion in the final report submitted to the Minnesota Pollution Control Agency (MPCA).

A motion was made by Council Member Kielblock, seconded by Council Member Ernst and unanimously carried to schedule the annual public meeting for Monday, June 23, 2025, 5:30 p.m., during the regular City Council meeting.

**APPROVED LRIP GRANT AGREEMENT AND RESOLUTION NO. 2025-05-33 ADOPTED
APPROVING BOND FINANCED PROPERTY CERTIFICATION AND RESOLUTION NO.
2025-05-34 ADOPTED APPROVING LOCAL ROAD IMPROVEMENT PROGRAM
AGREEMENT GRANT TERMS AND CONDITIONS SAP - 177-594-001**

Mr. Goo said the City of Worthington has been awarded approximately \$1.44 million in Local Road Improvement Program (LRIP) funding from the State of Minnesota for the Service Drive and Service Road Improvement Project.

The following Grant Agreement along with the following supporting documents were included for review:

- Completed Sources and Uses of Funds Schedule
- Project Schedule, Workforce Certificate, and Equal Pay Certificate
- Resolution Bond-Financed Property Certification
- Grant Application
- Grantee Resolution Approving Grant Agreement
- General Terms and Conditions

A motion was made by Council Member Kuhle, seconded by Council Member Weber and unanimously carried to approve the LRIP Grant Agreement.

A motion was made by Council Member Kielblock, seconded by Council Member Ernst and unanimously carried to adopt the following resolution:

RESOLUTION NO. 2025-05-33

A RESOLUTION APPROVING BOND FINANCED PROPERTY CERTIFICATION STATE OF MINNESOTA GENERAL OBLIGATION BOND FINANCED PROPERTY

(Refer to Resolution File for complete copy of Resolution)

A motion was made by Council Member Kielblock, seconded by Council Member Weber and unanimously carried to adopt the following resolution:

RESOLUTION NO. 2025-05-34

A RESOLUTION APPROVING LOCAL ROAD IMPROVEMENT PROGRAM GRANT
AGREEMENT GRANT TERMS AND CONDITIONS SAP 177-594-001

(Refer to Resolution File for complete copy of Resolution)

RESOLUTION NO. 2025-05-35 ADOPTED APPROVING VACATING AN EASEMENT

Mr. Goo said the City of Worthington holds a public utility easement in the southeast corner of parcel 31-0687-100 (northeast of Casey's at 1097 W Gateway Drive). The purpose of the easement was to cover a sanitary sewer stub that the gas stations service line then branches off of.

With the recent approval of the plat of South Shore Apartments, this easement area is now in the right-of-way of East Flower Lane and the easement is no longer needed. To clear up the title to the property, it is requested this easement be vacated.

The Water and Light Commission recommended approval of the request at the May 5, 2025 meeting.

The motion was made by Council Member Ernst, seconded by Council Member Weber and unanimously carried to adopt the following resolution:

RESOLUTION NO. 2025-05-35

A RESOLUTION VACATING AN EASEMENT

(Refer to Resolution File for complete copy of Resolution)

COUNCIL COMMITTEE REPORTS

Mayor Rick Von Holdt - No report.

Council Member Ernst - Said there is great progress being made on the Centennial Park Trail.

Council Member Weber - No report.

Council Member Kuhle - Said everyone should have received a CGMC Action Alert regarding the proposed LGA cuts that the Senate has put in their bill. If passed it could take approximately \$128,000 in LGA away from the City. Everyone should reach out to our representatives and suggested the Mayor reach out to area Mayors as well.

Council Member Kielblock - Attended the Planning Commission meeting.

ADJOURNMENT

Worthington City Council
Regular Meeting, May 12, 2025
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The motion was made by Council Member Kielblock, seconded by Council Member Weber and unanimously carried to adjourn the meeting at 6:07 p.m.

Mindy Eggers, MCMC
City Clerk

UNAPPROVED

WATER AND LIGHT COMMISSION MINUTES REGULAR MEETING MAY 19, 2025

President Debra Weg called the regular meeting of the Water and Light Commission to order in the Worthington Public Utilities Conference Room at 3:00 P.M., with the following members present: Kathy Hayenga, Chad Nixon, Mike Fury, and Amy Ernst.

Staff members present were Scott Hain, General Manager; Eric Roos, Utility Coordinator; and Kristy Taylor, Secretary to the Commission.

Others present: Samuel Martin, The Globe

AGENDA ADDITIONS/CLOSURE

A motion was made by Commissioner Nixon, seconded by Commissioner Fury, and unanimously carried to close the agenda as presented.

CONSENT AGENDA APPROVED

A motion was made by Commissioner Ernst, seconded by Commissioner Hayenga, and unanimously carried to approve the consent agenda as follows:

- Water and Light Commission minutes of the regular meeting held on May 5, 2025
- Staff reports for April
- Utility bills payable totaling \$192,695.64 for May 9 and May 16, 2025

FINANCIAL STATEMENTS AND SALES REPORTS

A motion was made by Commissioner Fury, seconded by Commissioner Hayenga, and unanimously carried to accept the financial statements and sales reports for April.

LONG-RANGE PLANNING DISCUSSION

General Manager Hain reviewed with staff and the Commission long-term plans for capital improvements in the Electric, Water, and Wastewater departments. There was also discussion on potential financial participation in the construction of a new city-owned ice arena.

COMMISSION COMMITTEE REPORTS

There were no Commission committee reports.

GENERAL MANAGER REPORT

General Manager Hain had nothing additional to report.

Water and Light Commission Minutes

May 19, 2025

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ADJOURNMENT

President Weg declared the meeting adjourned at 4:10 P.M.

Kristy Taylor

Secretary to the Commission

**MINUTES OF THE PRAIRIE JUSTICE CENTER JOINT OPERATIONS COMMITTEE
MAY 14, 2025 – PRAIRIE JUSTICE CENTER**

PRESENT: City of Worthington – Troy Appel, Dennis Weber, Mike Kuhle

Nobles County – Ryan Kruger, Bruce Heitkamp, Bob Paplow, Chris Dybevic

Chairman, Chris Dybevic called the meeting to order at 1:02 p.m.

Dennis Weber made a motion to approve the agenda. Bob Paplow seconded the motion. The motion carried.

After reviewing the minutes of the February 12, 2025 meeting, Mike Kuhle made a motion to approve the minutes. Dennis seconded the motion. Motion carried.

New Business:

- **ATM's**
 - Ryan Kruger shared that at the County Management Meeting they talked about ATM's in county offices and that we'd like one at the PJC. Tom Cotter, owner of TC's Tavern in Brewster, has a side ATM business and asked to be considered. Nobles County would have to supply power and internet and he'd take care of the rest. A small percentage of each transaction would go back to Nobles County. Ryan stated there is definitely a need in this building with gun permits, finger printing, electric home monitors, etc.
 - Chris asked if the county would need a service agreement or other paperwork.
 - Bruce Heitkamp said there needs to be an indemnity no fault to county.
 - Chris suggested that Ryan ask Tom if he has examples of these agreements.
 - Ryan will get more information from Tom.

Old Business:

- **Vehicle Storage Update**
 - Ryan said that sealing of the concrete began yesterday and that it needs to seal for a couple of days before using.
 - Bruce stated that only one coat was needed to seal the floor. There is crushed concrete ready to be laid at the building entrance and other rock to landscape the sides of the building for splash guard.
 - Troy expressed concern of only two outlets in the building.
- **Walking/Parking Surfaces**
 - Bruce shared that Jeff Kuhn, Widseth Smith Nolting, is working on a design that is hopefully available for the next board regular meeting. He stated with all the ADA restrictions and surface space reduction that they needed to contact an engineer.
 - Bruce will notify board as soon as he receives anything back.
- **Behavioral Health Stabilization Center Update**
 - Bruce stated that construction is going well and should be finished mid-July. An agreement did come from New Life Treatment Center on the lease. ElevaCare Avera wanted an

attorney to look over the agreement. Once the signature is received, then hiring can begin for the center.

- Bruce explained that the Sheriff and Chief expressed interest in a meeting with the new staff to offer a welcome, share information, and discuss ground rules to create a healthy working relationship.
- Chris asked if there was an alarm on the exit door since it was a non-secure facility or if residents could come and go. He stated that a medical doctor or mental health professional would be needed to admit them to the facility. Ryan said that 90% of the time when someone is brought for a 72-hour hold, the doctor releases them and sometimes they end up in jail.
- Ryan questioned issues like security with work release and staff parking close by, contracted meals, laundry, or protocol for accepting people. These issues should have been discussed earlier.
- Troy voiced concerns of other counties bringing people here. Where do they go when they get out. Who picks them up if they refuse treatment? Troy stated that this is a very needed facility and that more are needed. We just need to figure out the details.
- Bob explained that only people who want treatment will be accepted. He said that ElevaCare has hired someone who has started other facilities and has expertise in coexisting with law enforcement. When she arrives, a meeting will be scheduled with Ryan and Troy.

- **Other Items**

- Chris asked about the Probation grant and if the money was all spent. Bruce said that they are planning to put up a garage on the north side of the jail in the green space. May add extra space for maintenance supplies.
- Bob would still like to see a fence between the Behavior Health facility and LEC, before the facility opens to the public. Chris asked if the architect could look into the fence.
- Mike requested a list of upcoming building projects. Bruce will have the 2026 Capital Improvement items for the next meeting.
- Mike asked about jail population. Ryan stated average daily population is between 55-62. We get Rock and Pipestone County inmates and sometimes Jackson or Cottonwood County.

2025 Meeting Dates:

August 13, 2025 – 1:00 p.m.

November 12, 2025 – 1:00 p.m.

Dennis made a motion to adjourn the meeting. Bob seconded the motion. The motion passed.

Action Items

- Ryan to get more information from Tom Cotter on ATM's and possible agreements.
- Bruce to notify board as soon as he receives information from Wideth Smith Nolting.
- Bruce or Bob to schedule a meeting between Ryan, Troy, and new ElevaCare representative.
- Bruce to ask architect about fence possibilities between Behavior Health Center and LEC.
- Bruce to bring 2026 Capital Improvements to next meeting.

LEC Joint Powers Committee

Journal of Votes

February 12, 2025

QUARTERLY BOARD MEETING

PJC Training Room

1530 Airport Rd

Worthington MN 56187

DATE	ITEM VOTED ON	VOTES
5-14-25	Approval of Agenda with addition 1 st – Dennis Weber 2 nd – Bob Paplow	Unanimous
	Approval of Minutes 1 st – Mike Kuhle 2 nd – Dennis Weber	Unanimous
	Adjourn Meeting 1 st – Dennis Weber 2 nd – Bob Paplow	Unanimous

UNAPPROVED

WATER AND LIGHT COMMISSION MINUTES REGULAR MEETING MAY 5, 2025

President Debra Weg called the regular meeting of the Water and Light Commission to order in the Worthington Public Utilities Conference Room at 3:00 P.M., with the following members present: Chad Nixon, Mike Fury, and Amy Ernst. Kathy Hayenga was absent (excused).

Staff members present were Eric Roos, Utility Coordinator, and Kristy Taylor, Secretary to the Commission.

Others present: None

AGENDA ADDITIONS/CLOSURE

A motion was made by Commissioner Fury, seconded by Commissioner Nixon, and unanimously carried to close the agenda as presented.

CONSENT AGENDA APPROVED

A motion was made by Commissioner Ernst, seconded by Commissioner Fury, and unanimously carried to approve the consent agenda as follows:

- Water and Light Commission minutes of the regular meeting held on April 21, 2025
- Utility bills payable totaling \$120,988.45 for April 25 and May 2, 2025

WATER AND LIGHT COMMISSION ANNUAL MEETING

During the Water and Light Commission annual meeting, a motion was made by Commissioner Ernst, seconded by Commissioner Fury, and unanimously carried to re-elect Commissioner Weg as President, Commissioner Nixon as Vice-President, and designate Kristy Taylor as Secretary to the Commission for the coming year.

WATER AND LIGHT COMMISSION AND STAFF APPOINTMENTS TO BOARDS AND COMMITTEES

Appointments to the following boards and committees that Commission members and staff are currently serving on were reviewed. A motion was made by Commissioner Nixon, seconded by Commissioner Fury, and unanimously carried to approve the appointments:

<u>Committee</u>	<u>Commissioner</u>	<u>Appointed</u>	<u>Term Limit</u>
APPA Association Business Meeting	Hain	02/17/15	No Limit
APPA Legislative & Resolutions Committee	Hain	02/17/15	No Limit
Compensation Committee	Weg	09/18/23	No Limit

Water and Light Commission Minutes

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Compensation Committee	Hayenga	04/15/19	No Limit
E. O. Olson Board of Trustees	Weg	04/20/20	No Limit
Economic Development Authority	Nixon	09/18/23	No Limit
Employee Advisory Committee	Hayenga	04/20/20	No Limit
Housing Committee	Fury	05/06/23	No Limit
Housing Committee	Nixon	04/15/19	No Limit
Lewis & Clark Joint Powers Board (Delegate)	Weg	05/03/21	12/31/24
Lewis & Clark Joint Powers Board (Alternate)	Hayenga	05/03/21	12/31/24
MMUA (Delegate)	Hain	07/06/21	No Limit
MMUA (Alternate)	Roos	05/06/24	No Limit
WREDC Ex-Officio	Hain	05/22/22	No Limit

EASEMENT VACATION

After review of the easement, a motion was made by Commissioner Ernst, seconded by Commissioner Nixon, and unanimously carried to recommend to City Council to vacate the easement that covers the sanitary sewer that serves Casey's (formerly Sinclair), located north of Highway 59/60 and west of Plotts Avenue.

COMMISSION COMMITTEE REPORTS

Commissioner Weg attended the E.O. Olson Board of Trustees meeting on April 29, 2025.

There were no other Commission committee reports.

ADJOURNMENT

A motion was made by Commissioner Fury, seconded by Commissioner Nixon, and unanimously carried to adjourn the meeting at 3:05 P.M. President Weg declared the meeting adjourned.

Kristy Taylor
Secretary to the Commission

CITY OF WORTHINGTON
LOCAL BOARD OF APPEAL / EQUALIZATION
May 1, 2025

The meeting was called to order at 9:00 a.m. in the Nobles County Commissioners Board Room by Rick Von Holdt with the following Members present: Amy Ernst, Chris Kielblock, Dennis Weber, Mike Kuhle. Larry Janssen (absent).

Staff present: Mindy Eggers, City Clerk.

Others present: Valerie Ruesch, Nobles County Assessor; John Meyer, Ben Puthoff, Linda Pospisil, Dave Maras, and Erik Skogquist.

Val Ruesch, Nobles County Assessor, said the City of Worthington has issued two trends from the Department of Revenue for the 2025 Assessment. The Residential/Seasonal Recreational Residential is at a 6.4912% Annual Trend and the Agriculture/Rural Vacant Bare Land >34.5 Acres: is at a -9.3071% Annual Trend

The 2025 Assessment year began with the reviewing of properties that were viewed for the 2020 assessment quintile. Adjustments and changes were made accordingly based on current information and from any changes that took place in the past 5 years. The requirement for parcels to be viewed is 20% each year.

The Assessor's Office also viewed open market, arm's-length sales for the sale period from October 1, 2022 to September 30, 2023 for accuracy and building permits were also reviewed.

There were 108 open market, arm's length sales (including 2 sales on water) for the sales period of October 1, 2023 to September 30, 2024 that were used to make adjustments to areas of the city.

Components affecting value increases city wide:

- Time trends based on a 21 month sales study.
- Map/Neighborhood area adjustments.
- New construction and/or omitted structures.
- Viewed open market, arm's length sales for the period from October 1, 2023 to September 30, 2024 for accuracy.
- Building Permits were viewed.

Apartments

There were no open market, arms length sales for the period October 1, 2023 to September 30, 2024. The apartment average was minimal with a small increases of 2% on average. Small sample study five-year ratio is 86.8% with six sales. The county continues to monitor internally.

Industrial

City of Worthington Local Board of Appeal / Equalization
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Page 2

Over the past few years, this segment has increased faster than commercial properties in general but had limited sales activity locally. There are no qualified industrial sales during the assessment period and this category saw an average increase of 5% coming off a 14% increase last year. High demand for these types of properties coupled with large increases in construction costs of the past few years continues to increase values on this segment of property.

For assessment year 2024, all industrial class properties were viewed.

Commercial

There were 6 open market, arms length sales that were used to make adjustments. This is enough to create a stand-alone sales study according to the MN Department of Revenue. However, this class of property encompasses a wide range of building types and ages from downtown retail to restaurants, offices, and big box stores. Historically, assessed values for commercial properties have been below sale prices in Worthington due to a strong market, however, the median assessed values in most areas of the city were only slightly below the acceptable ratio per the State of Minnesota. On average, commercial properties saw 8-16% increases with larger big box retail only receiving slight increases due to the demand for those decreasing in a changing retail marketplace.

For assessment year 2025, the Beltline area was viewed and was the only area that saw values remain relatively flat year over year.

Exempt

Every 6 years the State of Minnesota requires a review of all tax exempt properties in order to set proper valuations. These mainly include schools, churches, parks, and other government property. These valuations are used for many purposes, however, for cities the distribution of fire aid is the most tangible benefit derived from these valuations. Traditionally, we try to view and set these values annually when viewing surrounding properties (e.g. view city hall when viewing downtown commercial).

Joseph & Patricia Kinley - 31-2325-000 - Staff did an inspection and lowered the valuation from \$602,600.00 to \$597,300.00 and recommends no further change. A motion was made by Council Member Kielblock, seconded by Council Member Weber and unanimously carried to approve no further change to the value or classification.

Nicholas & Kara Kramer - 31-2059-000 - Staff recommends reducing the valuation from \$370,900.00 to \$355,400.00. A motion was made by Council Member Ernst, seconded by Council Member Weber and unanimously carried to approve the reduction to the value.

ADJOURNMENT

City of Worthington Local Board of Appeal / Equalization
May 1, 2025
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A motion was made by Council Member Kielblock, seconded by Council Member Weber and unanimously carried to adjourn the meeting at 9:42 a.m.

Mindy L. Eggers, MCMC
City Clerk

HLWD Regular meeting 4-16-2025

Members present; Wanye Rasche, Cory Rieth, Phil Kruger, Travis Sievers. Absent Randy Lubben

Office Manager Jen Willaert, others virtual

Meeting was called to order by Chairman Wayne Rasche at 8:00 am.

Motion was made by Cory to approve agenda. Second by Travis. Motion carried 4-0

Motion was made by Wayne to approve minutes. Second by Phil. Motion carried 4-0

Drainage systems

Discussion was had on clarification on charges from ISG. Motion was made by Phil to pay bills. Second by Travis. Motion carried 4-0

JD3

Request was made to increase bond to 2.5 million. Motion was made by Travis to increase bond to 2.5 million. Second by Cory. Motion carried 4-0

District business

Thompson Wetland There was no report from ISG

CD Due

Phil made Motion to appoint Wayne to take care of renewing the CD. Second by Cory. Motion carried 4-0

Pre-fund admin. account for stream sampling

Paperwork for having DNR reimburse cost of sampling equip and etc. needed to be filled out. Estimate cost of \$1600. Travis made a motion to have \$1600 transferred to admin acct. second by Cory. After discussion motion was made by Travis to amend motion to change transfer amount to \$2000. Second by Cory. Amendment carried 4-0. Motion with amendment carried 4-0.

Treasurer's report

Motion was made by Cory to transfer \$5500 from MM acct. and to pay bills. Second by Travis. Motion carried 4-0

Discussion was had about invite to ISG tour.

Cory made a motion to adjourn meeting. Second by Travis Motion carried 4-0

Submitted by

Phil Kruger, Secretary

CITY OF WORTHINGTON, MINNESOTA

MUNICIPAL LIQUOR STORE
INCOME STATEMENT
For the Period 1/1/25 Through 4/30/25
(Amounts in Dollars)

	Total 2025 Budget	APRIL		%	YTD	
		Actual	Previous Year	YTD Actual to Budget	Actual	Previous Year
Sales						
Liquor	2,200,000	145,079	154,249	28.1%	567,936	617,263
Wine	605,000	43,762	42,028	27.3%	155,702	165,392
Beer	2,550,000	184,045	185,882	26.6%	664,634	677,290
Mix/nonalcohol	82,000	5,849	5,575	24.0%	20,848	19,693
TCH	45,000	5,673	1,984	15.6%	19,591	7,037
NSF charges	100	-	-	0.0%	-	-
Net Sales	5,482,100	384,408	389,718	26.1%	1,428,711	1,486,675
Cost of Goods Sold						
Liquor	1,577,030	78,766	101,780	26.9%	354,540	424,659
Beer	1,950,000	134,283	147,003	25.4%	469,601	494,834
Wine	440,894	25,481	31,267	29.8%	93,908	131,362
Soft drinks/mix	56,000	3,847	3,433	21.5%	11,661	12,031
TCH	22,000	3,647	1,313	17.7%	10,296	3,889
Freight	37,000	2,481	4,197	31.0%	9,304	11,455
Total Cost of Goods Sold	4,082,924	248,505	288,993	23.3%	949,310	1,078,230
Gross Profit	1,399,176	135,903	100,725	34.3%	479,401	408,445
Operating Expenses						
Personnel services	489,850	35,277	35,716	29.0%	133,180	142,163
Supplies	34,800	2,880	2,585	28.7%	8,191	9,972
Other services & charges	248,527	21,979	14,411	30.9%	69,309	76,822
Interest	6,642	-	-	0.0%	-	-
Depreciation (estimated)	115,000	9,583	9,151	23.9%	38,333	27,452
Total Operating Expenses	894,819	69,719	61,863	27.8%	249,013	256,409
Operating Income (Loss)	504,357	66,184	38,862	45.7%	230,388	152,036
Non-Operating Revenues (Expenses)						
Interest earnings **	20,000	1,333	1,333	26.7%	5,333	5,333
Other non-operating	-	-	-	-	-	-
Sale of fixed asset	-	-	-	-	-	-
Loss on fixed asset	-	-	-	-	-	-
Total Non-Operating Revenue (Expense)	20,000	1,333	1,333	26.7%	5,333	5,333
Net Income (Loss) b/Operating Transfers	524,357	67,517	40,195	45.0%	235,721	157,369
Operating Transfers-In	-	-	-	-	-	-
Operating Transfers-Out	(375,000)	(31,250)	(22,917)	33.3%	(125,000)	(91,668)
Net Income (Loss)	149,357	36,267	17,278	N/A	110,721	65,701

** Includes four months budget

CITY OF WORTHINGTON, MINNESOTA

OLSON PARK CAMPGROUND
STATEMENT OF REVENUES VS. EXPENDITURES
For the Period 1/1/25 Through 4/30/25
(Amounts in Dollars)

	Total Current Year Budget	April Actual	YTD	
			Actual	Previous Year
Revenues				
Park fees-daily taxable	92,000	6,957	24,201	31,288
Park fees-other (fire wood, pop & ice)	1,500	-	-	-
Total Revenues	93,500	6,957	24,201	31,288
Expenditures				
Personnel services				
Full-time employees	5,514	-	-	1,323
Part-time employees	21,332	284	284	169
PERA contributions	1,251	-	-	99
FICA/medicare	2,054	22	22	106
Misc. employer paid insurance	1,272	-	-	358
Workmen's comp.-ins. premium	1,480	-	406	493
Supplies				
Misc. office supplies	500	-	-	-
Cleaning supplies	2,500	1,084	1,084	1,066
Misc. operating supplies	1,500	-	817	-
Building repair supplies	3,000	-	-	10
Misc. repair & maint supplies	3,000	-	-	-
Concessions	500	-	-	-
Other services and charges				
Misc. professional services	5,000	583	2,050	1,160
Telephone	750	43	130	124
Misc advertising	1,000	-	-	-
General liability insurance	1,100	-	-	-
Property insurance	1,880	-	-	387
Electric utilities	7,500	111	417	461
Water utilities	950	38	136	139
Gas utilities	2,600	149	683	608
Refuse disposal	3,000	140	180	-
Sewer utilities	700	47	162	141
Buildings-repair & maintenance	1,000	-	-	-
Improv other than bldg-repair & mai	1,000	-	-	-
Machinery/equipment-repair/mainte	-	-	-	-
Misc rentals	-	-	(539)	-
Cash short and over	-	-	-	-
Dues and subscriptions	600	-	-	-
Licenses and taxes	650	-	-	-
Capital outlay				
Improvement Misc	-	-	-	-
Total Expenditures	71,633	2,501	5,832	6,644
Excess (Deficiency) of Revenue Over Expenditures	21,867	4,456	18,369	24,644

CITY OF WORTHINGTON, MINNESOTA

FIELD HOUSE
STATEMENT OF REVENUES VS. EXPENDITURES
For the Period 1/1/25 Through 4/30/25
(Amounts in Dollars)

	Total Current Year Budget	April Actual	YTD	
			Actual	Previous Year
Revenues				
Field House - User fees	45,000	2,603	16,148	18,774
Field House - Rental fees	60,000	6,659	34,697	26,517
Total Revenues	105,000	9,262	50,845	45,291
Expenditures				
Personnel services				
Full-time employees	131,930	11,213	48,753	43,546
Overtime	-	-	-	-
Part-time employees	35,144	2,734	12,259	16,173
Other wage reimbursement	-	-	(20)	-
PERA contributions	9,895	1,356	3,854	3,657
FICA/medicare	12,781	1,572	4,514	4,392
Health insurance admin/claims	30,939	2,789	11,544	11,782
Life Insurance	86	17	37	35
LTD Insurance	892	107	228	198
HSA contribution	3,167	438	1,178	738
Health insurance-claims-TPA	-	-	931	309
Workmen's comp.-ins. premium	3,219	-	2,129	2,299
Supplies				
Office supplies	1,000	49	315	295
Cleaning supplies	4,000	402	2,041	2,048
Motor Fuels	100	-	-	-
Misc. operating supplies	2,500	638	712	228
Equipment parts	2,000	-	-	142
Building repair supplies	1,000	13	13	113
Misc. repair & maint supplies	1,500	415	415	474
Small tools	500	34	34	-
Equipment minor	2,000	-	-	-
Misc. equip, furniture/fixtures	800	-	-	-
Other services and charges				
Management Fees	-	8	32	32
Misc. professional services	8,000	608	7,845	4,739
Telephone	2,000	129	482	628
Postage	100	2	2	-
Travel, conferences, schools	400	-	225	-
Subsistence of Persons	200	-	-	-
Misc advertising	2,500	400	3,054	1,684
General liability insurance	5,100	-	-	1,384
Property Insurance	5,750	-	-	1,181
Electric utilities	23,000	1,129	4,751	3,879
Water utilities	1,000	20	82	87
Gas utilities	8,000	814	4,575	3,674
Refuse disposal	2,000	138	415	641
Sewer utilities	600	47	193	170
Storm water utilities	1,900	218	871	731
Misc. utilities	6,000	1,052	3,920	4,042
Buildings-repair & maintenance	700	-	-	-
Structure repair & maintenance	1,000	22	51	-
Misc repairs & maintenance	3,000	-	-	584
Cash short and over	-	-	*(1,327)*	50*
Dues and subscriptions	2,000	1,129	1,368	239
Licenses and taxes	1,000	-	-	21
Miscellaneous	100	72	72	273
Capital Outlay				
Bldgs & structure-misc	-	-	-	-
Machinery and equipment misc	-	-	-	-
Office equipment purchase	-	-	-	-
Total Expenditures	317,803	27,565	115,548	110,468
Excess (Deficiency) of Revenue				
Over Expenditures	(212,803)	(18,303)	(64,703)	(65,177)

* Cash short and over usually arises from deposit/revenue split between Ice Arena and Field House

ADMINISTRATIVE SERVICES MEMO

DATE: MAY 23, 2025

TO: HONORABLE MAYOR AND CITY COUNCIL

SUBJECT: ITEMS REQUIRING CITY COUNCIL ACTION OR REVIEW

CONSENT AGENDA CASE ITEMS

**1. APPLICATION FOR PARADE PERMIT OR TO BLOCK STREET(S)
-WORTHINGTON INDEPENDENT SCHOOL DISTRICT 518**

The Worthington Independent School District 518 has submitted an application to block the following streets from 5:30 p.m. to 6:30 p.m. on Friday, June 13, 2025 for their 2025 Band Parent Preview Event:

Clary Street Marine Avenue to Church Avenue

A map of the street closure is included as *Exhibit 1*. Mike Thompson has been designated as the Safety Officer for the event. The required insurance certificate naming the City of Worthington as additional insured will be issued once approval of the street closure is obtained.

Council action is requested on the Application to Block Street(s) for the 2025 Band Parent Preview Event.

**2. APPLICATION FOR PARADE PERMIT OR TO BLOCK STREET(S) - SOLID
ROCK CHURCH**

Scott Peterson, Solid Rock Church, has submitted an application to block the following street on Sunday, June 1, 2025 from 11:30 a.m. to 12:30 p.m. for their 2025 Bike Blessing/Ride event:

South Shore Drive from Linda Lane to Freedom Shore Veterans Memorial Park

A map of the street closure is included as *Exhibit 2*. Jay Petersen has been designated as the Safety Officer for the event. The required insurance certificate naming the City of Worthington as additional insured has been provided.

Council action is requested on the Application to Block Street(s) for the Solid Rock Church Bike Blessing/Ride event on June 1, 2025.

CASE ITEMS

1. PFA LOAN EXTENSION

In August 2021, the City of Worthington and the Minnesota Public Facilities Authority (PFA) entered into a loan agreement relating to financing improvements to the municipal wastewater treatment plant. The agreement made up to \$30,322,821 available at 1% interest for a twenty-year term. Under the terms of the agreement, all project related expenses incurred prior to June 30, 2025, were eligible for reimbursement from loan proceeds. Through April 2025, \$28,183,189.45 in loan proceeds have been disbursed and we have repaid a total of \$3,562,821.00 in principal and \$601,956.09 in interest.

Worthington Public Utilities staff was recently contacted by PFA staff inquiring whether Worthington was interested in extending the current June 30, 2025, deadline for eligible expenses to June 30, 2026. While the vast majority of project expenses have been incurred, staff anticipates that there will be additional expenses incurred after June 30, 2025, and indicated to PFA that we would like the one-year extension that they were offering. This extension would not have any impact on the loan maturity date of August 20, 2041.

PFA staff is currently working on the extension document which must be executed prior to June 30, 2025. The agreement will be offered via DocuSign and the Council is requested to authorize the Mayor and the City Clerk to execute the extension document once it is received.

2. NOMINATING COMMITTEE APPOINTMENTS / REAPPOINTMENTS

A Nominating Committee Meeting was held on Thursday, May 22, 2025 and they are making the following recommendations for the following appointments / reappointments:

Planning Commission

Appoint Craig Stock to a first-three year term, term to expire March 31, 2028

Airport Advisory Board

Appoint Brian Larsen to as first three-year term, term to expire September 30, 2028

Appoint Andrew Dorsey to fill the unexpired term of Chad Jaycox, term to expire September 30, 2026

3. COMMITMENT OF SPENDING PLAN FUNDS FOR WETLAND MITIGATION AT THE SOUTH SHORE APARTMENTS DEVELOPMENT

Kuepers Architects and Builders is moving forward with plans for a market rate rental housing development in which the first phase includes of two 38-unit apartment buildings with future plans for three additional buildings. Kuepers' undertook a wetland delineation

study of the project site. The delineation report (***Exhibit 3***) determined that approximately 1.11 acres planned for building and roadway sites are classified as Type 1, 2 or 3 wetlands. Mitigation of the wetlands may be accomplished by buying wetland credits on the basis of two credit acres for each wetland acre. The estimated cost for 2.22 acres of wetland credits is \$155,400 plus engineering and administrative fees.

City Council established a Spending Plan in December 2022 by transferring \$2,329,478 of unobligated TIF 7 funds to the Spending Plan. Funds in the Spending Plan are authorized to "...provide assistance to private development consisting of the development of a variety of housing to address the City's housing needs. This includes, but is not limited to, both market rate and workforce housing along with a combination of owner occupied and rental housing".

The Spending Plan currently has \$1,000,000 in uncommitted funds that may be directed to assist Kuepers and the City in mitigating the wetlands identified in both the developer's building site and the proposed street right-of-way.

Council is requested to commit Spending Plan funds to reimburse the Developer for wetland mitigation in both the building and right-of-way sites. The estimated cost is \$155,400 plus reimbursable expenses.

APPLICATION FOR PARADE PERMIT OR TO BLOCK STREET

Mike Thompson (name), as representative of Independent School District 514 (organization sponsoring event) does hereby apply for a permit for a parade or to temporarily block a street for an event.

The date for the requested parade/event is June 13 / 2025 with the time starting at 5:30 PM and ending at 7:30 PM. The route of this parade or blockage of the street shall be limited to the area delineated on the attached map.

The following person, Mike Thompson, is designated by the requesting organization as safety officer. He/she shall monitor this activity to ensure the safety of the crowd from such hazards as traffic conflicts because of the blocking of the roadways, inspection of barricades, etc. In addition, this person shall be the contact person and shall be available during the event should the police or City officials have concerns with the safety aspects of this event. The requesting organization agrees to conduct this event in the safest possible manner. In those cases where a street or public access is blocked, it shall be done in a clearly visible condition taking into account the speed of traffic and the need to ensure visibility during the various times of the event (taking into account the need for flashers should the obstructions be in place during times of darkness). In addition, said blockades will only be in place as long as necessary. Barricades must be approved by the City of Worthington and be placed according to Uniform Traffic and Marking specifications.

The requesting organization shall provide \$ 1,000,000.00 liability insurance coverage and shall name the City as an additional insured for this event with _____

Insurance Company. The local agent who can confirm this coverage is _____.
A certificate of insurance will be provided after the permit is approved and prior to
conducting the event. The insurance must remain in effect until after the scheduled event.

Mike Thompson

Name of Person Applying for Organization

June 13, 2025

Date

ISD 518

Name of Applying Organization

1117 Marine Ave, Worthington, MN, 56187

Address of Organization

(507) ~~6326~~ 372-2172

Telephone Number of Organization

Mike Thompson

Name of Safety Officer

Address of Safety Officer

Telephone Number of Safety Officer

With Band

Location and Telephone Number of Safety Officer
during the event

Attachments needed:

- 1) Map of delineated area
- 2) Certificate of Insurance

Approved By: _____

Dated: _____

Requested Temporary Street Closures



Exhibit 1

Esri Community Maps Contributors, Esri, HERE, Garmin, SafeGraph, GeoTechnologies, Inc., METI/NA, USGS, EPA, NPS, US Census Bureau, USDA

APPLICATION FOR PARADE PERMIT OR TO BLOCK STREET

Scott Peterson (name), as representative of
Solid Rock Church (organization sponsoring event) does
hereby apply for a permit for a parade or to temporarily block a street for an event.

The date for the requested parade/event is June 1, 2025
with the time starting at 11:30 Am and ending at 12:30 Am. The
route of this parade or blockage of the street shall be limited to the area delineated on the
attached map.

The following person, Jay Peterson, is designated
by the requesting organization as safety officer. He/she shall monitor this activity to ensure
the safety of the crowd from such hazards as traffic conflicts because of the blocking of the
roadways, inspection of barricades, etc. In addition, this person shall be the contact person
and shall be available during the event should the police or City officials have concerns with
the safety aspects of this event. The requesting organization agrees to conduct this event in
the safest possible manner. In those cases where a street or public access is blocked, it shall
be done in a clearly visible condition taking into account the speed of traffic and the need
to ensure visibility during the various times of the event (taking into account the need for
flashers should the obstructions be in place during times of darkness). In addition, said
blockades will only be in place as long as necessary. Barricades must be approved by the
City of Worthington and be placed according to Uniform Traffic and Marking
specifications.

The requesting organization shall provide \$ 1,000,000.00 liability insurance coverage and
shall name the City as an additional insured for this event with Boothman & Mutual

Insurance Company. The local agent who can confirm this coverage is Todd Savage ⁹⁴
A certificate of insurance will be provided after the permit is approved and prior to
conducting the event. The insurance must remain in effect until after the scheduled event.

Scott Peterson
Name of Person Applying for Organization

5-19-25
Date

Solid Rock Church
Name of Applying Organization

1730 Diagonal Rd
Address of Organization

Telephone Number of Organization

Jay Peterson
Name of Safety Officer

Brewster, MN
Address of Safety Officer

Telephone Number of Safety Officer

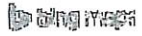
Freedom Shore Park
Location and Telephone Number of Safety Officer
during the event

Attachments needed:

- 1) Map of delineated area
- 2) Certificate of Insurance

Approved By: _____

Dated: _____



Worthington

Area: 8.74 sq miles (22.64 km²)

Population: 13,090 (2015)

Type your notes here.

W L

O k a b e n a L a k e

8th St
2nd Ave
9th St 1st

57 Sherwood St

Flum Creek
Library
System

US Highway 59 Highway 59 and 60 S

S Lake St

59

1000 feet 250 m
© 2017 HERE

S Shore Dr

Bay St

Summit Ave

Knollwood Dr

Prairie
Elementary
School

Knollwood Dr

Sunny Days
Pre-School

Hillcrest Ave

Indian Hill Rd

Edgerton Dr

Pleasant St

Minnesota Dr

Schoon Dr

59

Paul Ave

S Shore Dr

US Highway 59

Flexor Ln
Linda Ln
Lt Ave SW

Plotts Ave

Data from: Wikipedia



Worthington Apartments

Joint Application

Kuepers Inc
April 21, 2025

Submitted by:

Bolton & Menk, Inc.
1960 Premier Drive
Mankato, MN 56001
P: (507) 625-4171
F: (507) 625-4177



Real People. Real Solutions.

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PART TWO: SITE LOCATION INFORMATION	1
PART THREE: GENERAL PROJECT/SITE INFORMATION	1
PART FOUR: AQUATIC RESOURCE IMPACT SUMMARY	2
PART FIVE: APPLICANT SIGNATURE	2
ATTACHMENT A	3

Appendix

WETLAND DELINEATION REPORT

PART ONE: Applicant Information

If applicant is an entity (company, government entity, partnership, etc.), an authorized contact person must be identified. If the applicant is using an agent (consultant, lawyer, or other third party) and has authorized them to act on their behalf, the agent's contact information must also be provided.

Applicant/Landowner Name:	Chhris Raimann Kuepers, Inc.
Mailing Address:	17018 Commercial Park Road Brainerd, MN 56401
Phone:	218-829-0707
E-mail Address:	chrisr@kuepers.com

Authorized Contact (do not complete if same as above):

Mailing Address:

Phone:

E-mail Address:

Agent Name:	Bolton & Menk, Inc. Addeline Theis Paradis Natural Resource Specialist
Mailing Address:	1960 Premier Drive Mankato, MN 56001
Phone:	507-380-4578
E-mail Address:	Addeline.theisparadis@bolton-menk.com

PART TWO: Site Location Information

County: Nobles

City/Township: City of Worthington

Parcel ID and/or Address: 31-0687-100

Legal Description (Section, Township, Range): 26, 102N, 40W

Lat/Long (decimal degrees):

Attach a map showing the location of the site in relation to local streets, roads, highways.

Approximate size of site (acres) or if a linear project, length (feet): 24.24 acres

If you know that your proposal will require an individual Permit from the U.S. Army Corps of Engineers, you must provide the names and addresses of all property owners adjacent to the project site. This information may be provided by attaching a list to your application or by using block 25 of the Application for Department of the Army permit which can be obtained at:

http://www.mvp.usace.army.mil/Portals/57/docs/regulatory/RegulatoryDocs/engform_4345_2012oct.pdf

PART THREE: General Project/Site Information

If this application is related to a delineation approval, exemption determination, jurisdictional determination, or other correspondence submitted **prior to** this application then describe that here and provide the Corps of Engineers project number.

Describe the project that is being proposed, the project purpose and need, and schedule for implementation and completion. The project description must fully describe the nature and scope of the proposed activity including a description of all project elements that effect aquatic resources (wetland, lake, tributary, etc.) and must also include plans and cross section or profile drawings showing the location, character, and dimensions of all proposed activities and aquatic resource impacts.

PART FOUR: Aquatic Resource Impact¹ Summary

If your proposed project involves a direct or indirect impact to an aquatic resource (wetland, lake, tributary, etc.) identify each impact in the table below. Include all anticipated impacts, including those expected to be temporary. Attach an overhead view map, aerial photo, and/or drawing showing all of the aquatic resources in the project area and the location(s) of the proposed impacts. Label each aquatic resource on the map with a reference number or letter and identify the impacts in the following table.

Aquatic Resource ID (as noted on overhead view)	Aquatic Resource Type (wetland, lake, tributary etc.)	Type of Impact (fill, excavate, drain, or remove vegetation)	Duration of Impact Permanent (P) or Temporary (T) ¹	Size of Impact ²	Overall Size of Aquatic Resource ³	Existing Plant Community Type(s) in Impact Area ⁴	County, Major Watershed #, and Bank Service Area # of Impact Area ⁵

¹If impacts are temporary; enter the duration of the impacts in days next to the "T". For example, a project with a temporary access fill that would be removed after 220 days would be entered "T (220)".

²Impacts less than 0.01 acre should be reported in square feet. Impacts 0.01 acre or greater should be reported as acres and rounded to the nearest 0.01 acre. Tributary impacts must be reported in linear feet of impact and an area of impact by indicating first the linear feet of impact along the flowline of the stream followed by the area impact in parentheses). For example, a project that impacts 50 feet of a stream that is 6 feet wide would be reported as 50 ft (300 square feet).

³This is generally only applicable if you are applying for a de minimis exemption under MN Rules 8420.0420 Subp. 8, otherwise enter "N/A".

⁴Use *Wetland Plants and Plant Community Types of Minnesota and Wisconsin* 3rd Ed. as modified in MN Rules 8420.0405 Subp. 2.

⁵Refer to Major Watershed and Bank Service Area maps in MN Rules 8420.0522 Subp. 7.

If any of the above identified impacts have already occurred, identify which impacts they are and the circumstances associated with each:

N/A

PART FIVE: Applicant Signature

☐ Check here if you are requesting a pre-application consultation with the Corps and LGU based on the information you have provided. Regulatory entities will not initiate a formal application review if this box is checked.

By signature below, I attest that the information in this application is complete and accurate. I further attest that I possess the authority to undertake the work described herein.

Signature:  Date: 4-22-2025

I hereby authorize **Bolton & Menk, Inc** to act on my behalf as my agent in the processing of this application and to furnish, upon request, supplemental information in support of this application.

¹ The term "impact" as used in this joint application form is a generic term used for disclosure purposes to identify activities that may require approval from one or more regulatory agencies. For purposes of this form it is not meant to indicate whether or not those activities may require mitigation/replacement.

Attachment A

Request for Delineation Review, Wetland Type Determination, or Jurisdictional Determination

By submission of the enclosed wetland delineation report, I am requesting that the U.S. Army Corps of Engineers, St. Paul District (Corps) and/or the Wetland Conservation Act Local Government Unit (LGU) provide me with the following (check all that apply):

☒ **Wetland Type Confirmation**

☒ **Delineation Concurrence.** Concurrence with a delineation is a written notification from the Corps and a decision from the LGU concurring, not concurring, or commenting on the boundaries of the aquatic resources delineated on the property. Delineation concurrences are generally valid for five years unless site conditions change. Under this request alone, the Corps will not address the jurisdictional status of the aquatic resources on the property, only the boundaries of the resources within the review area (including wetlands, tributaries, lakes, etc.).

☐ **Preliminary Jurisdictional Determination.** A preliminary jurisdictional determination (PJD) is a non-binding written indication from the Corps that waters, including wetlands, identified on a parcel may be waters of the United States. For purposes of computation of impacts and compensatory mitigation requirements, a permit decision made on the basis of a PJD will treat all waters and wetlands in the review area as if they are jurisdictional waters of the U.S. PJDs are advisory in nature and may not be appealed.

☐ **Approved Jurisdictional Determination.** An approved jurisdictional determination (AJD) is an official Corps determination that jurisdictional waters of the United States are either present or absent on the property. AJDs can generally be relied upon by the affected party for five years. An AJD may be appealed through the Corps administrative appeal process.

In order for the Corps and LGU to process your request, the wetland delineation must be prepared in accordance with the 1987 Corps of Engineers Wetland Delineation Manual, any approved Regional Supplements to the 1987 Manual, and the *Guidelines for Submitting Wetland Delineations in Minnesota* (2013).

<http://www.mvp.usace.army.mil/Missions/Regulatory/DelineationJDGuidance.aspx>

Appendix



Worthington Apartments

Wetland Delineation Report

Kuepers Inc
April 21, 2025

Submitted by:
Bolton & Menk, Inc.
1960 Premier Drive
Mankato, MN 56001
P: (507) 625-4171
F: (507) 625-4177

Exhibit 3



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Appendix

Exhibit A: Site Location Map
Exhibit B: 2-Foot LiDAR Contours Map
Exhibit C: National Wetlands Inventory Map
Exhibit D: Public Waters Inventory Map
Exhibit E: Nobles County Soil Survey Map
Exhibit F: Delineated Wetlands Map
Exhibit G: Historical Aerial Imagery
Exhibit H: Off-site Data Sheet
Exhibit I: Delineation Data Sheets

I. INTRODUCTION

Kuepers Inc is proposing building apartments at this 24.24 acres property located southern portion of the City of Worthington, found in Nobles County. This report details the findings of the field delineation completed on April 16, 2025.

The project is found in Section 26, in Township 102 North of Range 40 West.

II. WETLAND DELINEATION METHODOLOGY

The wetland boundaries were delineated and staked in the field in April 2025, using methods described in the “Regional Supplement to the Corps of Engineers Wetland Delineation Manual: Midwest Region (Version 2.0)”. Wetlands identified were classified using “Classification of Wetlands and Deepwater Habitats of the United States (Cowardin, et al., 1979)”, “Wetlands of the United States (United States Fish and Wildlife Service Circular No. 39, 1971 edition)” and “Wetland Plants and Plant Communities of Minnesota and Wisconsin” (Eggers and Reed Third Edition). Subsequently, the three mandatory technical criteria for wetland determinations are as follows:

Hydrophytic Vegetation. A hydrophytic plant community is present when the dominant plant species present can endure prolonged inundation and/or soil saturation during the growing season. A plant’s Wetland Indicator Status is determined using the 2016 National Wetland Plant List for Minnesota, published by the Army Corp of Engineers.

Hydric Soils. A hydric soil is defined as a soil that is formed under conditions of saturation, flooding or ponding long enough during the growing season (the portion of the year when there is above ground growth and development of vascular plants and/or soil temperature at 12 inches below the soil surface is above 41 degrees Fahrenheit or higher) to develop anaerobic conditions in the upper part.

Wetland Hydrology. An area has wetland hydrology if it experiences 14 or more consecutive days of flooding, ponding or a water table within 12 inches of the surface during the growing season at a minimum frequency of five out of ten years. This is determined by using both primary and secondary Wetland Hydrology indicators.

III. BACKGROUND INFORMATION

Prior to conducting a field investigation of this site, Exhibits A through E were used to complete a preliminary evaluation. The data gathered during the preliminary investigation was used as described below:

Exhibit A is a location map of the study area.

Exhibits B is an aerial photo with topographic information overlaid on it. This provides information regarding topography of the site, helping to identify areas that may have wetland characteristics.

Exhibit C is the National Wetlands Inventory of the site and surrounding properties. This information is used to complete a preliminary investigation of the wetlands that may or may not exist on the site.

Exhibit D is used to identify waters that are regulated by the DNR. This exhibit shows where there are DNR public waters relative to the site.

Exhibit E is the Nobles County Soil Survey and is used to identify hydric soils that may lie within the study area.

Delineation Exhibits F and I were prepared from the information gathered at the site.

Exhibit F is the site map showing the delineated aquatic resources.

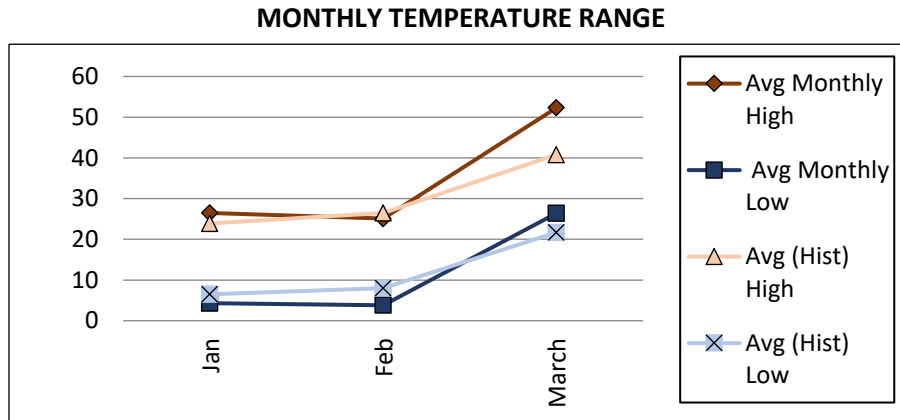
Exhibit G is the historical aerial imagery figures that show the land use over time.

Exhibit H includes the offsite data sheet.

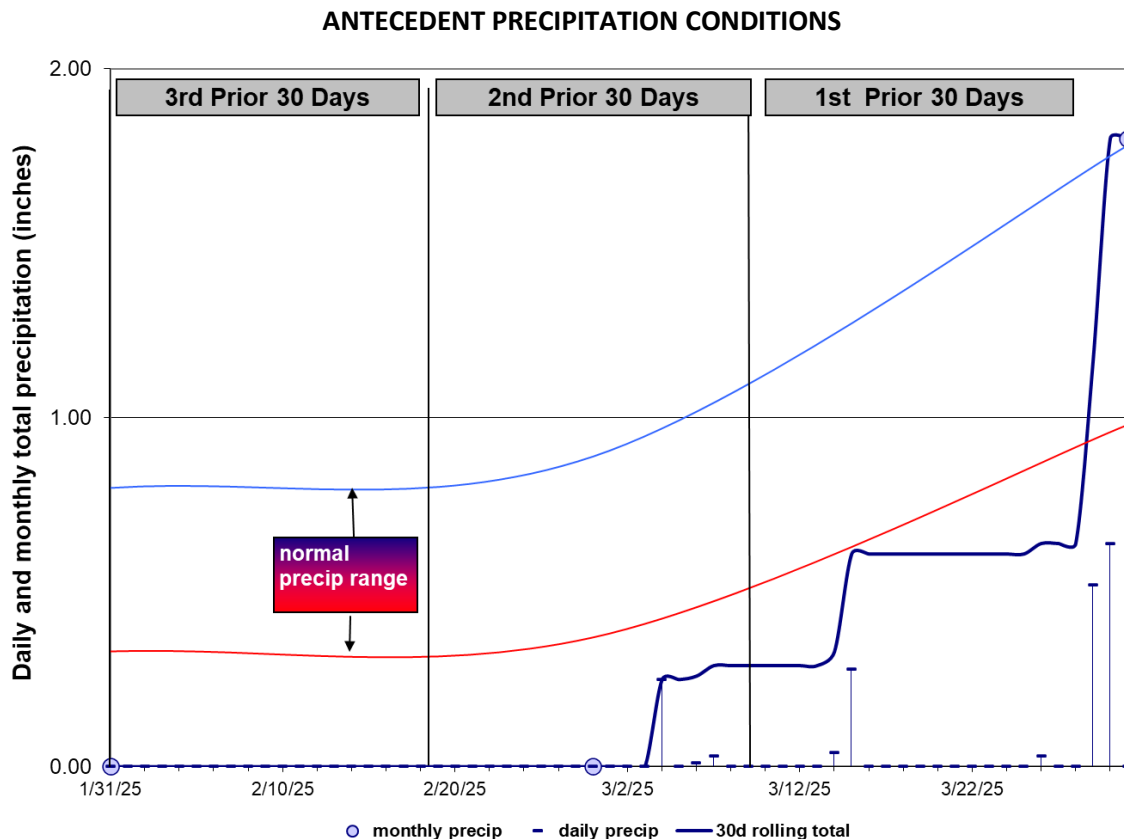
Exhibit I includes the wetland delineation data sheets.

IV. CLIMATE DATA

The monthly temperature table below shows the average high and low temperatures for the three months prior to the field delineation, along with the historical averages for these months. The monthly low and high temperatures have been within normal ranges for this period.



Antecedent precipitation was evaluated using a combination of the NRCS Method and the Rolling Totals Method. The analysis found that precipitation was below normal over the past two months.



This climatic data was gathered using the Climatology Working Group Website, <http://climate.umn.edu/> and the National Weather Service Forecast Office, <http://w2.weather.gov/climate/>. The information for the investigation was retrieved from the WETS Station in Worthington 2 NNE (ID 219170)

V. FINDINGS

On April 16, 2025 a field investigation was performed to evaluate and verify the existence and boundary of any aquatic resources located within the study area. Two wetlands complex were found to exist within the study area. The following describes the aquatic resources identified, together with a brief description of wetland types and observations made during the field investigation.

Wetland 1 (W1-A & W1-B):

NWI Cowardin: None

PWI (Hydro) ID: None

Field Observation Circular 39: Type 1

Field Observation Eggers and Reed:

Seasonally Flooded Basin

Field Observation HGM: Depression

Soil Mapping Unit(s): L83A-Webster clay loam

Wetland 1 consists of closed concave depression that holds water in this agricultural field. No tile inlet or outlet was observed at this site.

The field investigation found that the site has met all three wetland indicators and this wetland complex should be considered a combination of a palustrine emergent persistent temporarily flooded (PEM1A) wetland. Two sample points were taken to determine the wetland boundary. The wetland boundary was largely drawn based on the changes in topography.

Both the W1-A wetland pit location and W1-B upland pit location had vegetation significantly disturbed from continuous row crop agriculture. No germination of crops yet and no volunteers present. W1-A wetland pit location would have hydrophytic vegetation develop overtime without the affects of pesticides. Soils in the W1-A and W1-B both met the hydric soil indicator of A12- Thick Dark Soils. The W1-A wetland pit location met two secondary hydrology indicators of D2- Geomorphic Position and C9-Saturation Visible on Aerial Imagery. The W1-B upland pit location did not meet any of the hydrology indicators.

Wetland 2 (W2-A & W2-B):

NWI Cowardin: None

PWI (Hydro) ID: None

Field Observation Circular 39: Types 1, 2 & 3.

Field Observation Eggers and Reed:

Seasonally Flooded Basin, Fresh (wet)

Meadow and Shallow Marsh

Field Observation HGM: Depression

Soil Mapping Unit(s): L83A-Webster clay loam

Wetland 2 consists of a combination of wetland types. Landowner has farmed the edge of this wetland throughout the years.

The field investigation found that the site has met all three wetland indicators and this wetland complex should be considered a combination of a palustrine emergent



Photo 1: View of the W1-A & W1-B sample points looking into the wetland.



Photo 2: View of the W2-A & W2-B sample points looking into the wetland.

persistent temporarily flooded (PEM1A) wetland, palustrine emergent persistent seasonally saturated (PEM1B), and palustrine emergent persistent seasonally flooded (PEM1C). Two sample points were taken to determine the wetland boundary. The wetland boundary was largely drawn based on the changes in topography.

Both the W2-A wetland pit location and W2-B upland pit location had vegetation significantly disturbed from continuous row crop agriculture. No germination of crops yet and no volunteers present.. Soils in the W2-A and W2-B both met the hydric soil indicator of A12-Thick Dark Soils. The W2-A wetland pit location met two secondary hydrology indicators of D2-Geomorphic Position and C9-Saturation Visible on Aerial Imagery. The W2-B upland pit location did not meet any of the hydrology indicators.



Photo 3: View Wetland 2 from the road.



Photo 4: View of Wetland 2 from the south of the study area.



Photo 4: View of infiltration pond found offsite but near wetland 2.



Photo 5: View of wetland 2 from the shallow marsh looking at the fresh (wet) meadow.

VI. CONCLUSION

The delineation was performed on April 16, 2025. The boundaries of the wetlands were staked in the field with three foot "Wetland Delineation" pin flags. The location of the pin flags were surveyed by Bolton & Menk, Inc. using a Juniper Geode Data Collector and tied to the Nobles County coordinate system. The delineated limits are believed to be the upper limits of where all three of the required wetland criteria were present.

Bolton & Menk, Inc., was asked to determine the boundaries of those jurisdictional wetlands that exist upon this property as defined by the Wetland Conservation Act.

Based upon all available information, the existing conditions that currently prevail, and the on-site investigation, evidence supports the presence of two wetland complex within the boundaries of the study area.

WETLAND SUMMARY

Id #	Wetland Type^	Size*
W1	Type 1 (Seasonally Flooded Basin)	0.05 ac
W2	Type 1 (Floodplain Forest)	0.64 ac
W2	Type 2 (Fresh (wet) Meadow)	0.20 ac
W2	Type 3 (Shallow Marsh)	0.27 ac
Total:		1.16 ac

**size measured within study area.*

^wetland type within study area

Sincerely,
BOLTON & MENK, INC.



Addeline Theis Paradis
Certified Wetland Delineator, No. 1434

APPENDIX

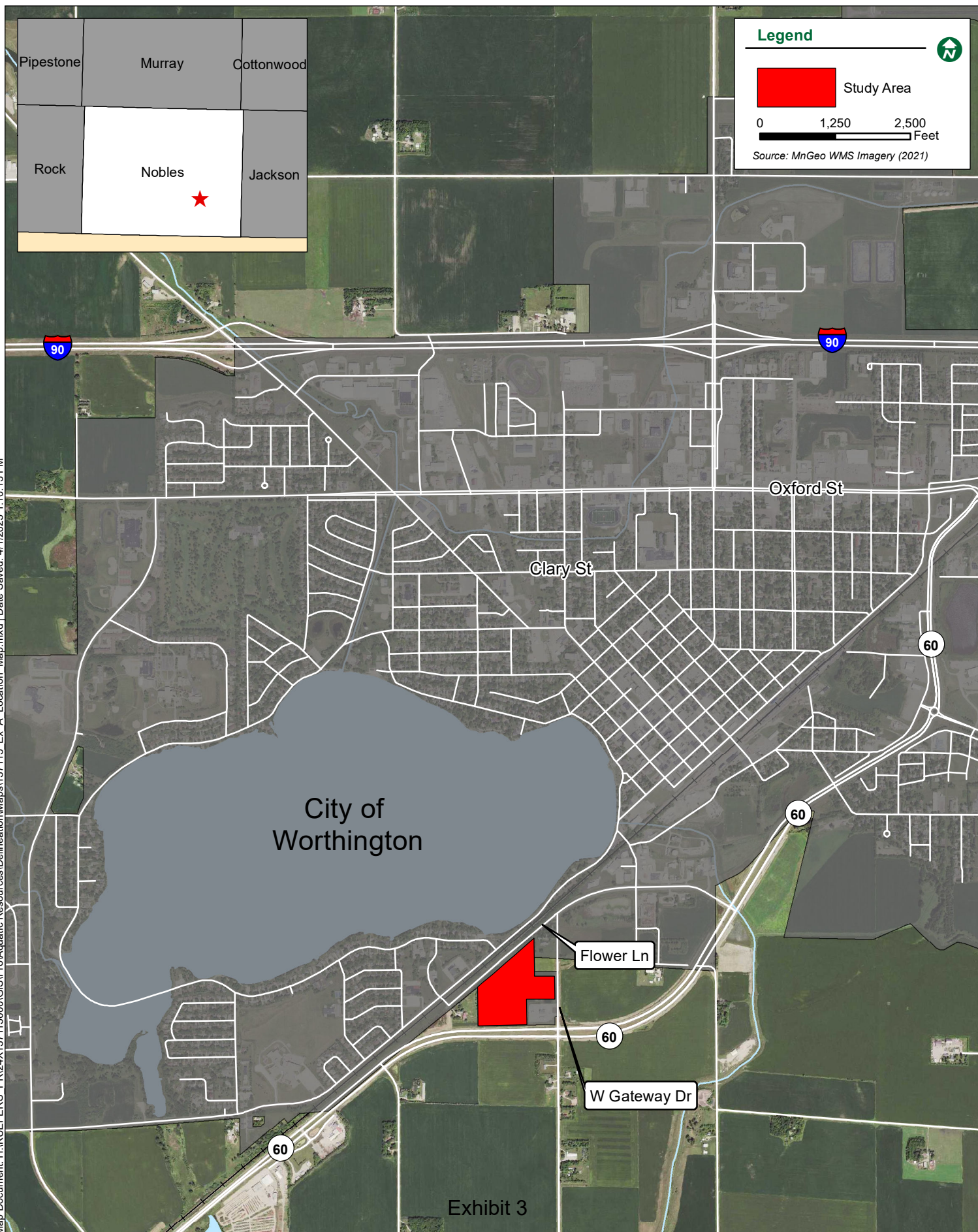


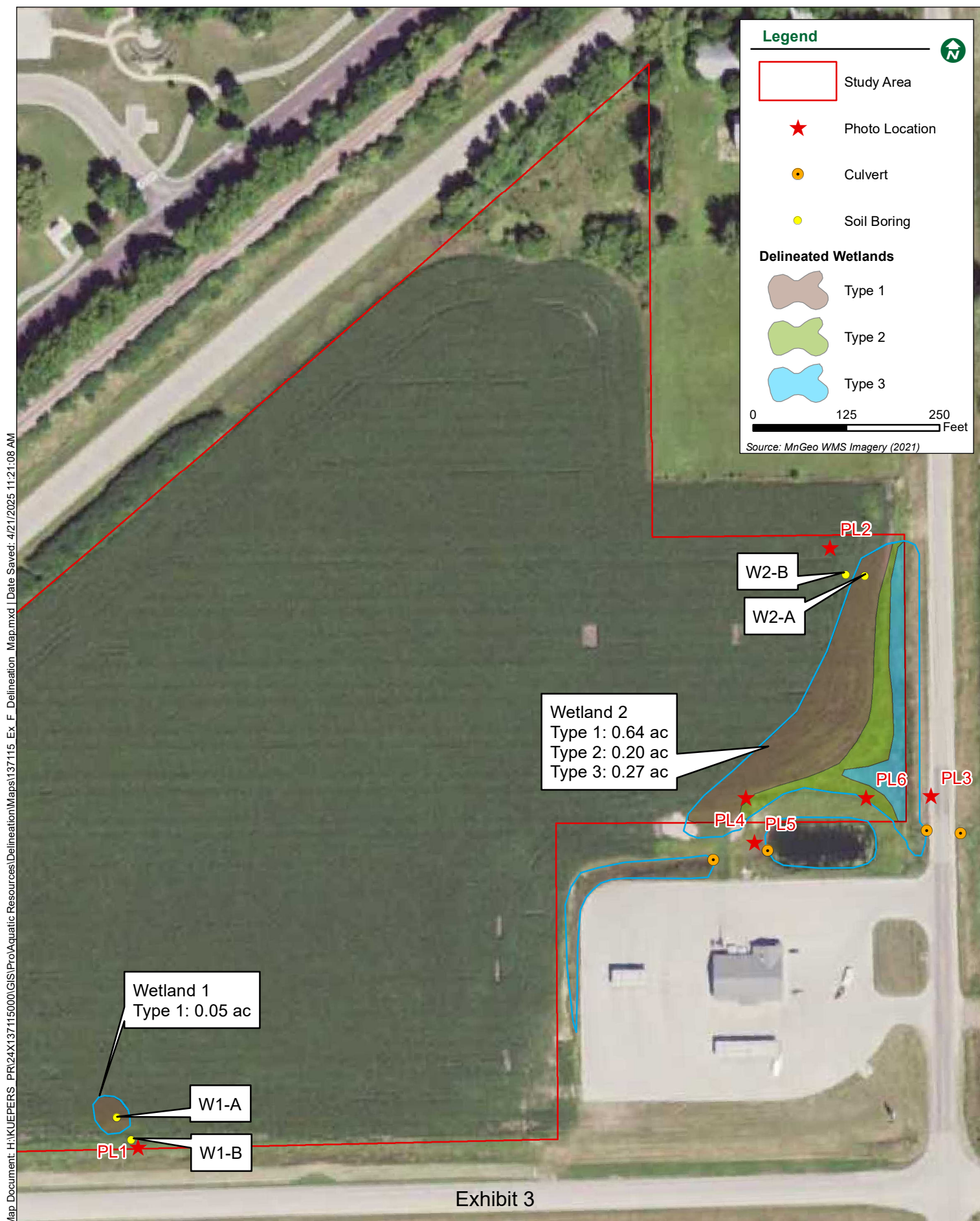


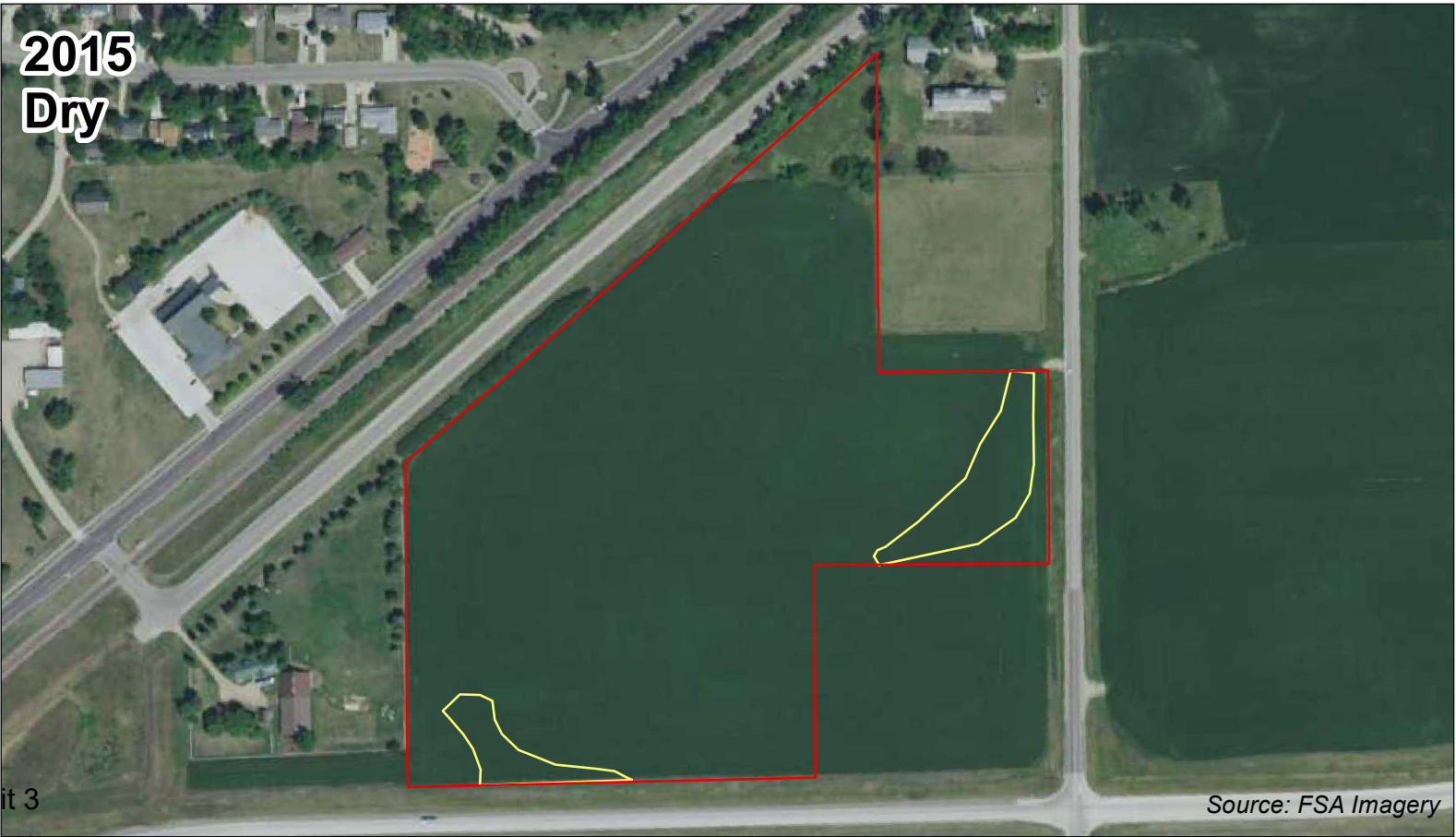
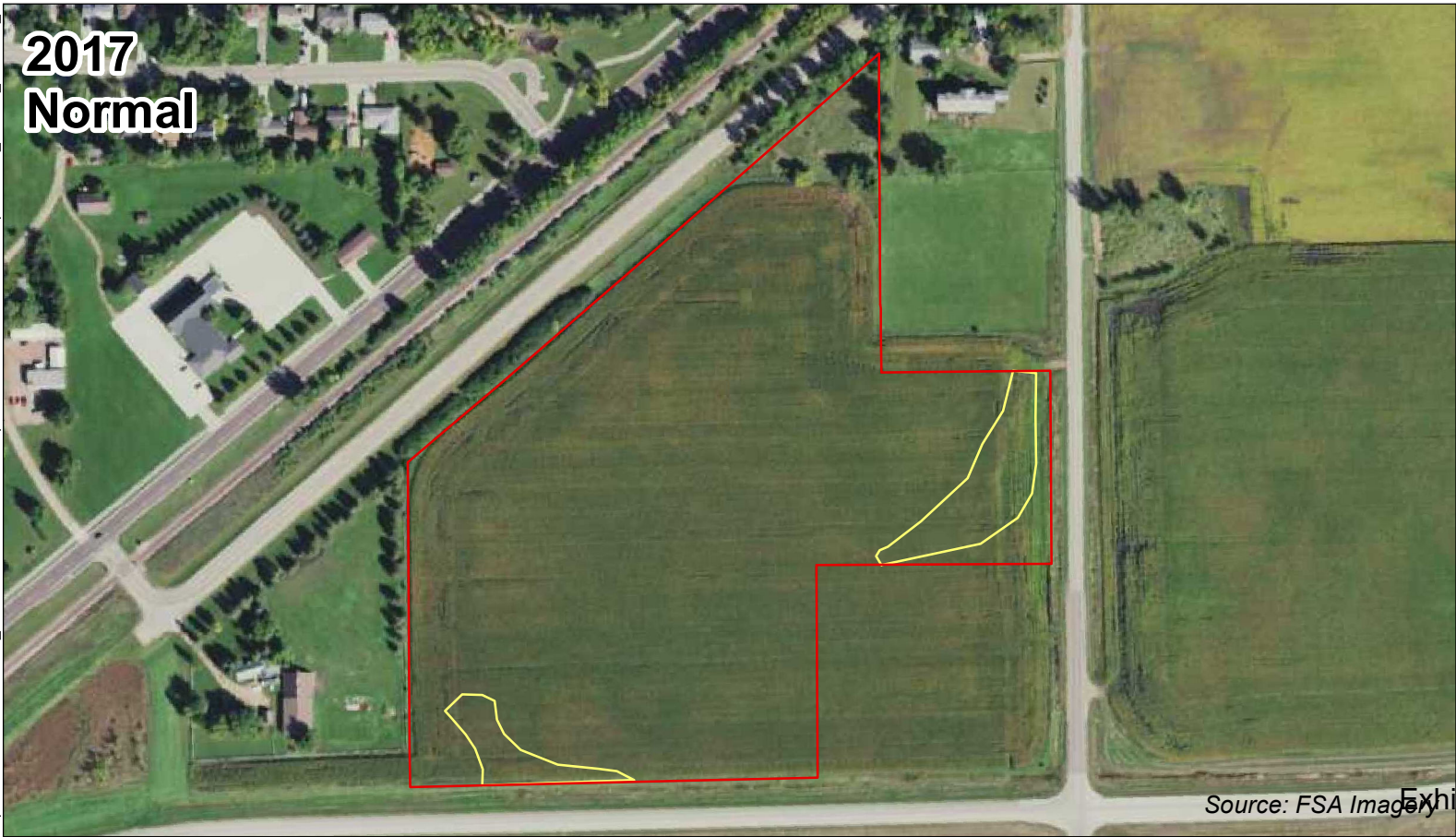
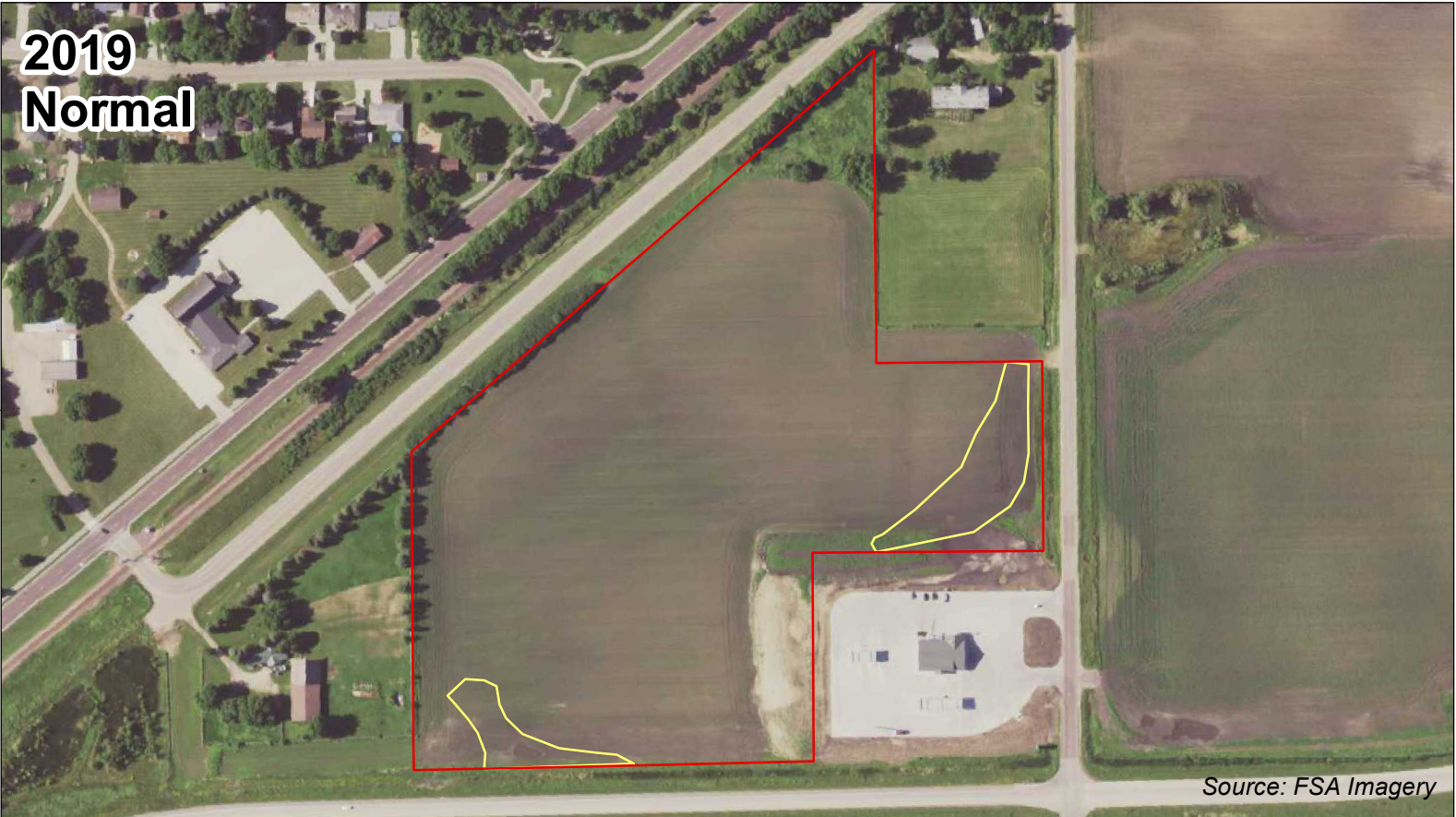
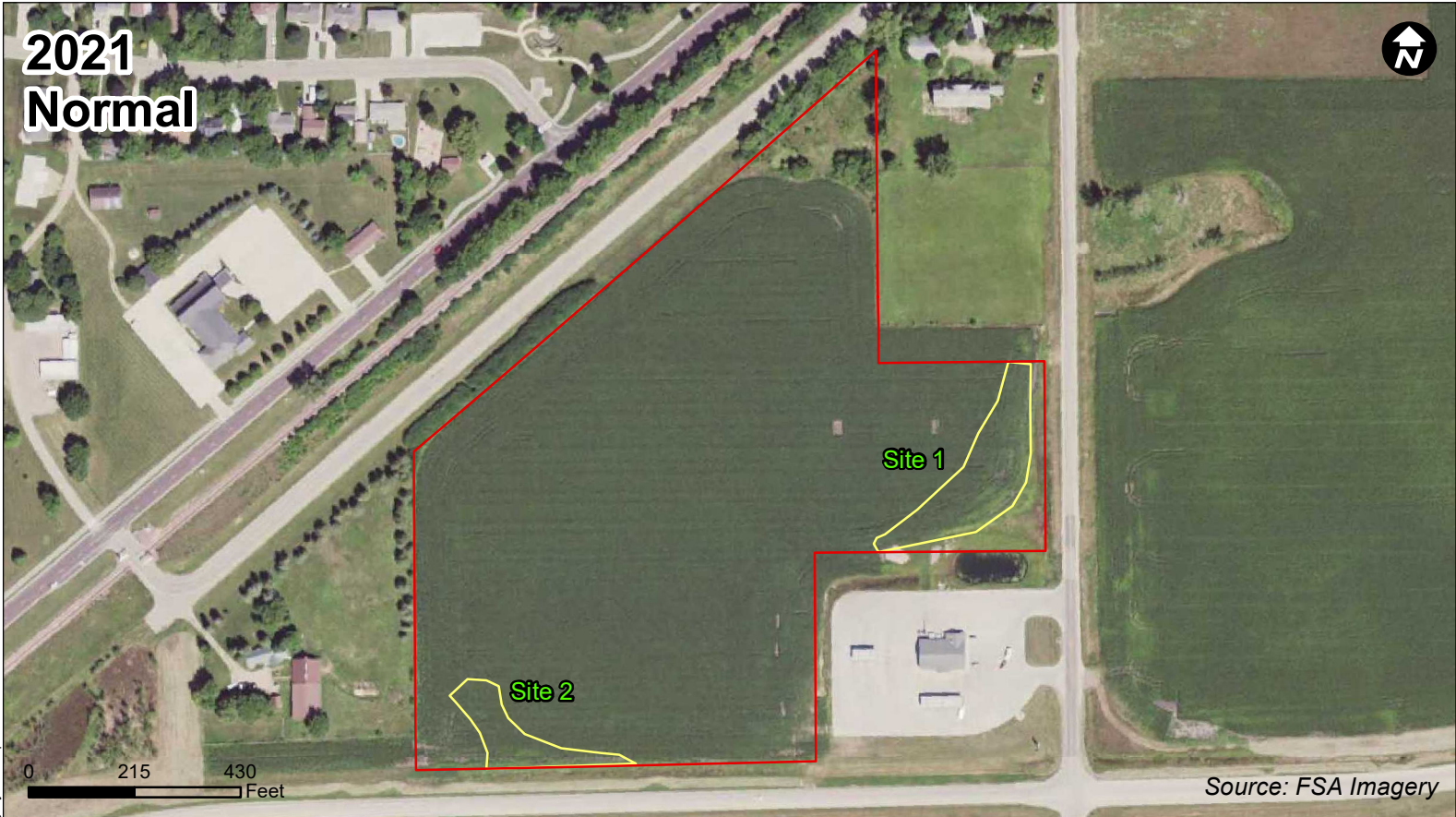


Exhibit 3

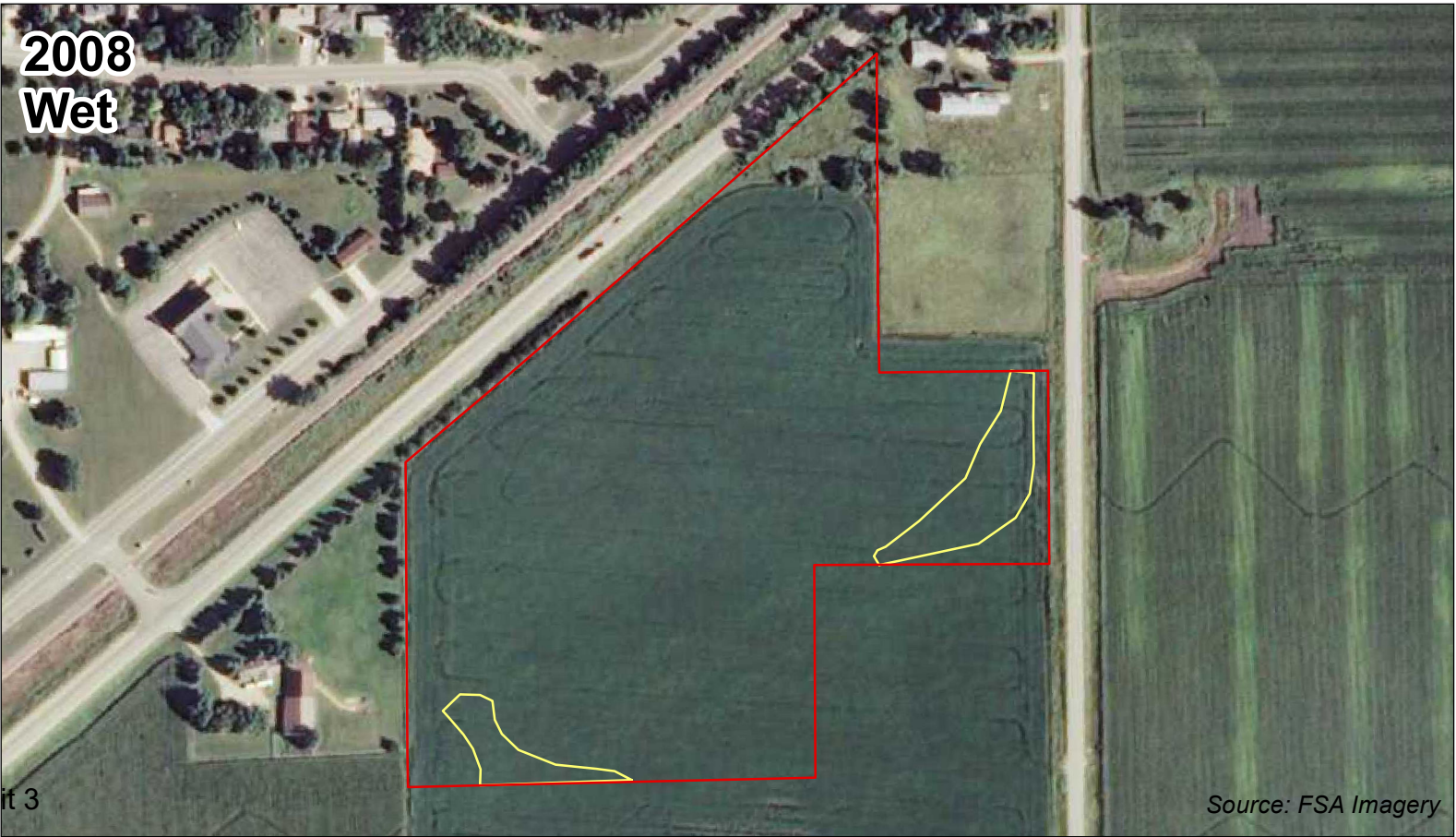
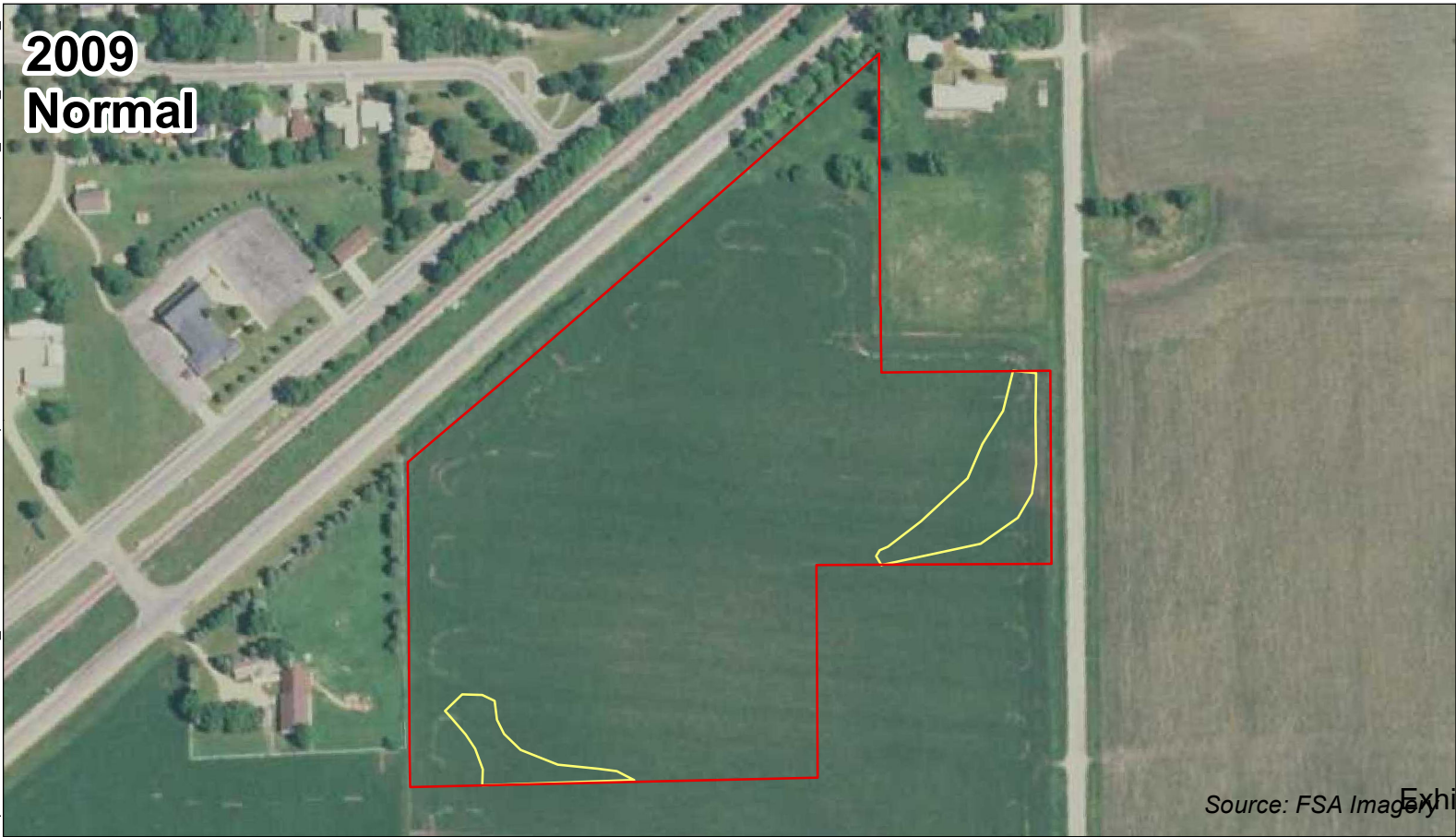
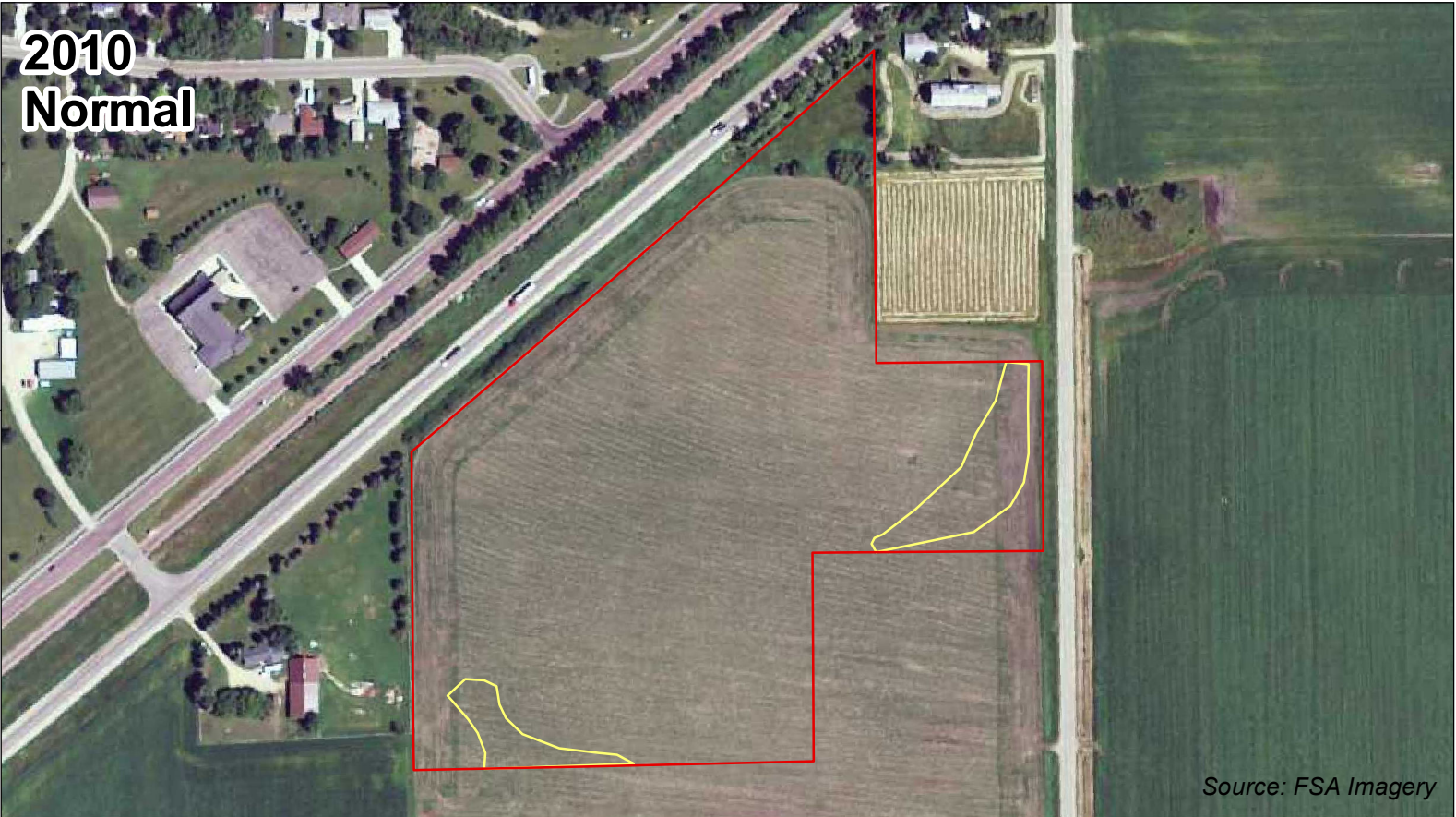
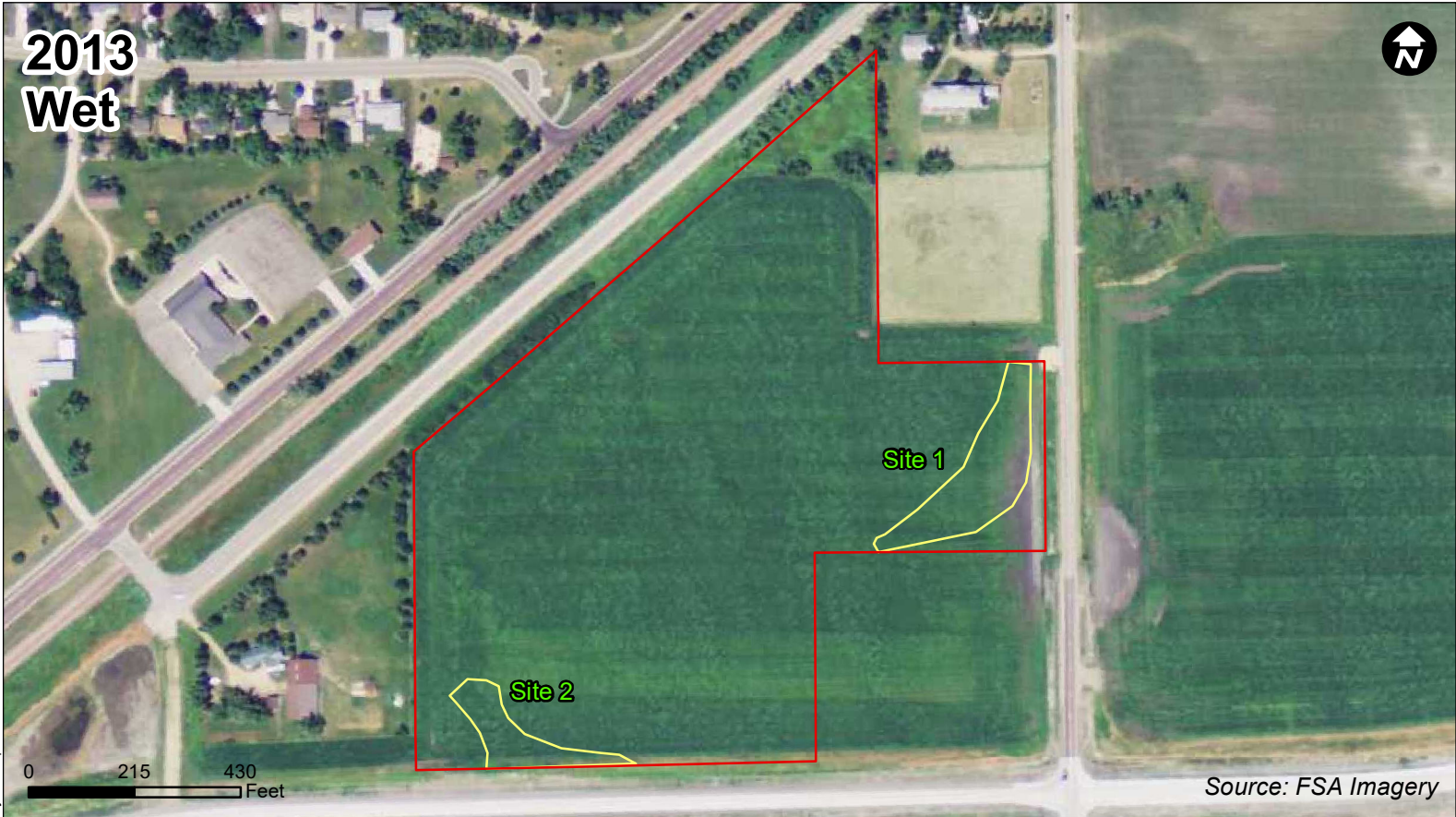




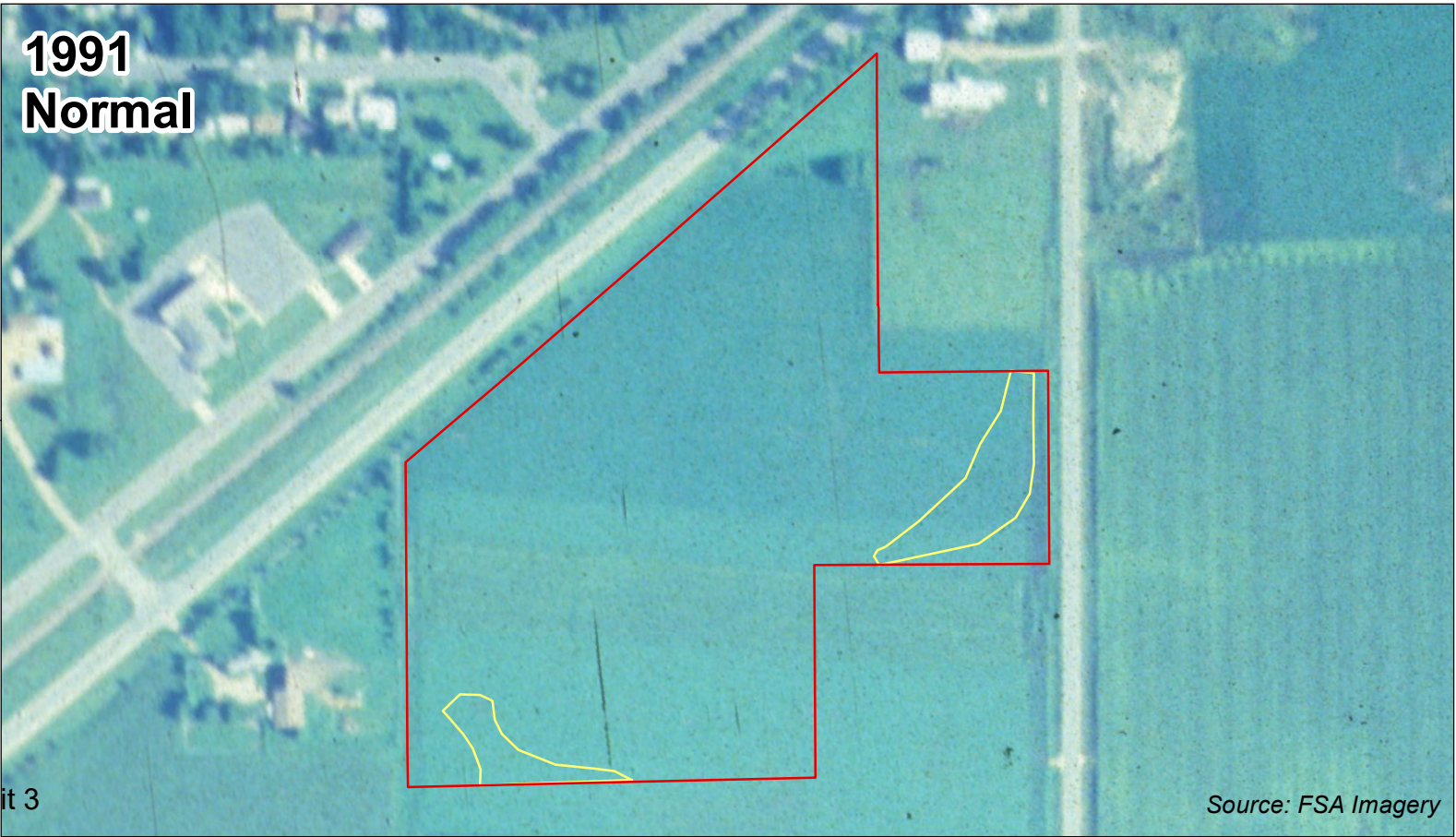
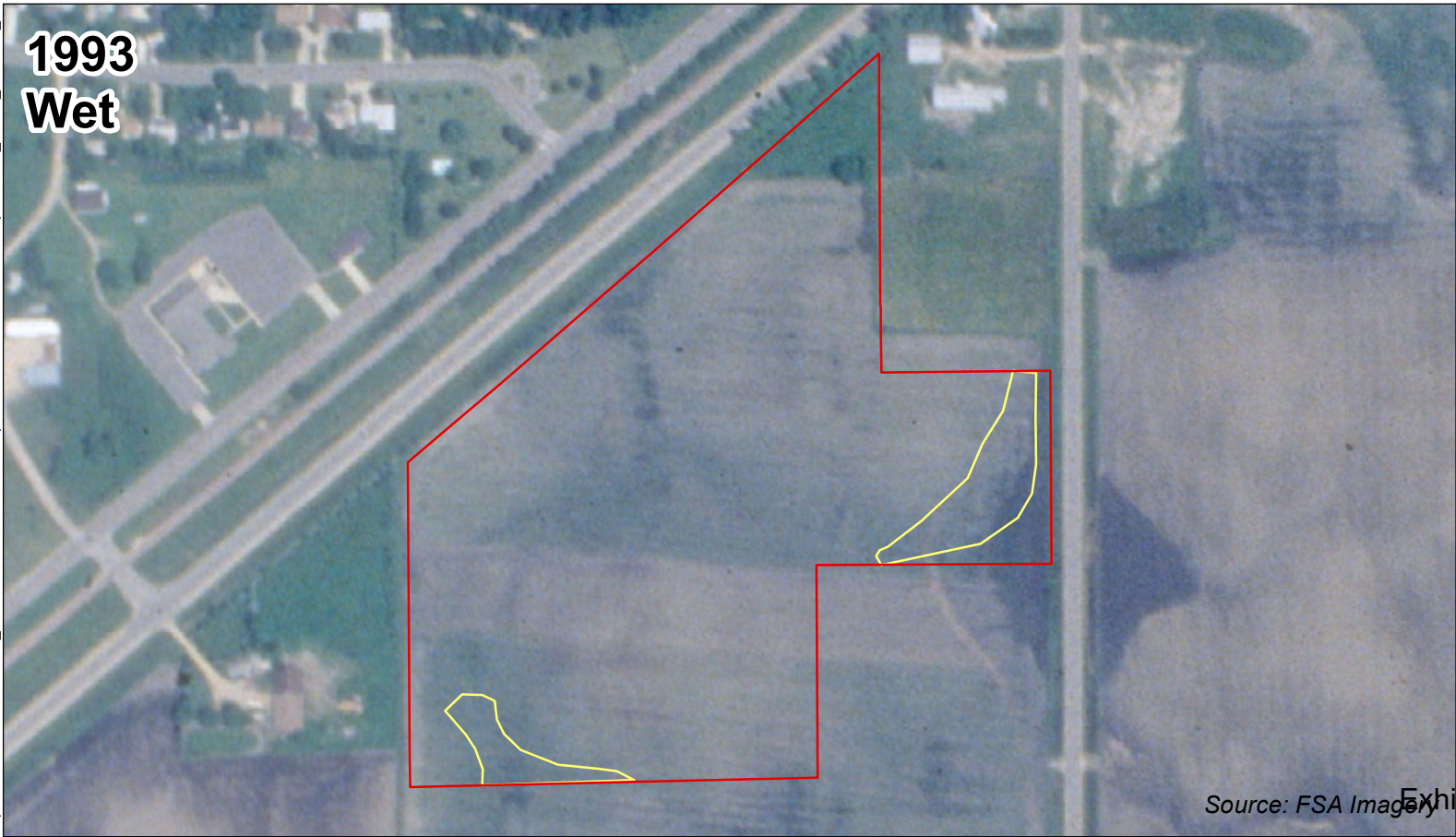
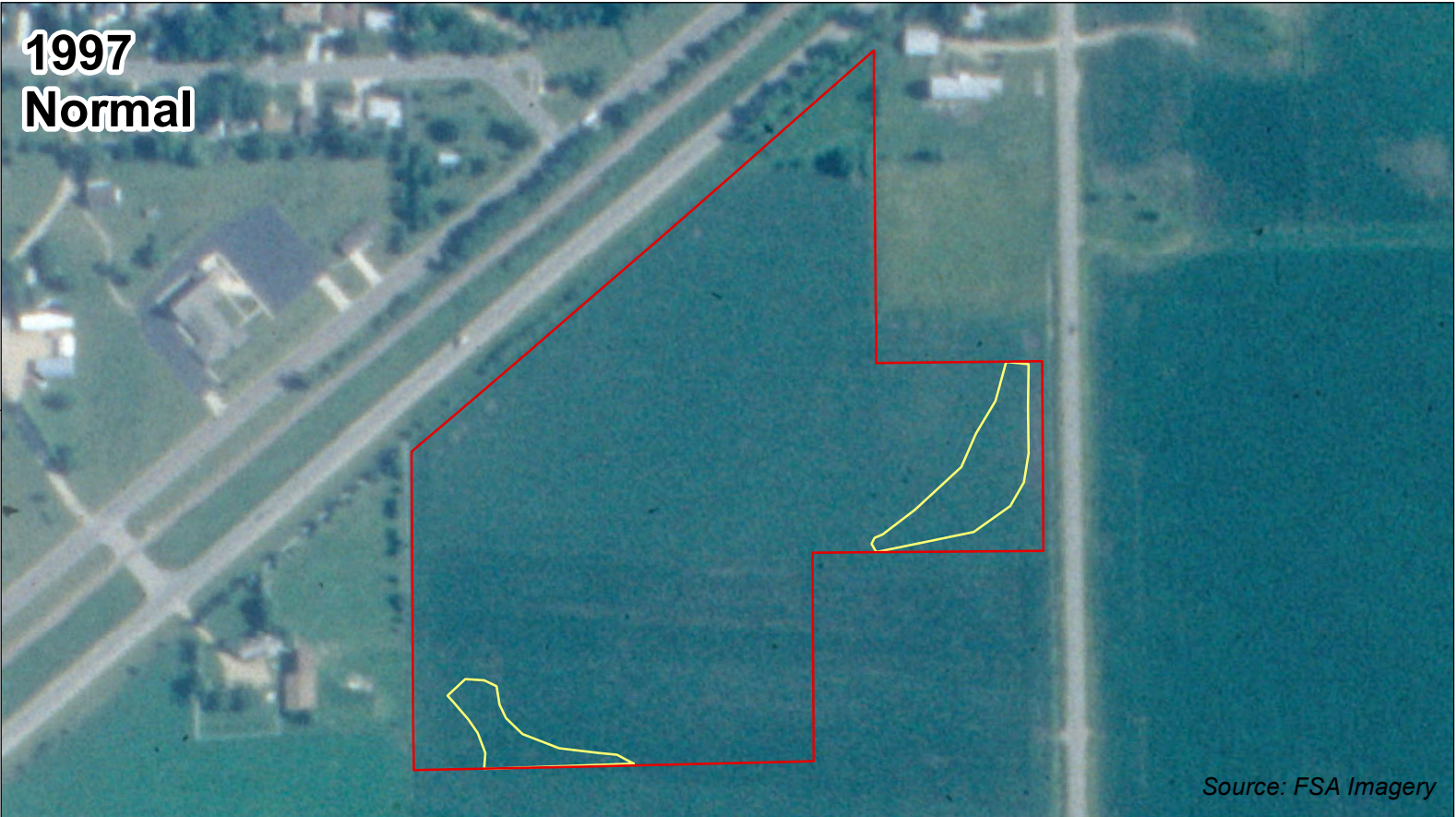




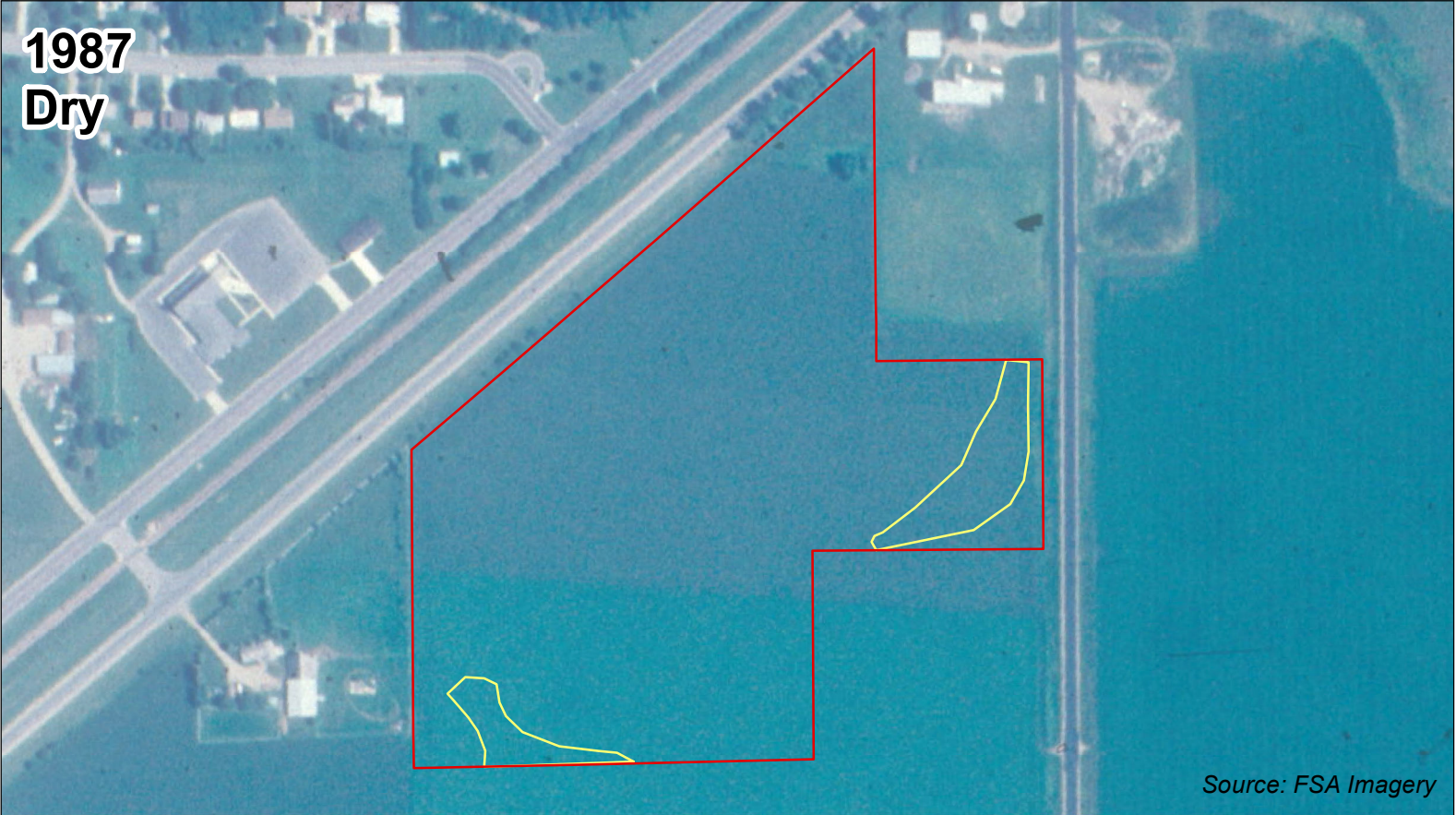
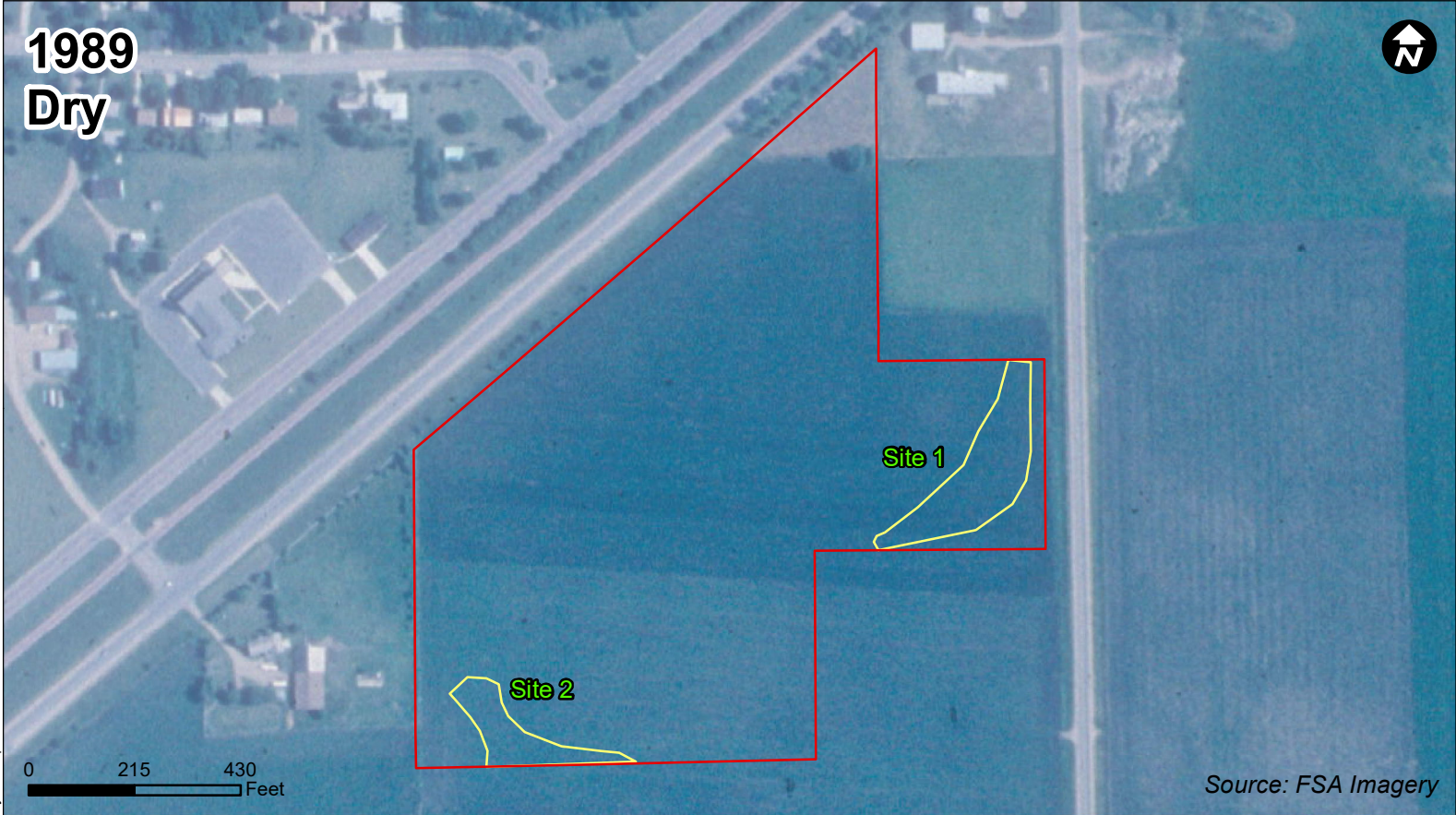
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EXHIBIT H:
OFF-SITE HYDROLOGY ASSESSMENT
RECORDING FORM

Project/Site:

Applicant/Owner:

Investigator(s):

WETS Station ID:

Worthington Apartments

Chris Raimann

Addeline Theis Paradis

St. James WWTP 217326

City/County:

State:

Sec, Twp, Ran:

Date:

City of Worthington/ Nobles County

Minnesota

S26 T102 R40

4/1/2025

Date:	Source:	Assumed Month:	Climatic Condition:	Image Interpretations			
				Site 1	Site 2		Notes
2021	FSA	June	Normal	CS	CS		
2019	FSA	July	Normal	DO	DO		
2017	FSA	July	Normal	NV	NV		
2015	FSA	July	Dry	NV	NV		
2013	FSA	July	Wet	CS	NV		
2010	FSA	July	Normal	NV	NV		
2009	FSA	August	Normal	CS	NV		
2008	FSA	July	Wet	NV	NV		
2003	FSA	July	Normal	NV	NV		
1997	FSA	July	Normal	NV	NV		
1993	FSA	June	Wet	DO	DO		
1991	FSA	June	Normal	NV	NV		
1989	FSA	July	Dry	NV	NV		
1987	FSA	July	Dry	NV	NV		
1954			Normal	NV	NV		
1938			Wet	NV	NV		
Hydric Soil				Yes	Yes		
NWI				No	No		
Normal Years				9	9		
Wet Signatures				3	2		
Percent Wet Signatures				33%	22%		
NV - Normal Vegetation, WS - Wet Signature, CS - Crop Stress, DO - Drown Out, SW - Standing Water, AP - Altered Pattern, NC - Not Cropped							

Decision Matrix					
Hydric Soil	NWI	% Wet	% Wet	Field visit?	Wetland?
Yes	Yes	>50%	>50%	No	Yes
Yes	Yes	30-50%	30-50%	No	Yes
Yes	Yes	<30%	<30%	Yes	Yes, w/field hydro
Yes	No	>50%	>50%	No	Yes
Yes	No	30-50%	30-50%	Yes	Yes, w/field hydro
Yes	No	<30%	<30%	No	No
No	Yes	>50%	>50%	No	Yes
No	Yes	30-50%	30-50%	No	Yes
No	Yes	<30%	<30%	No	No
No	No	>50%	>50%	Yes	Yes, w/field hydro
No	No	30-50%	30-50%	Yes	Yes, w/field hydro
No	No	<30%	<30%	No	No



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EXHIBIT I: WETLAND DETERMINATION DATA FORM

(Midwest Region)

Project/Site: Worthington Apartments City/County: City of Worthington/Nobles County Sampling Date: 4/16/2025
Applicant/Owner: Kuepers Inc State: Minnesota Sample Point: W1-A
Investigator(s): Addeline Theis Paradis Section, Township, Range: S26, T102, R40
Landforms (hillside, terrace, etc.): Depression Local Relief (concave, convex, none): Concave
Slope (%): 0-2% Latitude: _____ Longitude: _____ Datum: _____
Soil Map Unit Name: L83A-Webster Clay Loam NWI Classification: None

Are climatic/hydrologic conditions of the site typical for this time of year? No (If no, explain in remarks)
Are vegetation X, soils _____, or hydrology _____ significantly disturbed? Are normal circumstances present? No
Are vegetation _____, soils _____, or hydrology _____ naturally problematic? (If needed, explain any answers in Remarks)

SUMMARY OF FINDINGS

Hydrophytic vegetation present?	<u>Yes</u>	Is the sampled area within a wetland? <u>Yes</u>
Hydric soils present?	<u>Yes</u>	
Wetland hydrology present?	<u>Yes</u>	
Remarks: <u>Antecedent precipitation is below normal.</u>		

VEGETATION - Use scientific names of plants

Tree Stratum	(Plot size: <u>30 feet</u>)	Absolute % Cover	Dominant Species	Indicator Status	Dominance Test Worksheet Number of dominant species that are OBL, FACW, or FAC: _____ (A) Total number of dominant species across all strata: _____ (B) Percent of dominant species that are OBL, FACW or FAC: _____ (A/B)
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	=Total Cover _____
Sapling/Shrub stratum	(Plot size: <u>15 feet</u>)				Prevalence Index Worksheet Total % cover of: OBL Species: _____ x 1 = _____ FACW Species: _____ x 2 = _____ FAC Species: _____ x 3 = _____ FACU species: _____ x 4 = _____ UPL Species: _____ x 5 = _____ Totals: _____ (A) _____ (B) Prevalence Index (B/A): _____
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	=Total Cover _____
Herb stratum:	(Plot size: <u>5 feet</u>)				Hydrophytic Vegetation Indicators Rapid test for hydrophytic vegetation _____ Dominance test >50% _____ Prevalence index is ≤3.0* _____ Morphological adaptations* (Provide supporting data in remarks) _____ Problematic hydrophytic vegetation* <u>X</u> (Explain in remarks)
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	
6	_____	_____	_____	_____	
7	_____	_____	_____	_____	
8	_____	_____	_____	_____	
9	_____	_____	_____	_____	
10	_____	_____	_____	_____	
					*Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic
=Total Cover _____					
Woody vine stratum:	(Plot size: <u>15 feet</u>)				Hydrophytic vegetation present? <u>Yes</u>
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
=Total Cover _____					

Remarks: Row crop agricultural field. Without this land use, hydrophytic vegetation would develop.

Exhibit 3



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EXHIBIT G:
WETLAND DETERMINATION DATA FORM

Sample Point: W1-A

(Midwest Region)

SOILS

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type*	Loc**		
0-21	10YR 2/1	100					Clay Loam	Very dark

*Type: C = Concentration, D = Depletion, RM = Reduced Matrix, MS = Masked Sand Grains. **Location: PL = Pore Lining, M = Matrix

Hydric Soil Indicators:		Indicators for Problematic Hydric Soils*:	
<input type="checkbox"/> Histisol (A1)	<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> Coast Prairie Redox (A16)(LRR K,L,R)	
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Dark Surface (S7)(LRR K, L)	
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Iron-Manganese Masses (F12)(LRR K, L, R)	
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Mucky Material (F1)	<input type="checkbox"/> Very Shallow Dark Surface (TF12)	
<input type="checkbox"/> Stratified Layers (A5)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	<input type="checkbox"/> Other (Explain in remarks)	
<input type="checkbox"/> 2 cm Muck (A10)	<input type="checkbox"/> Depleted Matrix (F3)		
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Redox Dark Surface (F6)		
<input checked="" type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Depleted Dark Surface (F7)		
<input type="checkbox"/> Sandy Mucky Material (S1)	<input type="checkbox"/> Redox Depressions (F8)		
<input type="checkbox"/> 5 cm Mucky Peat or Peat (S3)			

*Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic

Restrictive Layer (if observed):		Hydric Soils Present? <u>Yes</u>	
Type: _____			
Depth (inches): _____			

Remarks: _____

HYDROLOGY

Wetland Hydrology Indicators:		
Primary Indicators (minimum of one is required; check all that apply)		Secondary Indicators (minimum of two required)
<input type="checkbox"/> Surface Water (A1)	<input type="checkbox"/> Water-Stained Leaves (B9)	<input type="checkbox"/> Surface Soil Crack (B6)
<input type="checkbox"/> High Water Table (A2)	<input type="checkbox"/> Aquatic Fauna (B13)	<input type="checkbox"/> Drainage Patterns (B10)
<input type="checkbox"/> Saturation (A3)	<input type="checkbox"/> True Aquatic Plants (B14)	<input type="checkbox"/> Dry-Season Water Table (C2)
<input type="checkbox"/> Water Marks (B1)	<input type="checkbox"/> Hydrogen Sulfide Odor (C1)	<input type="checkbox"/> Crayfish Burrows (C8)
<input type="checkbox"/> Sediment Deposits (B2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)	<input checked="" type="checkbox"/> Saturation Visible on Aerial Imagery (C9)
<input type="checkbox"/> Drift Deposits (B3)	<input type="checkbox"/> Presence or Reduced Iron (C4)	<input type="checkbox"/> Stunted or Stressed Plants (D1)
<input type="checkbox"/> Algal Mat or Crust (B4)	<input type="checkbox"/> Recent Iron Reduction in Tilled Soils (C6)	<input checked="" type="checkbox"/> Geomorphic Position (D2)
<input type="checkbox"/> Iron Deposits (B5)	<input type="checkbox"/> Thin Muck Surface (C7)	<input type="checkbox"/> FAC-Neutral Test (D5)
<input type="checkbox"/> Inundation Visible on Aerial Imagery (B7)	<input type="checkbox"/> Gauge or Well Data (C7)	
<input type="checkbox"/> Sparsely Vegetated Concave Surface (B8)	<input type="checkbox"/> Other (Explain in Remarks)	

Field Observations:		Indicators of Wetland Hydrology Present? <u>Yes</u>	
Surface Water Present? <u>No</u>	Depth (inches): _____		
Water Table Present? <u>No</u>	Depth (inches): _____		
Saturation Present? <u>Yes</u>	Depth (inches): <u>18</u>		

Remarks: _____

Exhibit 3



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EXHIBIT I: WETLAND DETERMINATION DATA FORM

(Midwest Region)

Project/Site: Worthington Apartments City/County: City of Worthington/Nobles County Sampling Date: 4/16/2025

Applicant/Owner: Kuepers Inc State: Minnesota Sample Point: W1-B

Investigator(s): Addeline Theis Paradis Section, Township, Range: S26, T102, R40

Landforms (hillside, terrace, etc.): Toeslope Local Relief (concave, convex, none): Concave

Slope (%): 3-5% Latitude: _____ Longitude: _____ Datum: _____

Soil Map Unit Name: L83A-Webster Clay Loam NWI Classification: None

Are climatic/hydrologic conditions of the site typical for this time of year? No (If no, explain in remarks)

Are vegetation X, soils _____, or hydrology _____ significantly disturbed? Are normal circumstances present? No

Are vegetation _____, soils _____, or hydrology _____ naturally problematic? (If needed, explain any answers in Remarks)

SUMMARY OF FINDINGS

Hydrophytic vegetation present?	<u>No</u>	Is the sampled area within a wetland? <u>No</u>
Hydric soils present?	<u>Yes</u>	
Wetland hydrology present?	<u>No</u>	

Remarks: Antecedent precipitation is below normal.

VEGETATION - Use scientific names of plants

Tree Stratum	(Plot size: <u>30 feet</u>)	Absolute % Cover	Dominant Species	Indicator Status	Dominance Test Worksheet Number of dominant species that are OBL, FACW, or FAC: _____ (A) Total number of dominant species across all strata: _____ (B) Percent of dominant species that are OBL, FACW or FAC: _____ (A/B)
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	
				=Total Cover	
Sapling/Shrub stratum	(Plot size: <u>15 feet</u>)				Prevalence Index Worksheet Total % cover of: OBL Species: _____ x 1 = _____ FACW Species: _____ x 2 = _____ FAC Species: _____ x 3 = _____ FACU species: _____ x 4 = _____ UPL Species: _____ x 5 = _____ Totals: _____ (A) _____ (B) Prevalence Index (B/A): _____
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	
				=Total Cover	
Herb stratum:	(Plot size: <u>5 feet</u>)				Hydrophytic Vegetation Indicators Rapid test for hydrophytic vegetation Dominance test >50% Prevalence index is ≤3.0* Morphological adaptations* (Provide supporting data in remarks) Problematic hydrophytic vegetation* (Explain in remarks)
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	
6	_____	_____	_____	_____	
7	_____	_____	_____	_____	
8	_____	_____	_____	_____	
9	_____	_____	_____	_____	
10	_____	_____	_____	_____	
				=Total Cover	*Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic
Woody vine stratum:	(Plot size: <u>15 feet</u>)				
1	_____	_____	_____	_____	Hydrophytic vegetation present? <u>No</u>
2	_____	_____	_____	_____	
				=Total Cover	

Remarks: Row crop agricultural field. No volunteers. Edge of field vegetation were upland grasses and forbs.

Exhibit 3



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EXHIBIT G:
WETLAND DETERMINATION DATA FORM

Sample Point: W1-B

(Midwest Region)

SOILS

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type*	Loc**		
0-21	10YR 2/1	100					Clay Loam	Very dark

*Type: C = Concentration, D = Depletion, RM = Reduced Matrix, MS = Masked Sand Grains. **Location: PL = Pore Lining, M = Matrix

Hydric Soil Indicators:		Indicators for Problematic Hydric Soils*:	
<input type="checkbox"/> Histisol (A1)	<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> Coast Prairie Redox (A16)(LRR K,L,R)	
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Dark Surface (S7)(LRR K, L)	
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Iron-Manganese Masses (F12)(LRR K, L, R)	
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Mucky Material (F1)	<input type="checkbox"/> Very Shallow Dark Surface (TF12)	
<input type="checkbox"/> Stratified Layers (A5)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	<input type="checkbox"/> Other (Explain in remarks)	
<input type="checkbox"/> 2 cm Muck (A10)	<input type="checkbox"/> Depleted Matrix (F3)		
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Redox Dark Surface (F6)		
<input checked="" type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Depleted Dark Surface (F7)		
<input type="checkbox"/> Sandy Mucky Material (S1)	<input type="checkbox"/> Redox Depressions (F8)		
<input type="checkbox"/> 5 cm Mucky Peat or Peat (S3)			

*Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic

Restrictive Layer (if observed):		Hydric Soils Present? <u>Yes</u>	
Type: _____			
Depth (inches): _____			

Remarks: _____

HYDROLOGY

Wetland Hydrology Indicators:		
Primary Indicators (minimum of one is required; check all that apply)		Secondary Indicators (minimum of two required)
<input type="checkbox"/> Surface Water (A1)	<input type="checkbox"/> Water-Stained Leaves (B9)	<input type="checkbox"/> Surface Soil Crack (B6)
<input type="checkbox"/> High Water Table (A2)	<input type="checkbox"/> Aquatic Fauna (B13)	<input type="checkbox"/> Drainage Patterns (B10)
<input type="checkbox"/> Saturation (A3)	<input type="checkbox"/> True Aquatic Plants (B14)	<input type="checkbox"/> Dry-Season Water Table (C2)
<input type="checkbox"/> Water Marks (B1)	<input type="checkbox"/> Hydrogen Sulfide Odor (C1)	<input type="checkbox"/> Crayfish Burrows (C8)
<input type="checkbox"/> Sediment Deposits (B2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)	<input type="checkbox"/> Saturation Visible on Aerial Imagery (C9)
<input type="checkbox"/> Drift Deposits (B3)	<input type="checkbox"/> Presence or Reduced Iron (C4)	<input type="checkbox"/> Stunted or Stressed Plants (D1)
<input type="checkbox"/> Algal Mat or Crust (B4)	<input type="checkbox"/> Recent Iron Reduction in Tilled Soils (C6)	<input type="checkbox"/> Geomorphic Position (D2)
<input type="checkbox"/> Iron Deposits (B5)	<input type="checkbox"/> Thin Muck Surface (C7)	<input type="checkbox"/> FAC-Neutral Test (D5)
<input type="checkbox"/> Inundation Visible on Aerial Imagery (B7)	<input type="checkbox"/> Gauge or Well Data (C7)	
<input type="checkbox"/> Sparsely Vegetated Concave Surface (B8)	<input type="checkbox"/> Other (Explain in Remarks)	

Field Observations:		Indicators of Wetland Hydrology Present? <u>No</u>	
Surface Water Present? <u>No</u>	Depth (inches): _____		
Water Table Present? <u>No</u>	Depth (inches): _____		
Saturation Present? <u>No</u>	Depth (inches): _____		

Remarks: _____



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EXHIBIT I: WETLAND DETERMINATION DATA FORM

(Midwest Region)

Project/Site: Worthington Apartments City/County: City of Worthington/Nobles County Sampling Date: 4/16/2025
Applicant/Owner: Kuepers Inc State: Minnesota Sample Point: W2-A
Investigator(s): Addeline Theis Paradis Section, Township, Range: S26, T102, R40
Landforms (hillside, terrace, etc.): Depression Local Relief (concave, convex, none): Concave
Slope (%): 0-2% Latitude: _____ Longitude: _____ Datum: _____
Soil Map Unit Name: L83A-Webster Clay Loam NWI Classification: None
Are climatic/hydrologic conditions of the site typical for this time of year? No (If no, explain in remarks)
Are vegetation X, soils _____, or hydrology _____ significantly disturbed? Are normal circumstances present? No
Are vegetation _____, soils _____, or hydrology _____ naturally problematic? (If needed, explain any answers in Remarks)

SUMMARY OF FINDINGS

Hydrophytic vegetation present?	<u>Yes</u>	Is the sampled area within a wetland? <u>Yes</u>
Hydric soils present?	<u>Yes</u>	
Wetland hydrology present?	<u>Yes</u>	

Remarks: Antecedent precipitation is below normal.

VEGETATION - Use scientific names of plants

Tree Stratum	(Plot size: <u>30 feet</u>)	Absolute % Cover	Dominant Species	Indicator Status	Dominance Test Worksheet Number of dominant species that are OBL, FACW, or FAC: _____ (A) Total number of dominant species across all strata: _____ (B) Percent of dominant species that are OBL, FACW or FAC: _____ (A/B)
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	=Total Cover _____
Sapling/Shrub stratum	(Plot size: <u>15 feet</u>)				Prevalence Index Worksheet Total % cover of: OBL Species: _____ x 1 = _____ FACW Species: _____ x 2 = _____ FAC Species: _____ x 3 = _____ FACU species: _____ x 4 = _____ UPL Species: _____ x 5 = _____ Totals: _____ (A) _____ (B) Prevalence Index (B/A): _____
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	=Total Cover _____
Herb stratum:	(Plot size: <u>5 feet</u>)				Hydrophytic Vegetation Indicators Rapid test for hydrophytic vegetation Dominance test >50% Prevalence index is ≤3.0* Morphological adaptations* (Provide supporting data in remarks) Problematic hydrophytic vegetation* <u>X</u> (Explain in remarks)
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	
6	_____	_____	_____	_____	
7	_____	_____	_____	_____	
8	_____	_____	_____	_____	
9	_____	_____	_____	_____	
10	_____	_____	_____	_____	
					*Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic
=Total Cover _____					
Woody vine stratum:	(Plot size: <u>15 feet</u>)				Hydrophytic vegetation present? <u>Yes</u>
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
=Total Cover _____					

Remarks: Row crop agricultural field. Without this land use, hydrophytic vegetation would develop.



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EXHIBIT G:
WETLAND DETERMINATION DATA FORM

Sample Point: W2-A

(Midwest Region)

SOILS

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)

Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type*	Loc**		
0-21	10YR 2/1	100					Clay Loam	Very dark

*Type: C = Concentration, D = Depletion, RM = Reduced Matrix, MS = Masked Sand Grains. **Location: PL = Pore Lining, M = Matrix

Hydric Soil Indicators:

<input type="checkbox"/> Histisol (A1)	<input type="checkbox"/> Sandy Gleyed Matrix (S4)
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Sandy Redox (S5)
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Stripped Matrix (S6)
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Mucky Material (F1)
<input type="checkbox"/> Stratified Layers (A5)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)
<input type="checkbox"/> 2 cm Muck (A10)	<input type="checkbox"/> Depleted Matrix (F3)
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Redox Dark Surface (F6)
<input checked="" type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Depleted Dark Surface (F7)
<input type="checkbox"/> Sandy Mucky Material (S1)	<input type="checkbox"/> Redox Depressions (F8)
<input type="checkbox"/> 5 cm Mucky Peat or Peat (S3)	

Indicators for Problematic Hydric Soils*:

<input type="checkbox"/> Coast Prairie Redox (A16)(LRR K,L,R)
<input type="checkbox"/> Dark Surface (S7)(LRR K, L)
<input type="checkbox"/> Iron-Manganese Masses (F12)(LRR K, L, R)
<input type="checkbox"/> Very Shallow Dark Surface (TF12)
<input type="checkbox"/> Other (Explain in remarks)

*Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic

Restrictive Layer (if observed):

Type: _____
Depth (inches): _____

Hydric Soils Present? Yes

Remarks:

HYDROLOGY

Wetland Hydrology Indicators:

Primary Indicators (minimum of one is required; check all that apply)

<input type="checkbox"/> Surface Water (A1)	<input type="checkbox"/> Water-Stained Leaves (B9)
<input type="checkbox"/> High Water Table (A2)	<input type="checkbox"/> Aquatic Fauna (B13)
<input type="checkbox"/> Saturation (A3)	<input type="checkbox"/> True Aquatic Plants (B14)
<input type="checkbox"/> Water Marks (B1)	<input type="checkbox"/> Hydrogen Sulfide Odor (C1)
<input type="checkbox"/> Sediment Deposits (B2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)
<input type="checkbox"/> Drift Deposits (B3)	<input type="checkbox"/> Presence or Reduced Iron (C4)
<input type="checkbox"/> Algal Mat or Crust (B4)	<input type="checkbox"/> Recent Iron Reduction in Tilled Soils (C6)
<input type="checkbox"/> Iron Deposits (B5)	<input type="checkbox"/> Thin Muck Surface (C7)
<input type="checkbox"/> Inundation Visible on Aerial Imagery (B7)	<input type="checkbox"/> Gauge or Well Data (C7)
<input type="checkbox"/> Sparsely Vegetated Concave Surface (B8)	<input type="checkbox"/> Other (Explain in Remarks)

Secondary Indicators (minimum of two required)

<input type="checkbox"/> Surface Soil Crack (B6)
<input type="checkbox"/> Drainage Patterns (B10)
<input type="checkbox"/> Dry-Season Water Table (C2)
<input type="checkbox"/> Crayfish Burrows (C8)
<input checked="" type="checkbox"/> Saturation Visible on Aerial Imagery (C9)
<input type="checkbox"/> Stunted or Stressed Plants (D1)
<input checked="" type="checkbox"/> Geomorphic Position (D2)
<input type="checkbox"/> FAC-Neutral Test (D5)

Field Observations:

Surface Water Present?	<u>No</u>	Depth (inches):	_____
Water Table Present?	<u>No</u>	Depth (inches):	_____
Saturation Present?	<u>Yes</u>	Depth (inches):	<u>16</u>

Indicators of Wetland Hydrology Present? Yes

Remarks:



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EXHIBIT I: WETLAND DETERMINATION DATA FORM

(Midwest Region)

Project/Site: Worthington Apartments City/County: City of Worthington/Nobles County Sampling Date: 4/16/2025
Applicant/Owner: Kuepers Inc State: Minnesota Sample Point: W2-B
Investigator(s): Addeline Theis Paradis Section, Township, Range: S26, T102, R40
Landforms (hillside, terrace, etc.): Toeslope Local Relief (concave, convex, none): Concave
Slope (%): 3-5% Latitude: _____ Longitude: _____ Datum: _____
Soil Map Unit Name: L83A-Webster Clay Loam NWI Classification: None

Are climatic/hydrologic conditions of the site typical for this time of year? No (If no, explain in remarks)
Are vegetation X, soils _____, or hydrology _____ significantly disturbed? Are normal circumstances present? No
Are vegetation _____, soils _____, or hydrology _____ naturally problematic? (If needed, explain any answers in Remarks)

SUMMARY OF FINDINGS

Hydrophytic vegetation present?	<u>No</u>	Is the sampled area within a wetland? <u>No</u>
Hydric soils present?	<u>Yes</u>	
Wetland hydrology present?	<u>No</u>	
Remarks: <u>Antecedent precipitation is below normal.</u>		

VEGETATION - Use scientific names of plants

Tree Stratum	(Plot size: <u>30 feet</u>)	Absolute % Cover	Dominant Species	Indicator Status	Dominance Test Worksheet Number of dominant species that are OBL, FACW, or FAC: _____ (A) Total number of dominant species across all strata: _____ (B) Percent of dominant species that are OBL, FACW or FAC: _____ (A/B)
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	Prevalence Index Worksheet Total % cover of: OBL Species: _____ x 1 = _____ FACW Species: _____ x 2 = _____ FAC Species: _____ x 3 = _____ FACU species: _____ x 4 = _____ UPL Species: _____ x 5 = _____ Totals: _____ (A) _____ (B) Prevalence Index (B/A): _____
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	Hydrophytic Vegetation Indicators _____ Rapid test for hydrophytic vegetation _____ Dominance test >50% _____ Prevalence index is ≤3.0* _____ Morphological adaptations* (Provide supporting data in remarks) _____ Problematic hydrophytic vegetation* (Explain in remarks) _____ *Indicators of hydric soil and wetland hydrology must be present, unless disturbed or problematic
=Total Cover					
Sapling/Shrub stratum	(Plot size: <u>15 feet</u>)				Hydrophytic vegetation present? <u>No</u>
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	
=Total Cover					
Herb stratum:	(Plot size: <u>5 feet</u>)				
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
3	_____	_____	_____	_____	
4	_____	_____	_____	_____	
5	_____	_____	_____	_____	
6	_____	_____	_____	_____	
7	_____	_____	_____	_____	
8	_____	_____	_____	_____	
9	_____	_____	_____	_____	
10	_____	_____	_____	_____	
=Total Cover					
Woody vine stratum:	(Plot size: <u>15 feet</u>)				
1	_____	_____	_____	_____	
2	_____	_____	_____	_____	
=Total Cover					

Remarks: Row crop agricultural field. No volunteers. Edge of field vegetation were upland grasses and forbs.



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EXHIBIT G:
WETLAND DETERMINATION DATA FORM

Sample Point: W2-B

(Midwest Region)

SOILS

Profile Description: (Describe to the depth needed to document the indicator or confirm the absence of indicators.)								
Depth (inches)	Matrix		Redox Features				Texture	Remarks
	Color (moist)	%	Color (moist)	%	Type*	Loc**		
0-21	10YR 2/1	100					Clay Loam	Very dark

*Type: C = Concentration, D = Depletion, RM = Reduced Matrix, MS = Masked Sand Grains. **Location: PL = Pore Lining, M = Matrix

Hydric Soil Indicators:		Indicators for Problematic Hydric Soils*:	
<input type="checkbox"/> Histisol (A1)	<input type="checkbox"/> Sandy Gleyed Matrix (S4)	<input type="checkbox"/> Coast Prairie Redox (A16)(LRR K,L,R)	
<input type="checkbox"/> Histic Epipedon (A2)	<input type="checkbox"/> Sandy Redox (S5)	<input type="checkbox"/> Dark Surface (S7)(LRR K, L)	
<input type="checkbox"/> Black Histic (A3)	<input type="checkbox"/> Stripped Matrix (S6)	<input type="checkbox"/> Iron-Manganese Masses (F12)(LRR K, L, R)	
<input type="checkbox"/> Hydrogen Sulfide (A4)	<input type="checkbox"/> Loamy Mucky Material (F1)	<input type="checkbox"/> Very Shallow Dark Surface (TF12)	
<input type="checkbox"/> Stratified Layers (A5)	<input type="checkbox"/> Loamy Gleyed Matrix (F2)	<input type="checkbox"/> Other (Explain in remarks)	
<input type="checkbox"/> 2 cm Muck (A10)	<input type="checkbox"/> Depleted Matrix (F3)		
<input type="checkbox"/> Depleted Below Dark Surface (A11)	<input type="checkbox"/> Redox Dark Surface (F6)		
<input checked="" type="checkbox"/> Thick Dark Surface (A12)	<input type="checkbox"/> Depleted Dark Surface (F7)		
<input type="checkbox"/> Sandy Mucky Material (S1)	<input type="checkbox"/> Redox Depressions (F8)		
<input type="checkbox"/> 5 cm Mucky Peat or Peat (S3)			

*Indicators of hydrophytic vegetation and wetland hydrology must be present, unless disturbed or problematic

Restrictive Layer (if observed):		Hydric Soils Present? <u>Yes</u>	
Type: _____			
Depth (inches): _____			

Remarks: _____

HYDROLOGY

Wetland Hydrology Indicators:	
Primary Indicators (minimum of one is required; check all that apply)	
<input type="checkbox"/> Surface Water (A1)	<input type="checkbox"/> Water-Stained Leaves (B9)
<input type="checkbox"/> High Water Table (A2)	<input type="checkbox"/> Aquatic Fauna (B13)
<input type="checkbox"/> Saturation (A3)	<input type="checkbox"/> True Aquatic Plants (B14)
<input type="checkbox"/> Water Marks (B1)	<input type="checkbox"/> Hydrogen Sulfide Odor (C1)
<input type="checkbox"/> Sediment Deposits (B2)	<input type="checkbox"/> Oxidized Rhizospheres on Living Roots (C3)
<input type="checkbox"/> Drift Deposits (B3)	<input type="checkbox"/> Presence or Reduced Iron (C4)
<input type="checkbox"/> Algal Mat or Crust (B4)	<input type="checkbox"/> Recent Iron Reduction in Tilled Soils (C6)
<input type="checkbox"/> Iron Deposits (B5)	<input type="checkbox"/> Thin Muck Surface (C7)
<input type="checkbox"/> Inundation Visible on Aerial Imagery (B7)	<input type="checkbox"/> Gauge or Well Data (C7)
<input type="checkbox"/> Sparsely Vegetated Concave Surface (B8)	<input type="checkbox"/> Other (Explain in Remarks)

Secondary Indicators (minimum of two required)	
<input type="checkbox"/> Surface Soil Crack (B6)	
<input type="checkbox"/> Drainage Patterns (B10)	
<input type="checkbox"/> Dry-Season Water Table (C2)	
<input type="checkbox"/> Crayfish Burrows (C8)	
<input type="checkbox"/> Saturation Visible on Aerial Imagery (C9)	
<input type="checkbox"/> Stunted or Stressed Plants (D1)	
<input type="checkbox"/> Geomorphic Position (D2)	
<input type="checkbox"/> FAC-Neutral Test (D5)	

Field Observations:		Indicators of Wetland Hydrology Present? <u>No</u>	
Surface Water Present? <u>No</u>	Depth (inches): _____		
Water Table Present? <u>No</u>	Depth (inches): _____		
Saturation Present? <u>No</u>	Depth (inches): _____		

Remarks: _____

ENGINEERING MEMO**DATE: MAY 22, 2025****TO: MAYOR AND CITY COUNCIL****SUBJECT: ITEMS REQUIRING COUNCIL ACTION OR REVIEW****CASE ITEMS****1. RESOLUTION PERTAINING TO INTENT TO BOND FOR COST INCURRED IN MAKING PUBLIC IMPROVEMENTS**

To comply with federal tax regulations and Minnesota Statutes Section 429, the City must adopt a resolution stating its intent to reimburse itself with bond proceeds for project costs incurred before bonds are issued.

It is common for the City to cover early project expenses using internal funds and issue bonds later when project costs justify it.

Staff recommends that the City Council approve the reimbursement resolutions shown in **Exhibit 1** for the East Flower Lane Improvement Project. The amounts listed in each resolution are approximately 10% higher than the current project estimates to allow for cost adjustments.

2. APPROVE AT GRANT AGREEMENT AND SUPPORTING DOCUMENTS FOR EAST OKABENA LAKE TRAIL PROJECT

The City of Worthington has been awarded \$1million in Active Transportation (AT) funding from the State of Minnesota for the East Okabena Lake Trail Project.

Included in **Exhibit 2** is the AT Grant Agreement along with the following supporting documents:

- **Exhibit 2A** - Completed Sources and Uses of Funds Schedule
- **Exhibit 2B** - Grant Application
- **Exhibit 2C** - Grantee Resolution Approving Grant Agreement

Staff recommends that the Council approve the AT Grant Agreement in **Exhibit 2**, along with resolution in **Exhibit 2C**.

3. **APPROVE PLANS FOR EAST FLOWER LANE STREET AND UTILITY IMPROVEMENTS AND AUTHORIZE ADVERTISEMENT FOR BIDS**

City staff has prepared the plans and specifications for the East Flower Lane Street and Utility Improvement Project. The project is scheduled for underground utility construction and first lift of pavement during the 2025 season, and final layer of the pavement in the Spring of 2026. Staff is requesting that the City Council approve the plans and specifications and authorize the advertisement for bids.

The plans and specifications are attached as **Exhibit 3A** for Council review.

Bids will be received until 2:00 p.m. on June 18, 2025, and will be brought forward for consideration of award at the June 23, 2025, City Council meeting.

Staff recommends Council pass the resolution Approving Plan and Authorize Advertisement for Bids in **Exhibit 3B**.

4. **AMENDED LICENSE AGREEMENT WITH MINNESOTA ENERGY RESOURCES CORPORATION (MERC) FOR EAST OKABENA TRAIL**

At the Special Council Meeting held on April 16, 2025, the Council approved a License Agreement with Minnesota Energy Resources Corporation (MERC) for the development and use of East Okabena Trail on a portion of MERC's property.

Since that time, MERC has requested amendments to the original agreement regarding the term and termination notice. The amended agreement reflects the following revised terms:

Key Revisions to the Agreement:

- Term: The license term has been revised from 25 years to an initial term of 20 years, continuing year-to-year thereafter.
- Termination: Either party may terminate the agreement with 180 days' written notice, rather than the original 90-day notice..

All other terms of the agreement remain unchanged, including the City's rights to construct and maintain a public recreational trail for non-motorized use, the requirement to begin construction within one year of the effective date, and the City's responsibility for ongoing maintenance and safety.

Staff recommends that Council approve the amended License Agreement with Minnesota Energy Resources Corporation reflecting the revised term and termination provisions for the East Okabena Trail.

5. **CONSIDERATION OF ENTERING INTO NEGOTIATION TO ACQUIRE A**

SECTION OF PARCEL 31-3497-250

As part of the Service Drive/Service Road Utility and Street Improvement Project, staff has identified the need to acquire a portion of parcel 31-3497-250, owned by Roger and Ann Baschke at 2221 Oxford Street. Acquisition of this land is necessary to accommodate proper roadway alignment and support ongoing infrastructure improvements in the area.

This section will also serve as a vital connection between Service Drive and the Service Road, and is being planned as a potential detour route for the 2026/2027 Trunk Highway 59/60 turnback project. Securing this right-of-way is essential to ensure uninterrupted traffic flow during future state highway construction.

An appraisal completed in April determined the value of the land to be \$50,000. A purchase agreement reflecting this valuation has been signed by the property owners and is included as **Exhibit 5**. Staff is requesting council approval to proceed with the acquisition as outlined in the agreement.

Council is requested to approve the purchase agreement and authorize the acquisition of the necessary right-of-way.

RESOLUTION NO. 2025-05-_____

**RELATING TO FINANCING OF CERTAIN PUBLIC IMPROVEMENTS
BY THE CITY OF WORTHINGTON;
ESTABLISHING COMPLIANCE WITH REIMBURSEMENT BOND REGULATIONS UNDER
THE INTERNAL REVENUE CODE**

WHEREAS, The Internal Revenue Service has issued Section 1.150-2 of the Income Tax Regulations (the "Regulations") dealing with the issuance of bonds, all, or a portion of the proceeds of which are to be used to reimburse the City for project expenditures made by the City prior to the date of issuance of the bonds.

WHEREAS, The Regulations generally require that the City, as issuer of the bonds, make a declaration of its official intent to reimburse itself for such prior expenditures out of the proceeds of a subsequently issued series of bonds within 60 days after payment of the expenditures, that the bonds be issued and the reimbursement allocation be made from the proceeds of such bonds within the reimbursement period (as defined in the Regulations), and that the expenditures reimbursed be capital expenditures or costs of issuance of the bonds.

WHEREAS, The City desires to comply with requirements of the Regulations with respect to certain public improvement projects to be undertaken within the City.

NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL (THE "COUNCIL") OF THE CITY OF WORTHINGTON (THE "CITY"), MINNESOTA, AS FOLLOWS:

1. The City expects to make original expenditures with respect to the following described projects prior to the issuance of reimbursement bonds, and the City reasonably expects to issue reimbursement bonds for such projects in the maximum principal amount shown below:

Project:

**EAST FLOWER LANE
EXTENSION, Starting at the East
Gateway Drive just North of
Casey's and extending
approximately 800' West;**

Maximum amount of Bonds
expected to be issued:

\$2,655,000.00

2. Other than (i) de minimis amounts permitted to be reimbursed pursuant to Section 1.150-2(f)(1) of the Regulations or (ii) expenditures constituting preliminary expenditures as defined in Section 1.150-2(f)(2) of the Regulations, the City will not seek reimbursement for any original expenditures with respect to the foregoing projects paid more than 60 days prior to the date of adoption of this resolution. All original expenditures for which reimbursement is sought will be capital expenditures or costs of issuance of the reimbursement bonds.

3. As of the date hereof, there are no City funds reserved, pledged, allocated on a long-term basis, or otherwise set aside (or reasonably expected to be reserved, pledged, allocated on a long-term basis or otherwise set aside) to provide permanent financing for the original expenditures related to the projects, other than pursuant to the issuance of the reimbursement bonds. Consequently, it is not expected that the issuance of the reimbursement bonds will result in the creation of any replacement proceeds.

4. The City's financial officer shall be responsible for making the "reimbursement allocations" described in the Regulations, being generally the transfer of the appropriate amount of proceeds of the reimbursement bonds to reimburse the source of temporary financing used by the City to make payment of the original expenditures relating to the projects. Each reimbursement allocation shall be made within 30 days of the date of issuance of the reimbursement bonds, shall be evidenced by an entry on the official books and records of the City maintained for the reimbursement bonds and shall specifically identify the original expenditures being reimbursed.

Adopted by the city council of the city of Worthington, Minnesota, this the 27th day of May, 2025.

(SEAL)

Rick Von Holdt, Mayor

Attest: _____
Mindy L. Eggers, City Clerk

**STATE OF MINNESOTA
ACTIVE TRANSPORTATION PROGRAM
GRANT AGREEMENT**

This agreement is between the State of Minnesota, acting through its Commissioner of Transportation ("State"), and ("Grantee"):

Public Entity (Grantee) name, address and contact person:

City of Worthington

303 Ninth Street

Worthington, MN 56187

Contact: Hyunmyeong Goo

hgoo@ci.worthington.mn.us

RECITALS

1. Minnesota Statute § 174.38 authorizes the State to enter into this agreement.
2. General Funds were appropriated for the Active Transportation Program in Minnesota Laws 2024, Chapter 127- H.F. 5247.
3. Grantee has been awarded Active Transportation (AT) Program funds under Minn. Stat. § 174.38.
4. Grantee represents that it is duly qualified and agrees to perform all services described in this agreement to the satisfaction of the State. Pursuant to [Minn.Stat. §16B.98](#), Subd.1, Grantee agrees to minimize administrative costs as a condition of this agreement.

AGREEMENT TERMS

1 Term of Agreement, Survival of Terms, and Incorporation of Exhibits

- 1.1 **Effective Date.** This agreement will be effective on the date the State obtains all required signatures under [Minn. Stat. §16B.98](#), Subd. 5. As required by [Minn.Stat. §16B.98](#) Subd. 7, no payments will be made to Grantee until this agreement is fully executed. Grantee must not begin work under this agreement until this agreement is fully executed and Grantee has been notified by the State's Authorized Representative to begin the work.
- 1.2 **Expiration Date.** This agreement will expire on September 30, 2029, or when all obligations have been satisfactorily fulfilled, whichever occurs first.
- 1.3 **Survival of Terms.** All clauses which impose obligations continuing in their nature and which must survive in order to give effect to their meaning will survive the expiration or termination of this agreement, including, without limitation, the following clauses: 8. Liability; 9. State Audits; 10. Government Data Practices and Intellectual Property; 11. Workers Compensation; 12. Governing Law, Jurisdiction, and Venue; and 14. Data Disclosure.
- 1.4 **Exhibits.** Exhibit A: Sources and Uses of Funds Schedule; Exhibit B: Grant Application; Exhibit C: Grantee Resolution Approving Grant Agreement are attached and incorporated into this agreement.

2 Grantee's Duties

- 2.1 Grantee will conduct activities in accordance with its grant application, or in the case of legislatively selected projects, in accordance with the enabling session law, which is attached to this Agreement as Exhibit B.
- 2.2 Grantee will comply with all required grants management policies and procedures set forth through [Minn.Stat. §16B.97](#), Subd. 4 (a) (1).
- 2.3 **Asset Monitoring.** If Grantee uses funds obtained by this agreement to acquire a capital asset, the Grantee is required to use that asset for a public purpose for the normal useful life of the asset. Grantee may not sell or change the purpose of use for the capital asset(s) obtained with grant funds under this agreement without the prior written consent of the State and an agreement executed and approved by the same parties who executed and approved this agreement, or their successors in office.

3 Time

- 3.1 Grantee must comply with all the time requirements described in this agreement. In the performance of this grant agreement, time is of the essence.

4 Consideration and Payment

4.1 Consideration. The State will pay for all services performed by Grantee under this agreement as follows:

- 4.1.1 Compensation.** Grantee will be reimbursed for actual, incurred costs that are eligible under Minn. Stat. § 174.38. Grantee shall use this grant solely to reimburse itself for expenditures it has already made to pay for the costs of one or more of the activities listed under section 2.1.
- 4.1.2 Sources and Uses of Funds.** Grantee represents to State that the Sources and Uses of Funds Schedule attached as Exhibit A accurately shows the total cost of the project and all of the funds that are available for the completion of the project. Grantee agrees that it will pay for any costs that are ineligible for reimbursement and for any amount by which the costs exceed State's total obligation in section 4.1.3. Grantee will return to State any amount appropriated but not required.
- 4.1.3 Total Obligation.** The total obligation of the State for all compensation and reimbursements to Grantee under this agreement will not exceed \$1,000,000.00.

4.2 Payment

- 4.2.1 Invoices.** Grantee will submit state aid pay requests for reimbursements requested under this grant agreement. The State will promptly pay Grantee after Grantee presents an itemized invoice for the services actually performed and the State's Authorized Representative accepts the invoiced services.
 - 4.2.2 All Invoices Subject to Audit.** All invoices are subject to audit, at State's discretion.
 - 4.2.3 State's Payment Requirements.** State will promptly pay all valid obligations under this agreement as required by Minnesota Statutes §16A.124. State will make undisputed payments no later than 30 days after receiving Grantee's invoices and progress reports for services performed. If an invoice is incorrect, defective or otherwise improper, State will notify Grantee within ten days of discovering the error. After State receives the corrected invoice, State will pay Grantee within 30 days of receipt of such invoice.
 - 4.2.4 Grant Monitoring Visit and Financial Reconciliation.** During the period of performance, the State will make at least annual monitoring visits and conduct annual financial reconciliations of Grantee's expenditures.
 - 4.2.4.1** The State's Authorized Representative will notify Grantee's Authorized Representative where and when any monitoring visit and financial reconciliation will take place, which State employees and/or contractors will participate, and which Grantee staff members should be present. Grantee will be provided with at least seven calendar days of notice prior to any monitoring visit or financial reconciliation.
 - 4.2.4.2** Following a monitoring visit or financial reconciliation, Grantee will take timely and appropriate action on all deficiencies identified by State.
 - 4.2.4.3** At least one monitoring visit and one financial reconciliation must be completed prior to final payment being made to Grantee.
 - 4.2.5 Unexpended Funds.** The Grantee must promptly return to the State at grant closeout any unexpended funds that have not been accounted for in a financial report submitted to the State.
 - 4.2.6 Closeout.** The State will determine, at its sole discretion, whether a closeout audit is required prior to final payment approval. If a closeout audit is required, final payment will be held until the audit has been completed. Monitoring of any capital assets acquired with grant funds will continue following grant closeout.
- 4.3 Contracting and Bidding Requirements.** If Grantee is a municipality as defined by Minn. Stat. § 471.345, subdivision 1, then Grantee shall comply with the requirements of Minn. Stat. § 471.345 for all procurement under this Agreement.

5 Conditions of Payment

All services provided by Grantee under this agreement must be performed to the State's satisfaction, as determined at the sole discretion of the State's Authorized Representative and in accordance with all applicable federal, state, and local laws, ordinances, rules, and regulations. The Grantee will not receive payment for work found by the State to be unsatisfactory or performed in violation of federal, state, or local law.

6 Authorized Representatives

6.1 The State's Authorized Representative is:

Marc Brieese,
Programs Engineer,
MnDOT State Aid Office
395 John Ireland Boulevard, MS 500
St. Paul, MN 55155
Office: 651-366-3802
marc.brieese@state.mn.us

or his/her successor. State's Authorized Representative has the responsibility to monitor Grantee's performance and the authority to accept the services provided under this agreement. If the services are satisfactory, the State's Authorized Representative will certify acceptance on each invoice submitted for payment.

6.2 Grantee's Authorized Representative is:

Hyunmyeong Goo, City of Worthington – City Engineer, 303 Ninth Street, Worthington, Minnesota 56187, (507) 372-8640, hgoo@ci.worthington.mn.us. If Grantee's Authorized Representative changes at any time during this agreement, Grantee will immediately notify the State.

7 Assignment Amendments, Waiver, and Grant Agreement Complete

7.1 Assignment. The Grantee may neither assign nor transfer any rights or obligations under this agreement without the prior written consent of the State and a fully executed Assignment Agreement, executed and approved by the same parties who executed and approved this agreement, or their successors in office.

7.2 Amendments. Any amendments to this agreement must be in writing and will not be effective until it has been executed and approved by the same parties who executed and approved the original agreement, or their successors in office.

7.3 Waiver. If the State fails to enforce any provision of this agreement, that failure does not waive the provision or the State's right to subsequently enforce it.

7.4 Grant Agreement Complete. This grant agreement contains all negotiations and agreements between the State and Grantee. No other understanding regarding this agreement, whether written or oral, may be used to bind either party.

7.5 Electronic Records and Signatures. The parties agree to contract by electronic means. This includes using electronic signatures and converting original documents to electronic records.

7.6 Certification. By signing this Agreement, the Grantee certifies that it is not suspended or debarred from receiving federal or state awards.

8 Liability

Grantee and State agree that each will be responsible for its own acts and the results thereof to the extent authorized by law, and neither shall be responsible for the acts of the other party and the results thereof. The liability of State is governed by the provisions of Minn. Stat. Sec. 3.736. If Grantee is a "municipality" as that term is used in Minn. Stat. Chapter 466, then the liability of Grantee is governed by the provisions of Chapter 466. Grantee's liability hereunder shall not be limited to the extent of insurance carried by or provided by Grantee, or subject to any exclusion from coverage in any insurance policy.

9 State Audits

Under Minn. Stat. § 16B.98, Subd.8, the Grantee's books, records, documents, and accounting procedures and practices of Grantee, or other party relevant to this grant agreement or transaction, are subject to examination by the State and/or the State Auditor or Legislative Auditor, as appropriate, for a minimum of six years from the end of this agreement, receipt and approval of all final reports, or the required period of time to satisfy all state and program retention requirements, whichever is later. Grantee will take timely and appropriate action on all deficiencies identified by an audit.

10 Government Data Practices and Intellectual Property Rights

10.1 Government Data Practices. Grantee and State must comply with the Minnesota Government Data Practices Act, [Minn. Stat. Ch. 13](#), as it applies to all data provided by the State under this grant agreement, and as it applies to all data created, collected, received, stored, used, maintained, or disseminated by the Grantee under this agreement. The civil remedies of [Minn. Stat. §13.08](#) apply to the release of the data referred to in this clause by

either Grantee or the State.

11 Workers Compensation

The Grantee certifies that it is in compliance with [Minn. Stat. §176.181](#), Subd. 2, pertaining to workers' compensation insurance coverage. The Grantee's employees and agents will not be considered State employees. Any claims that may arise under the Minnesota Workers' Compensation Act on behalf of these employees and any claims made by any third party as a consequence of any act or omission on the part of these employees are in no way the State's obligation or responsibility.

12 Governing Law, Jurisdiction, and Venue

Minnesota law, without regard to its choice-of-law provisions, governs this agreement. Venue for all legal proceedings out of this agreement, or its breach, must be in the appropriate state or federal court with competent jurisdiction in Ramsey County, Minnesota.

13 Termination; Suspension

13.1 Termination by the State. The State may terminate this agreement with or without cause, upon 30 days written notice to the Grantee. Upon termination, the Grantee will be entitled to payment, determined on a pro rata basis, for services satisfactorily performed.

13.2 Termination for Cause. The State may immediately terminate this grant agreement if the State finds that there has been a failure to comply with the provisions of this agreement, that reasonable progress has not been made, that fraudulent or wasteful activity has occurred, that Grantee has been convicted of a criminal offense relating to a state grant agreement, or that the purposes for which the funds were granted have not been or will not be fulfilled. The State may take action to protect the interests of the State of Minnesota, including the refusal to disburse additional funds and requiring the return of all or part of the funds already disbursed.

13.3 Termination for Insufficient Funding. The State may immediately terminate this agreement if:

13.3.1 It does not obtain funding from the Minnesota Legislature; or

13.3.2 If funding cannot be continued at a level sufficient to allow for the payment of the services covered here. Termination must be by written or fax notice to the Grantee. The State is not obligated to pay for any services that are provided after notice and effective date of termination. However, the Grantee will be entitled to payment, determined on a pro rata basis, for services satisfactorily performed to the extent that funds are available. The State will not be assessed any penalty if the agreement is terminated because of the decision of the Minnesota Legislature, or other funding source, not to appropriate funds. The State will provide the Grantee notice of the lack of funding within a reasonable time of the State's receiving that notice.

13.4 Suspension. The State may immediately suspend this agreement in the event of a total or partial government shutdown due to the failure to have an approved budget by the legal deadline. Work performed by the Grantee during a period of suspension will be deemed unauthorized and undertaken at risk of non-payment.

14 Data Disclosure

Under [Minn. Stat. § 270C.65](#), Subd. 3, and other applicable law, Grantee consents to disclosure of its social security number, federal employer tax identification number, and/or Minnesota tax identification number, already provided to the State, to federal and state tax agencies and state personnel involved in the payment of state obligations. These identification numbers may be used in the enforcement of federal and state tax laws which could result in action requiring the Grantee to file state tax returns and pay delinquent state tax liabilities, if any.

15 Fund Use Prohibited. The Grantee will not utilize any funds received pursuant to this Agreement to compensate, either directly or indirectly, any contractor, corporation, partnership, or business, however organized, which is disqualified or debarred from entering into or receiving a State contract. This restriction applies regardless of whether the disqualified or debarred party acts in the capacity of a general contractor, a subcontractor, or as an equipment or material supplier. This restriction does not prevent the Grantee from utilizing these funds to pay any party who might be disqualified or debarred after the Grantee's contract award on this Project.

16 Discrimination Prohibited by Minnesota Statutes §181.59. Grantee will comply with the provisions of Minnesota Statutes §181.59 which requires that every contract for or on behalf of the State of Minnesota, or any county, city, town,

township, school, school district or any other district in the state, for materials, supplies or construction will contain provisions by which Contractor agrees: 1) That, in the hiring of common or skilled labor for the performance of any work under any contract, or any subcontract, no Contractor, material supplier or vendor, will, by reason of race, creed or color, discriminate against the person or persons who are citizens of the United States or resident aliens who are qualified and available to perform the work to which the employment relates; 2) That no Contractor, material supplier, or vendor, will, in any manner, discriminate against, or intimidate, or prevent the employment of any person or persons identified in clause 1 of this section, or on being hired, prevent or conspire to prevent, the person or persons from the performance of work under any contract on account of race, creed or color; 3) That a violation of this section is a misdemeanor; and 4) That this contract may be canceled or terminated by the state of Minnesota, or any county, city, town, township, school, school district or any other person authorized to grant contracts for employment, and all money due, or to become due under the contract, may be forfeited for a second or any subsequent violation of the terms or conditions of this Agreement.

17 Limitation. Under this Agreement, the State is only responsible for receiving and disbursing funds. Nothing in this Agreement will be construed to make the State a principal, co-principal, partner, or joint venturer with respect to the Project(s) covered herein. The State may provide technical advice and assistance as requested by the Grantee, however, the Grantee will remain responsible for providing direction to its contractors and consultants and for administering its contracts with such entities. The Grantee's consultants and contractors are not intended to be third party beneficiaries of this Agreement.

18 Additional Provisions

18.1 Prevailing Wages. Grantee agrees to comply with all of the applicable provisions contained in Minnesota Statutes Chapter 177, and specifically those provisions contained in Minn. Stat. §. 177.41 through 177.435 as they may be amended or replaced from time to time with respect to the project. By agreeing to this provision, Grantee is not acknowledging or agreeing that the cited provisions apply to the project.

18.2 E-Verification. Grantee agrees and acknowledges that it is aware of Minn.Stat. § 16C.075 regarding e-verification of employment of all newly hired employees to confirm that such employees are legally entitled to work in the United States, and that it will, if and when applicable, fully comply with such order.

18.3 Telecommunications Certification. If federal funds are included in Exhibit A, by signing this agreement Grantee certifies that, consistent with Section 889 of the John S. McCain National Defense Authorization Act for Fiscal Year 2019, Pub. L. 115-232 (Aug. 13, 2018), Grantee does not and will not use any equipment, system, or service that uses "covered telecommunications equipment or services" (as that term is defined in Section 889 of the Act) as a substantial or essential component of any system or as critical technology as part of any system. Grantee will include this certification as a flow down clause in any contract related to this agreement.

18.4 Title VI/Non-discrimination Assurances. Grantee agrees to comply with all applicable US DOT Standard Title VI/Non-Discrimination Assurances contained in DOT Order No. 1050.2A, and in particular Appendices A and E, which can be found at: https://edocs-public.dot.state.mn.us/edocs_public/DMResultSet/download?docId=11149035. If federal funds are included in Exhibit A, Grantee will ensure the appendices and solicitation language within the assurances are inserted into contracts as required. State may conduct a review of the Grantee's compliance with this provision. The Grantee must cooperate with State throughout the review process by supplying all requested information and documentation to State, making Grantee staff and officials available for meetings as requested, and correcting any areas of non-compliance as determined by State.

18.5 Use, Maintenance, Repair and Alterations. The Grantee shall not, without the written consent of the State and the Commissioner, (i) permit or allow the use of any of the Property improved with these grant funds (the Real Property) for any purpose other than in conjunction with or for nonmotorized transportation, (ii) substantially alter any of the Real Property except such alterations as may be required by laws, ordinances or regulations, or such other alterations as may improve the Real Property by increasing its value or which improve its ability to be used for the purposes set forth in section (i), (iii) take any action which would unduly impair or depreciate the value of the Real Property, (iv) abandon

the Real Property, or (v) commit or permit any act to be done in or on the Real Property in violation of any law, ordinance or regulation.

If the Grantee fails to maintain the Real Property in accordance with this Section, the State may perform whatever acts and expend whatever funds necessary to so maintain the Real Property, and the Grantee irrevocably authorizes the State to enter upon the Real Property to perform such acts as may be necessary to so maintain the Real Property. Any actions taken or funds expended by the State shall be at its sole discretion, and nothing contained herein shall require the State to take any action or incur any expense and the State shall not be responsible, or liable to the Grantee or any other entity, for any such acts that are performed in good faith and not in a negligent manner. Any funds expended by the State pursuant to this Section shall be due and payable on demand by the State and will bear interest from the date of payment by the State at a rate equal to the lesser of the maximum interest rate allowed by law or 18% per year based upon a 365-day year.

[The remainder of this page has intentionally been left blank.]

GRANTEE

The Grantee certifies that the appropriate person(s) have executed the grant agreement on behalf of the Grantee as required by applicable articles, bylaws, resolutions, or ordinances.

By: _____

Title: _____

Date: _____

By: _____

Title: _____

Date: _____

By: _____

Title: _____

Date: _____

DEPARTMENT OF TRANSPORTATION

Approval and Certifying Encumbrance as required by Minnesota Statutes § 16A.15 and 16C.05

By: _____

State Aid Programs Manager
(with delegated authority)

Date: _____

**DEPARTMENT OF TRANSPORTATION
CONTRACT MANAGEMENT**

By: _____

Date: _____

EXHIBIT A**SOURCES AND USES OF FUNDS SCHEDULE****SOURCES OF FUNDS**

Entity Supplying Funds	Amount
State Funds:	
AT General Fund Grant	\$1,000,000.00
Other:	
	\$
Subtotal	\$1,000,000.00
Public Entity Funds:	
Local Matching Funds	\$153,000.00
Other:	
TA Grant	\$592,000.00
Local Trail Connections Program - MnDNR	\$250,000.00
Subtotal	\$995,000.00
TOTAL FUNDS	\$1,995,000.00

USES OF FUNDS

Expenses	Amount
Items Paid for with AT General Fund Grant Funds:	
Trail & Bridge	\$1,000,000.00
	\$
Subtotal	\$1,000,000.00
Items paid for with Non-AT General Fund Grant Funds:	
Trail & Bridge	\$153,000.00
Trail & Bridge	\$592,000.00
Trail & Bridge	\$250,000.00
Subtotal	\$995,000.00
TOTAL PROJECT COSTS	\$1,995,000.00

Application Template

2025 MnDOT Active Transportation Infrastructure Program

Applications due 1/17/2025

Applications can be submitted at:

https://mndotforms.formstack.com/forms/at_2024solicitation

I. Project Information

A. Applicant Information

Name: Hyunmyeong Goo

Agency Name: City of Worthington

Job Title/Position: City Engineer

Phone: (507) 372-8640

Email: hgoo@ci.worthington.mn.us

Agency Type: State Aid City

Address: 303 Ninth Street, Worthington, Minnesota 56187

County: Nobles

MnDOT District: 7

B. Lead Agency Sponsor Information

Section only required for townships, small cities, and non-profits.

C. Project Funding

Has this project been selected for federal funding?

☒ Yes

☐ No

Indicate the name of the federal funding program.

Transportation Alternatives Program

Which STIP version is the project included in?

2024-2027 STIP0

What is the Sequence # from the STIP?

SP 177-090-006

How much federal funding is programmed?

\$592,000

Active Transportation Program Request:

\$1,000,000

Federal Funding Amount:

\$592,000

County State Aid Funds:

\$0

Municipal State Aid Funds:

\$0

Local Township funding amount:

\$0

Local City funding amount:

\$153,000

Local County funding amount:

\$0

MnDOT trunk highway funds:

\$0

Tribal Funds:

\$0

Other Funds:

\$250,000 (DNR Local Trail Connections)

Total project funding: (Automatically calculated in online application)

\$1,995,000

Are all funding sources:

☒

Yes

☐

No

D. Project Location

Will any proposed infrastructure improvements be constructed on the right-of-way or property of a township, city, or county other than the applicant or lead agency sponsor?

☒ Yes

☐ No

If yes, list all public entities that are a part of this project. An entity should be listed if it is partnering on this project or if this project will be constructed at any point within its city/township/county limits.

Nobles County

Latitude: 43.615219

Longitude: -95.599039

E. Brief Project Description

Enter a brief description or title of your project. Example: Construct shared use path along north side of CSAH 12 (Cedar Street) including bumpouts at all quadrants at the CSAH 12 and Main Street intersection in the City of Moose River.* (300 characters) (294)

The Project will construct 0.8 miles of ADA-accessible shared-use path beginning at the S Lake Street , cross under the existing railroad and run along Sherwood Street and through Buss Field before aligning with East Lake Boulevard adjacent to the open space wetland area and connecting to the Highway 60 trail at Nobles Street.

F. Eligibility Check

****any no's in this section result in the project being ineligible****

The applicant must have a full resolution (not just a letter of support) from their council or governing board approving the project and pledging support to fund engineering, right of way, inspection, and other non-AT eligible costs, as well as AT-eligible items in excess of the AT Infrastructure grant amount. The applicant understands this eligibility requirement and has executed this document for attachment to the application.

☒ Yes

☐ No

Township, non-state aid city, and registered non-profit applicants will need a state aid city or county to serve as their lead agency project sponsor. If a lead agency sponsor is required, the applicant must have a full resolution (not just a letter of support) from the sponsoring county or state aid city agreeing to serve as a sponsor and to support the project by performing tasks identified above in section "Lead Agency Sponsor". The applicant understands this eligibility requirement and has obtained this document for attachment to the application.

☐ Yes

☐ No

☒ n/a

The applicant must have a full resolution (not just a letter of support) from all non-Tribal entities (except MnDOT) other than the applicant or lead agency sponsor whose property or right-of-way will be impacted by the proposed project. The applicant understands this eligibility requirement and has obtained, if required, this document from all impacted entities for attachment to the application.

☒ Yes

☐ No

☐ Not applicable (no other public agency right of way is impacted)

Does the applicant entity have the ability to maintain the infrastructure improvement and provide an expected service life of a minimum of 10 years? The applicant affirms to the best of their current knowledge and belief that this requirement will be met.*

☒ Yes

☐ No

Projects are required to be ready for construction in 2025 or 2026. The applicant understands this eligibility requirement and will award a contract and be under construction prior to December 31, 2026.

☒ Yes

☐ No

Please select the anticipated construction year.

☒ 2025

☒ 2026

Active Transportation infrastructure grant funds cannot be used on impacts to trunk highways or trunk highway right-of-way without an explicit letter of support from the MnDOT District Engineer. The applicant understands this eligibility requirement and has obtained, if required, this document for attachment to the application.

☐ Yes

☐ No

☒ Not applicable

Only construction costs are eligible for the program. Development of engineering and construction plans are not eligible nor are right-of-way acquisition costs. All awarded projects must follow the State Aid process, which includes developing a construction plan set signed

by a licensed engineer. The applicant must have the ability to develop this plan set or the funds to pay a consultant to develop this plan set. Exhibits from engineering studies do not qualify as a construction plan set. The applicant understands this requirement and has the ability or funds to develop the plan set.

☒ Yes

☐ No

Active Transportation funds cannot be used to pay non-profit, local agency, or federally recognized Indian Tribe staff time to construct or install any improvements. Non-profit, local agency, or federally recognized Indian Tribe staff time is not an eligible cost for the program. All awarded projects must be put out to bid and awarded to a contractor. The applicant understands this program requirement and plans to bid the project out to a contractor.

☒ Yes

☐ No

Has the project received a legislative appropriation (also known as an "earmark")?

☐ Yes

☒ No

Statute 16B.981 Subd. 2 (6) requires that no current principals of a grantee have been convicted of a felony financial crime in the last 10 years. A principal is defined as a public official, a board member, or staff (paid or volunteer) with the authority to access funds provided by this grant opportunity or to determine how those funds are used. Political subdivisions as defined in Statute 465.719 (including school districts) and Federally Recognized Indian Tribes are not subject to this requirement. Checking the following box is acknowledgement that if selected to receive a grant and if required by statute, the applicant will be required to complete a form certifying that no current principal of its organization has been convicted of a felony financial crime in the last 10 years.

☒ Yes

☐ No

G. Project Evaluation

Enter the contact information for the follow up project evaluations. This contact should be a role or title with whom MnDOT staff can contact in the case of applicant staff turnover. The post-construction evaluations will be to gauge effectiveness and use of the eligible infrastructure improvements.

Name: Mindy Eggers

Title/Role: City Clerk

Email Address: meggers@ci.worthington.mn.us

Phone Number: (507) 666-5011

II. Project Improvements & Safety

H. Safety Concerns

At any location in the project area, do pedestrians or bicyclists travel where safe infrastructure is not provided?

☒ Yes

☐ No

If yes, check any that apply below:

☒ Pedestrians walk along the grass or ditch.

☒ People walk or bike within a vehicle travel lane or roadway

☐ People cross a roadway where pedestrian or bicyclist crossing are explicitly prohibited by signage

☒ People cross a roadway at any point other than an intersection or marked crossing

☒ Other: People cross the railroad at unsafe locations.

Click or tap here to enter text.

Have safety risks or hazards related to vehicles been identified within the project area that prevent people from safely walking or biking in or near the project area?

☒ Yes

☐ No

If, yes, Check any that are present in the project area:

☒ High vehicle speeds

☒ High Levels of traffic

☐ Low stop or yield compliance for pedestrians or cyclists

☒ Low visibility of pedestrians or cyclist/ Drivers unable to see pedestrians or cyclists

☐ Other:

Click or tap here to enter text.

Please use this space to more completely list and describe the safety risks, hazards, or uncomfortable walking or biking conditions that have been identified above, including the locations of these risks and conditions. Applicants may also reference any survey data, crash data, pedestrian or bicycle plan, or other relevant sources. Upload any referenced sources

when submitting this application. Each attachment must be referenced in the application, otherwise the attachment will not be considered in the scoring of the application.* (1600 Character limit) (1364)

Currently, pedestrians, bicyclists, and other active transportation system users must share the road with vehicles through the project area. The railroad barrier hinders existing trail users from accessing businesses, amenities, and residential areas in downtown Worthington and the natural recreational areas surrounding Okabena Lake. The proposed trail will eliminate this safety hazard and the railroad barrier while connecting to the existing trail loop. Prairie Elementary School is located near the lakefront trail, and completing the proposed trail segment will allow students to safely reach this school without having to interact with vehicles. The current state of this roadway disproportionately impacts vulnerable road users such as older adults, children, those traveling with children, and people with reduced mobility or disabilities, as it is much more dangerous for them to walk within this area, especially through the undeveloped land near Buss Field. The proposed trail will provide a continuous, separated facility between the project area, existing trails around Okabena Lake, nearby residential areas, and downtown Worthington via the existing sidewalk and trail network. The construction of this trail will significantly improve access and comfort for all non-motorized road users by installing facilities that benefit vulnerable road users.

I. Types of Improvements

Check the boxes below for the AT improvements included in the project

Curbs and Medians:

- ☒ New curb and Gutter
- ☒ Curb Extensions (Concrete)
- ☐ Curb extensions (Paint and bollards)
- ☐ Median (Concrete)
- ☐ Median (Paint and Bollards)

Signage and Striping

- ☒ New Signage (all types)
- ☐ RRFB
- ☐ Pedestrian hybrid beacon
- ☒ New crosswalk striping
- ☐ Bicycle lane striping (all)
- ☐ School zone signage or markings

Sidewalks and trails

- ☐ New sidewalks

- ☒ ADA ramps
- ☒ Trails and Shared use path
- ☐ On-street striped walkway
- ☒ Other: Separated grade railroad crossing

Provide a full project description including the locations and uses of each improvement identified above. Please include descriptions for other improvements not listed above as well. Include any project maps or design exhibits. These exhibits may be uploaded with your application. (800 character limit) (790)

The Project will construct 0.8 miles of ADA-accessible concrete shared-use path beginning at S Lake Street, cross the railroad at an existing abandoned underpass, run along Sherwood Street, through the Buss Field city soccer complex, adjacent to East Lake Boulevard and connecting to Highway 60 at Nobles Street. This proposed trail segment will connect two existing trails. One trail segment is located on the west side of the community and goes around Okabena Lake, connecting residential, educational, and recreational areas. The other trail segment is located on the east side of the community along Trunk Highway 60 and connects residential, business, and industrial areas. The abandoned railroad crossing that divides the community will be upgraded to a separated grade trail crossing.

J. Project Improvements

Destinations: Describe how listed improvements would connect communities or connect key destinations within your community. See Solicitation Guide for example statement.* (800 characters) (771)

Currently, non-motorized transportation system users must share the road with vehicles when walking, rolling, or riding a bicycle through the project area. These grant funds will be used to fill the trail gap between the two existing segments, connecting nearby residents to the existing trail around Okabena Lake as well as to numerous businesses, parks, amenities, and the greater sidewalk network within the City of Worthington. This project will also connect trail users to the nearby Trunk Highway 60 trail system and the surrounding communities, a major transportation artery within the region. Prairie Elementary School is located near the project area, and the completion of this trail will allow students to access the school safely via the completed trail loop.

Safety Risk Mitigation: Explain how each of the listed improvements in "Section I. Types of Improvements" would mitigate the safety risks and hazards described in "Section H. Safety Concerns." See Solicitation Guide for example statement.* (1600 characters) (1552)

Safety for non-vehicular travelers will be improved with the addition of the new shared use path by closing the existing trail gap and ensuring users do not have to share the road with vehicles or cross the railroad at unsafe locations. People walking, rolling, and biking will be separated from high-speed traffic and will have increased visibility and comfort.

All roadway crossings will include the necessary trail crossing signage and painted crosswalks. In addition, the abandoned railroad underpass will be upgraded to a box culvert separated grade trail crossing with lighting and other required railroad crossing safety features.

Within the project area, there is currently no trail infrastructure, making it difficult or almost impossible for people with limited mobility to safely access other trails around Lake Okabena. The addition of ADA ramps will allow people of all mobilities to use the trail system within the City of Worthington and access the many businesses and community locations within the local trail system. A new curb and gutter will ensure that flooding will not jeopardize the utility of the trail in all types of weather.

The trail will connect with existing trails that provide pedestrian access to multiple community and business destinations, including Prairie Elementary School, the Buss Field Soccer Complex, and seven different community parks. Completion of this trail will allow users to access the Trunk Highway 60 trail system, an important connecting roadway in the City of Worthington and within the region.

III. Community Engagement & Transportation Policies

K. Plans, Policies, & Studies

Does the applicant agency or lead agency sponsor have any pedestrian or bicycle plan?

☒ Yes – please attach to application

☐ No

Describe the content and extents of the non-motorized transportation plan(s). Include the year of adoption for each document listed.

The goal of the City of Worthington Active Living Plan, adopted in 2015, is to identify destinations, gaps, areas of concern, and prioritize projects for addressing pedestrian infrastructure in the City.

Has the adopted plan received any updates, addendums, survey, public engagement sessions, or any other changes since it's adoption:

☐ Yes – please attach

☒ No

If yes, please describe the updates: (200 characters)

Click or tap here to enter text.

Are the improvements in this project identified in the listed plan?

☒ Yes

☐ No

If yes, please provide page number of project identification:

Pages 76-77

Explain how the proposed improvements in this project were identified, planned, and prioritized. This includes any community engagement or public outreach activities. (800 Character limit) (670)

The City of Worthington Active Living Planning Process was an efficient planning process that was based on identifying destinations, gaps, areas of concern, and potential projects. Community feedback was solicited online and during two open houses. A community survey was distributed between the first and second community meeting. Residents expressed that there were multiple gaps in the trail network where they felt unsafe sharing the road with vehicles. This project would directly address that concern by improving connectivity for bicyclists and pedestrians by giving users a continuous connection to many different businesses and attractions throughout the city.

Has the applicant entity and/or project sponsor adopted a Complete Streets Policy or other policies or practices encouraging and promoting Complete Streets policies and practices in planning, design and construction? If yes, please describe these policies or practices and attach them to the application.

☒ Yes – please attach

☐ No

The City of Worthington's Complete Streets Policy states that the city will seek to enhance the safety, access, convenience, and comfort of all users of all ages and abilities, including bicyclists, pedestrians (including people requiring mobility aids), motorists, and freight drivers, through the design, operation and maintenance of the transportation network so as to create a connected network of facilities accommodating each mode of travel that is consistent with and supportive of the local community, recognizing that all streets are different and that the needs of various users will need to be balanced in a flexible manner.

IV. Equity Score

L. Advancing Equity

Equity in transportation seeks fairness in mobility and accessibility to meet the needs of all community members. A central goal of transportation equity is to facilitate social and economic opportunities by providing equitable levels of access to affordable and reliable transportation options based on the needs of the populations being served, particularly populations that are traditionally underserved. Please mark which of the following priority populations are present within your project area or directly served by your proposed project. (Check all that apply) *

Priority Populations

☒ Residents of Color

☒ Low Income

☒ Low or No Vehicle Availability

☐ High Density Residential

☒ Aged Under 18

☒ Aged 65+

☒ Limited or Non-English Speaking

☒ Foreign-Born

☒ Renter or Non-Owner

☒ Disabled or Low Mobility

☐ Other

Describe how the ability for priority populations to use active transportation will be advanced with this proposed project. Include specific project area destinations and statistics that support the priority population boxes checked above.* (1600 characters)

Currently, there is no ADA-accessible bicycle or pedestrian infrastructure within the project area, making it difficult and dangerous for wheelchair users. According to American Communities Survey data, fourteen percent of Worthington residents live with a disability. The addition of an ADA-accessible shared-use path will allow wheelchair users to access homes, parks, and other amenities near the project area, including existing trail facilities, safely and efficiently while also providing a reliable connection to other places within the sidewalk and local trail network. This project will also give wheelchair users access to undeveloped areas near Buss Field as well as existing sidewalks and trails near the residential areas along East Lake Boulevard and Highway 60.

This proposed trail will fulfill the program goals by closing a major access gap within the City of Worthington's local trail and sidewalk system at the railroad crossing located on S Lake Street. The closure of this gap will result in a connection to a trail loop around Okabena Lake that also connects to the local sidewalk network as well as to existing bicycle and pedestrian infrastructure along Trunk Highway 60. This trail will provide a direct, off-street connection between destinations in downtown Worthington, nine parks, local businesses, Prairie Elementary School, multiple residential neighborhoods, and various other amenities. This will directly benefit residents with low household income and/or with limited access to personal vehicles by making it easier to access common destinations without a car.

M. Active Transportation Priority Areas for Walking

The Active Transportation Priority Areas for Walking Study (AT-PAWS) was developed as part of the Minnesota Statewide Pedestrian System Plan to support equitable planning and scoring processes for bicycle and pedestrian improvements throughout Minnesota. These scores were then modified to remove data related to the trunk highway system and to be better suited for use with the AT Program. The AT-PAWS scores used in the project evaluation process for this solicitation can be found here, Active Transportation Equity Score Map.

Scores on the map corresponding to the project location will be used to assess a final Active Transportation equity score. This section is for applicant information only. The final equity scores will be assessed by MnDOT staff based on the project location(s). Applicants do not need to enter any information into this section.

V. Attachments

Required attachments that do not affect final score

- *For non-Tribal applicants, approved submitting agency resolution of support, including approved resolutions from any non-Tribal agency (except MnDOT) that owns or manages right-of-way that will be impacted with AT-eligible infrastructure improvements.
- An approved sponsor resolution (if applicable)
- Approved letter of support from MnDOT District Engineer if the project has MnDOT trunk highway impacts
- *Engineering or planning based cost estimate with itemized breakdown.
- *A timeline indicating major milestones of the project and their anticipated completion dates
- Awarded federal application and award letter (if requesting local match for federal project)

Required attachments that may affect the final application score:

- *Pedestrian, bicycle, or other transportation plan or study that identifies the proposed project or improvements or a link to these documents
- Any updates, addendums, surveys, etc. that show updates to the above listed documents.
- *At least one project location map with project routes or improvements identified

Additional project attachments (if applicable/available) that may affect the final score:

- Planning documents that identify the proposed project
- Project design exhibits or sections
- Community survey or crash data (related to safety concerns)
- Baseline pedestrian and cyclist traffic counts
- Additional approved letters of support

VI. Conflicts of Interest Disclosure

See online application for full conflict disclosure language.

☒ Determined that no potential organization conflict of interest exists

☐ Determined a potential organization conflict of interest that is described below (2000 character)

[Click or tap here to enter text.](#)

VII. Affirmation of Noncollusion

Please affirm:

1. That I am the applicant or representative of the applicant entity; and
2. That the grant application in response to this solicitation has been arrived at by the applicant independently or in coordination with a consultant, partnering entity, or project sponsor and has been submitted without collusion and without any agreement, understanding or planned common course of action with any other solicitation applicant designed to limit fair and open competition; and
3. That I am fully informed regarding the accuracy of the statements made in this affirmation.

The Applicant affirms that this solicitation response has been submitted without collusion.*

☒ Yes

☐ No

RESOLUTION NO. 2024-11-81**RESOLUTION AUTHORIZING THE PURSUIT OF 2024 STATE ACTIVE TRANSPORTATION (AT) INFRASTRUCTURE PROGRAM FUNDING FOR THE WORTHINGTON EAST OKABENA LAKE TRAIL PROJECT**

WHEREAS, the Worthington East Okabena Lake Trail Project includes the trail from the intersection of South Shore Drive and South Lake Street to the intersection of Nobles Street & Trunk Highway 59/60, and

WHEREAS, the Worthington East Okabena Lake Trail Project provides the connection of two segments of existing trail and provide access to a culturally diverse segment of the community that is isolated by the Union Pacific railroad line, and

WHEREAS, the Active Transportation (AT) Infrastructure Program administered by the Minnesota Department of Transportation makes available up to \$1,000,000 to apply towards projects on local roads that are regionally significant, result in safety improvements, address transportation deficiencies, and contribute to economic development, and

WHEREAS, the proposed year for project construction is 2025.

NOW, THEREFORE BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF WORTHINGTON AS FOLLOWS; that

1. The City Council hereby supports the East Okabena Lake Trail Project, and
2. The City Council hereby supports the City of Worthington's pursuit of Active Transportation (AT) Infrastructure Program funding and authorizes staff to prepare and submit such application, and
3. The City Council hereby commits to funding project elements not eligible for AT funding, ensuring the project will comply with all AT funding requirements, and following the project schedule as presented in the application.

ADOPTED BY THE CITY COUNCIL OF WORTHINGTON, MINNESOTA, THIS 12th DAY OF NOVEMBER 2024.

(SEAL)

CITY OF WORTHINGTON

ATTEST:


Mindy Eggers, City Clerk


Rick Von Holdt, Mayor

RESOLUTION NO. 202450

**A RESOLUTION APPROVING CITY OF WORTHINGTON PURSUIT OF ACTIVE
TRANSPORTATION FUNDING AWARD**

WHEREAS, the City of Worthington is seeking Active Transportation (AT) Program funding to construct the East Okabena Lake Trail Project; and

WHEREAS, the East Okabena Lake Trail Project will construct 0.8 miles of ADA-accessible concrete shared-use path in Worthington beginning at S Lake Street, cross the railroad tracks, and then run along Sherwood Street before aligning with East Lake Boulevard adjacent to the open space wetland area and connecting to the Highway 60 at Nobles Street; and

WHEREAS, this trail will connect the existing trails and provide safe access under railroad tracks through a box culvert, eliminating a major safety hazard for bicyclists and pedestrians; and

WHEREAS, the Active Transportation (AT) Program is administered by the Minnesota Department of Transportation makes available up to \$1,000,000 to apply towards projects that make walking, biking and rolling better with the aim to increase the number of people walking and biking to destinations; and

WHEREAS, the Active Transportation Program requires a city, such as Worthington, to evaluate impacts of projects that cross different jurisdictional right-of-way; and

WHEREAS, the East Okabena Lake Trail Project crosses into Nobles County right-of-way; and

WHEREAS, the proposed project will have minimal impacts on Nobles County right-of-way, and

WHEREAS, the proposed year for project construction is 2025.

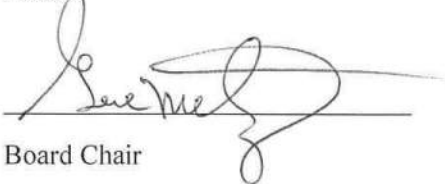
**NOW, THEREFORE BE IT RESOLVED BY THE BOARD OF COMMISSIONERS OF NOBLES
COUNTY, MINNESOTA:**

1. The County supports the City of Worthington's pursuit of Active Transportation (AT) funds for the East Okabena Lake Trail Project, and
2. The County acknowledges that the East Okabena Lake Trail project crosses into county-owned right-of-way and will have minimal impacts.

Resolution 202450

Adopted by the Nobles County Board of Commissioners this 19th day of November, 2024.

Attest:



Board Chair

CERTIFICATION

STATE OF MINNESOTA

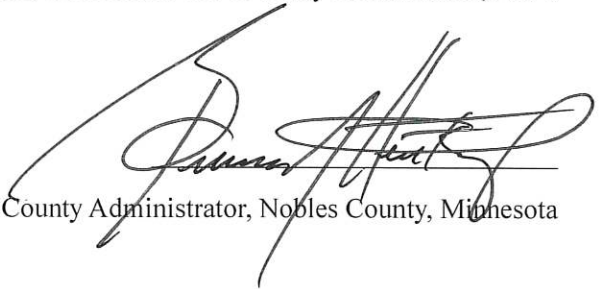
County of Nobles

I, Bruce Heitkamp, County Administrator of said County of Nobles, do hereby certify that I have compared the foregoing copy with the original resolution adopted by the County Board on the 19th day of November 2024 and now remaining on file and of record in my office and that the same is a correct transcript and of the whole of such original.

Witness my hand and official seal this 19th day of November, 2024.

SEAL

Bruce Heitkamp, County Administrator, Nobles County, Minnesota

A handwritten signature in black ink, appearing to read 'Bruce Heitkamp', is written over a horizontal line. The signature is stylized with a large, sweeping initial 'B'.

ENGINEERS COST ESTIMATE:

MnDOT Active Transportation Infrastructure Grant Application

East Okabena Lake Trail

City of Worthington, Minnesota

No.	Description	Estimated Cost	Expected Completion Date
TRAIL IMPROVEMENTS			
1	Mobilization	\$55,000	June 2025
2	Removals - surface features and trees	\$40,000	June 2025
3	Grading and Base	\$110,000	July 2025
4	Storm Drainage Improvements	\$20,000	August 2025
5	Channel Improvements	\$45,000	August 2025
6	6" Concrete Trail	\$430,000	October 2025
7	Street and Driveway Pavement	\$60,000	October 2025
8	Ornamental Metal Railing	\$30,000	October 2025
9	Erosion Control and Seeding	\$90,000	October 2025
10	Signage and Striping	\$20,000	November 2025
Subtotal		\$900,000	
RAILROAD/TRAIL CROSSING IMPROVEMENTS			
1	Mobilization	\$50,000	May 2026
2	14 x 10 Concrete Box Culvert	\$400,000	August 2026
3	Remove Existing Railroad Bridge Deck	\$200,000	October 2026
Subtotal		\$650,000	
Total Construction Costs			
		\$1,550,000	
Administration and Design Engineering		\$225,000	
Construction Engineering		\$100,000	
Railroad Engineering Reimbursement		\$100,000	
Right-of-Way Easements		\$20,000	
Total Project Costs			
		\$1,995,000	

Project Timeline



East Okabena Lake Trail City of Worthington, MN

Steps Required	Action Required By	Date	Comments
Resolutions of Support, Sponsorship and Agreement to Operate and Maintain	City Council	12/12/2022	Complete.
Preliminary Design	Engineer	July 2023 - May 2024	Complete.
Resolution Authorizing the Pursuit of 2024 State AT Program Funding	City Council	11/12/2024	Complete.
Resolution Authorizing the Pursuit of 2024 State AT Program Funding	Nobles County	11/19/2024	Complete.
Project Memorandum	Engineer	July 2023 - February 2025	
Final Design	Engineer	May 2024 - February 2025	
Right-of-Way Acquisitions	Engineer, Clerk	October 2024 - February 2025	
Bidding Process	Engineer	March 2025 - April 2025	
Begin Construction	Contractor	6/1/2025	2025 - Trail Construction
End Construction	Contractor	10/30/2026	2026 - UPRR Crossing

April 3, 2023

Stephen Schnieder
303 Ninth Street
PO Box 279
Worthington, MN 56187

RE: East Okabena Lake Trail

Dear Mr. Schnieder,

I am pleased to inform you that your Transportation Alternatives project was selected for funding through the Minnesota Department of Transportation District 7 Area Transportation Partnership 2027 solicitation.

The Transportation Alternatives (TA) program has set aside \$592,000 in federal funds towards the construction cost of your infrastructure project (SP 177-090-006) in FY 27. Since all of the funds currently available in the TA program are being allocated, consider this amount as the maximum federal TA funding allowed for this project.

Below is an initial breakdown of the funding on your project.

Federal Funds (80% maximum):	\$ 592,000
<u>Local Funds (20% minimum):</u>	<u>\$ 293,000</u>
Estimated Total Construction Cost:	\$ 885,000


Your project will be programmed into the 2024-2027 State Transportation Improvement Program (STIP) which includes the state's next four-years of planned transportation projects.

The next steps for your agency will be to complete the project environmental documentation and get Federal Highway Authorization on the project by following the procedures outlined in the Delegated Contract Process (DCP) for Local Agency Federal Aid Projects.

The MnDOT State Aid Office will help provide your agency with guidance on the environmental documentation and DCP process for project authorization, construction and reimbursement. Feel free to contact Lisa Bigham (507-381-2563) or Debra Yates (507-327-0204) for assistance.

Sincerely,

Lisa

 Digitally signed by Lisa Bigham
Date: 2023.04.03 14:56:07 -05'00'

Lisa Bigham
Assistant District Engineer – State Aid

City of
Worthington
Active
Living Plan

May

2015

This project was supported by a U-Care Foundation Community Grant.

Prepared by
the Southwest
Regional
Development
Commission

WORTHINGTON ACTIVE LIVING PLAN

Why Active Living?

“When you are trying to build a street, you really have to look at the place you are designing and say: Is the function of this place to create wealth or is the function of this particular roadway to move cars quickly?...”

*The highest returning types of building form--are the kind that were built prior to WWII. It's where you've got the line of shops with a second story of apartment or living unit. The kind of stuff that you see on Main Streets all over this country. There's a reason our ancestors built that way. It's really, really financially productive. Some of the obstacles to doing that today: one of them includes the fact that we've just over-engineered and over-built our streets. A lot of that comes from national standards that have been established; a lot of it comes from the way that funding comes down through the Feds and the state and the mandates that come with that in terms of what the design capacities have to be. But one of the simplest solutions to getting these neighborhoods back is just to go out and narrow up the street lanes. When you narrow up the street lanes, cars drive slower, people feel more comfortable there. They walk across the streets, to a store across the street. And in a real subtle and cheap way you get a lot more pedestrian traffic, a lot more retail, a lot more people. And people spend money, and that's what makes a place wealthier. **When people are there, people invest more.**”¹*

Active Living is a way of life that integrates physical activity into your daily routine. It starts with choosing to walk or bike instead of taking the car. This choice is influenced by the built environment.

Goal: To enable the City of Worthington to become a more walkable community through strategic pedestrian improvements outlined in the Worthington Active Living Plan.

Objective: Identify destinations, gaps, areas of concern, and prioritize projects for addressing pedestrian infrastructure in the City of Worthington.

A Proposal: Think about walkability and bikeability when considering decisions that influence the built environment.



¹ Econ Talk. Charles Marohn on Strong Towns, Urban Development, and the Future of American Cities. Accessed: 5/13/15. Available: http://www.econtalk.org/archives/2014/05/charles_marohn.html

For more information regarding Active Living, contact:



City of Worthington
303 9th Street
Worthington, MN 56187
Phone: 507.372.8600
www.ci.worthington.mn.us



Southwest Regional Development Commission

Southwest Regional Development Commission
2401 Broadway Ave, Ste 1
Slayton, MN 56172
Phone: 507.836.8547
www.swrdc.org



Health Educator, Statewide Health Improvement Program
Nobles County Community Health Services
318 Ninth Street, Worthington, MN 56187
507-295-5360
<http://www.co.nobles.mn.us/Departments/CommunityServices/CommunityHealthServices.aspx>



Statewide Health Improvement Program
Des Moines Valley Health & Human Services
235 9th Street, Windom, MN 56101
507-831-1987

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CHAPTER 1: INTRODUCTION

I Introduction

Active Living Plans are community plans to promote an active lifestyle for all residents. The focus of this Active Living Plan was on identifying destinations, gaps in pedestrian infrastructure (sidewalks and trails), areas of concern, and prioritizing projects to make the community more pedestrian friendly. Through the implementation of this plan, the City of Worthington will become more walkable and bikeable, and this will encourage the community to be more physically active. By promoting a more active lifestyle, there are a number of positive externalities that include: economic benefits, reduced traffic congestion, better air quality, and an overall healthier community.

“In 1969, approximately 50 percent of children in the US walked or bicycled to school, with approximately 87 percent of children living within one mile of school walking or bicycling. Today, fewer than 15 percent of schoolchildren walk or bicycle to school. As a result, kids today are less active, less independent and less healthy.”²

Benefits

There are a number of individual and community benefits of creating a more pedestrian friendly community.

Economic Development Benefits

Economic Development does not have one singular definition. Attracting businesses is only one strategy for economic development. Another strategy is to concentrate on attracting and retaining residents. Part of that strategy is planning for pedestrians and developing community facilities. Supporting walking and biking can have a positive impact on attracting and retaining residents, businesses, and workers. Compact, walkable developments provide economic development benefits through increased property values, enhanced marketability, and faster sales than conventional developments.³

Livability is a primary economic development strategy. A significant element of livability is creating a pedestrian friendly community. The City of Worthington does have a trail around the lake that attracts rural residents and residents from neighboring communities. The City of Worthington also has sidewalks on the majority of the busier streets, but there are gaps, connectivity issues, and some of these sidewalks are not in the best condition.

Property values are one of the more significant variables impacting where people want to live. If you build or buy a house you want to have a return on your investment. Another element of the economic benefits of a more walkable community is their effect on property values. “Houses with the above-

² Safe Routes to School National Partnership. Accessed: 12/26/14. Available: <http://saferoutespartnership.org/about/history/what-is-safe-routes-to-school>

³ Transportation Policy Institute: Accessed: 1/22/15. Available: <http://www.vtpi.org/walkability.pdf>

average levels of walkability command a premium of about \$4,000 to \$34,000.”⁴ Higher property values can help to increase the tax base for your city. In the City of Worthington, higher property values could help to entice new housing construction. Currently, the cost of building a new home in some areas of Southwest Minnesota may not generate a positive net return when trying to sell the new house. The construction costs of a new home are higher than the market price of a house in the region. This is only one of the reasons why several cities in Southwest Minnesota are facing housing shortages.

The National Realtors Association points to the changing behavior of home buyers, “people prefer to live in communities that allow them to walk to shops, parks and other destinations and will pay more for a home that allows them to do just that.”⁵ First time home buyers are looking for neighborhoods and cities that are more walkable. “Millennials, though, are just part of the picture. As baby boomers get older, many are opting to live in places where they don’t have to drive as much to get to services and where they can age in place.”⁶ This is a national trend and Southwest Minnesota needs to recognize livability, walkability, and bikeability as economic development tools.

Southwest Minnesota has had difficulties attracting workers to move to the region. Southwest Minnesota has some of the lowest unemployment rates in the state. Southwest MN has about as many people unemployed as there are jobs available; however, factors on both the supply side and the demand side come into play. On the supply side, hiring difficulties are generally caused by a mismatch between job requirements and the training, skills, and experience of the applicant. On the demand side, hiring difficulties are generally caused by candidates’ preferences such as work hours, compensation, geographic location, etc.

How can the City of Worthington and other cities in southwest Minnesota attract people to their city?

- ▶ Concentrate on livability as an economic development strategy.
- ▶ Make land use decisions that encourage compact efficient developments that are pedestrian friendly and increase connectivity.
- ▶ Promote regional activities to highlight everything southwest Minnesota has to offer. A livability strategy needs to be regional, since it is difficult to market a smaller population city on your own.

⁴ National Realtors Association. The Value of Walkability. Accessed: 5/13/15. Available: <http://www.realtor.org/newsroom/real-estate-story-ideas/the-value-of-walkability> Source: Brookings Institute: <http://www.brookings.edu/research/papers/2012/05/25-walkable-places-leinberger>

⁵ National Realtors Association. The Value of Walkability. Accessed: 5/13/15. Available: <http://www.realtor.org/newsroom/real-estate-story-ideas/the-value-of-walkability> Source: Brookings Institute: <http://www.brookings.edu/research/papers/2012/05/25-walkable-places-leinberger>

⁶ Builder Magazine. Why Smart Builders Care About Walkability. Accessed: 5/29/15. Available: http://www.builderonline.com/land/development/why-smart-builders-care-about-walkability_o

Health Benefits

Walking and biking are two of the most popular ways to integrate regular physical activity into your daily routine. Physical activity is one of the most important things you can do for your health. Physical activity can help:

- ▶ Control your weight
- ▶ Reduce your risk of cardiovascular disease
- ▶ Reduce your risk for type 2 diabetes and metabolic syndrome
- ▶ Reduce your risk of some cancers
- ▶ Strengthen your bones and muscles
- ▶ Improve your mental health and mood
- ▶ Improve your ability to do daily activities and prevent falls, if you're an older adult
- ▶ Increase your chances of living longer

Transportation Benefits

Communities that have pedestrian scale infrastructure and programs promoting walking and biking tend to be more physically active. “People who live by trails are 50 percent more likely to meet physical activity guidelines.”⁷ Adding pedestrian infrastructure and promoting walking and biking will help to reduce:

- ▶ Roadway congestion
- ▶ Time wasted stuck in traffic
- ▶ Driver frustration
- ▶ Pollution

“Roadway improvements to accommodate pedestrians and bicyclists also can enhance safety for motorists. For example, adding paved shoulders on two-lane roads has been shown to reduce the frequency of run-off-road, head-on, and sideswipe motor vehicle crashes.”⁸

⁷ Active Living Research. Accessed: 1/22/15. Available:
http://activelivingresearch.org/files/ALR_Brief_ActiveTransportation_0.pdf

⁸ University of North Carolina Highway Safety Research Center. Accessed: 1/22/15. Available:
<http://www.pedbikeinfo.org/data/factsheet.cfm>

Environmental Benefits

Newer developments in cities have moved away from sidewalks on both sides of the street, having garages facing the alleyway behind the house, and having similar sized lots. This creates a disincentive to walk and bike and decreases the interconnectedness of the community. A study conducted by the University of British Columbia found that lowering neighborhoods' walkability increases the use of motor vehicles and, therefore, raises the air pollution and body mass index per capita.⁹ Cul-de-Sacs were also found to decrease the walkability of a neighborhood.

Motor vehicle traffic generated by the travel to and from school adds 20 to 30 percent more traffic volume to the roads.¹⁰ Replacing short trips with walking or biking can help reduce air pollution and energy consumption. There are also a number health benefits (refer to health benefits above).

Geographic Location

City of Worthington

The City of Worthington is located in southwestern Minnesota at the intersection of Interstate 90 and Highway 60. The City of Worthington is a regional hub in Southwest Minnesota. With a population of 12,764, Worthington is the largest city in Nobles County.¹¹

"Home to research companies and several manufacturing, processing and shipping companies, Worthington also boasts an excellent school system and technical college, two clinics, and a hospital. And don't forget the various recreation activities including bike paths around Lake Okabena, nineteen city parks, a disc golf course and much more. There are also art and entertainment shows at the Memorial Auditorium, plus the many festivals that our city hosts throughout the year."¹²

Cottonwood County

Nobles County is located in southwest Minnesota. The county is bordered on the north by Murray County, on the south by the State of Iowa, on the east by Jackson County, and on the west by Rock County. Cities within Nobles County include: Adrian, Bigelow, Brewster, Dundee, Ellsworth, Kinbrae, Lismore, Round Lake, Rushmore, Wilmont, and Worthington (county seat).

⁹ Environmental Health Perspectives. Healthy Neighborhoods: Walkability and Air Pollution. Accessed 1/22/15. Available: <http://www.ncbi.nlm.nih.gov/pmc/articles/PMC2801167/>

¹⁰ Safe Routes to School Guide. Accessed: 1/22/15. Available: http://guide.saferoutesinfo.org/pdf/SRTS-Guide_Introduction.pdf

¹¹ American Fact Finder. 2010 Census. Accessed: 5/29/15. Available: http://factfinder.census.gov/faces/nav/jsf/pages/community_facts.xhtml

¹² City of Worthington. Accessed: 6/17/15. Available: <http://www.ci.worthington.mn.us/>

Table #1

Nobles County Jurisdiction Map



- +— Railroad
- Trunk Highways
- Lakes
- Named Streams and Rivers
- City
- Township
- County
- State Line



II Planning Process

The City of Worthington Active Living Planning Process was an efficient planning process that was based on identifying destinations, gaps, areas of concern, and potential projects. The Planning Team consists of community members and city staff who attended the two community meetings.

Planning Process

- Wikimapping (continuous...)
- Community Meeting #1
- Community Survey
- Community Meeting #2
- City Council will rank projects

Wikimapping

Wikimapping is an online public input tool community members can use to identify issues regarding walking and biking in the City of Worthington. Community members can provide input by adding a point or route on an interactive map or by commenting on existing posts. Wikimapping was an effective way of engaging community members who were not able to attend the two community meetings.

Community members could add points on an interactive map regarding: barriers to walking and biking, bus and transit stops, existing bike parking, lighting is poor, need bike parking, places I go, problem intersection, school, driving issue, traffic and congestion, and trash is an issue. Community members could add a route on the interactive map regarding: existing on street bike route, high stress area (speed/ traffic), no sidewalk, on street bike route needed, recreational route, routes I'd like to use, route to and from after school activity, route to and from school, shortcut I use (not a trail or road), sidewalk in poor condition. When a community member clicks on a point the user can select one of these categories and a box will appear. The user can then write a description of the issue in the box. Other users can click on the point or route and agree, disagree, or abstain with the comment. Other users can also comment on the existing comment.

Community Meeting #1

The first community meeting was held at the Worthington Fire Hall on March 19th, 2015, from 5:15pm to 6:45pm. The agenda for the meeting was to discuss the local issues and concerns that were identified via Wikimapping and conversations with community members. Community members were also able to identify additional issues and concerns that were not identified before the meeting. The first meeting was an opportunity to discuss the existing conditions and voice opinions and concerns regarding walking and biking in Worthington.

Community Survey

A community survey was distributed between the first and second community meeting. The survey was distributed along with the City's monthly utility bill. The survey was another tool to engage community members who were not able to attend the two community meetings. The survey asked community members eight questions. Five of these questions had community members quantify the category on a scale of one to 10. The Worthington Community Assessment Survey can be found in the Appendix to this plan.

The quantitative and qualitative questions include:

- Did you have room to walk?
- Was it is to cross the street?
- Did drivers behave well?
- Could you follow safety rules?
- Was your walk pleasant?
- General atmosphere (summary of all the categories)

The qualitative questions included:

- Are there any routes you would like to walk and currently cannot due to safety issues, existing gaps in the sidewalk network, or other pedestrian infrastructure that discourages you from walking or biking?
- Please highlight on the map the route you walked or biked in Worthington.

Community Meeting #2

The second community meeting was held at City Hall on April 22nd, 2015, from 5:15pm to 6:30pm. The agenda for the meeting was to discuss potential goals and strategies regarding walking and biking in Worthington. There were a number of issues that were identified and discussed at the first community meeting on March 19th. At the second meeting, community members were still able to discuss existing conditions that were not identified at the first meeting.

Traffic and Safety Committee Meeting

Southwest Regional Development Commission staff presented a Pedestrian Functional Classification System to the Worthington Traffic and Safety Committee on April 28th, 2015. The proposed Pedestrian Functional Classification System would be one way of implementing the City of Worthington's Complete Streets Policy. The Pedestrian Functional Classification System would have the City of Worthington classify every street in regards to pedestrian transportation needs.

Pedestrian transportation need would be based on a three tier classification system. Different streets require different pedestrian amenities to make it safe to walk and bike. A Complete Street does not have a singular definition. A Complete Street is any street you feel safe walking or biking on. A Complete Street does not have to have a sidewalk on both sides of the street, but you have to consider all users when deciding if it is safe for pedestrians.

The Traffic and Safety Committee formally supported establishing a Pedestrian Functional Classification System in the City of Worthington. An example Pedestrian Classification System is outlined in the Goals and Strategies Chapter. This system may be modified for implementation in the City of Worthington.

City Council Meeting

A draft Worthington Active Living Plan was presented to the Worthington City Council on July 13th, 2015. The City Council was asked to rank the goals and strategies and provide feedback regarding the plan. The Pedestrian Functional Classification System was also presented to the City Council as a way of implementing their Complete Streets Policy. An example Pedestrian Functional Classification System is outlined in the Goals and Strategies Chapter of this plan. A Draft Pedestrian Functional Classification Map can be found in the Appendix to this plan.

III Existing Conditions

Existing Health Issues

Research conducted by the USDA shows that one in three American children are overweight or obese, putting them at risk of preventable disease like diabetes, high blood pressure, and heart disease.¹³ In 2010, the obesity rate in Nobles County was 28 percent while the state average was 26 percent. In 2014, the obesity rate in Nobles County was 28 percent while the state average was 26 percent.¹⁴

According to the 2013 Minnesota Student Survey, on a typical day 29 percent of Nobles County 5th graders reported that they spend zero hours going outside, taking a walk, or going for a bike ride. Only 41 percent of 5th graders reported being physically active for at least 60 minutes per day.¹⁵ In 2013, 32 percent of Nobles County 8th grade students reported being overweight or obese.¹⁶

¹³ Kuphal, Kyle and Fodness, Mavis. New school meal standards target obesity. Accessed 12/26/14. Available at: <http://www.pipestonestar.com/Stories/Story.cfm?SID=38130>

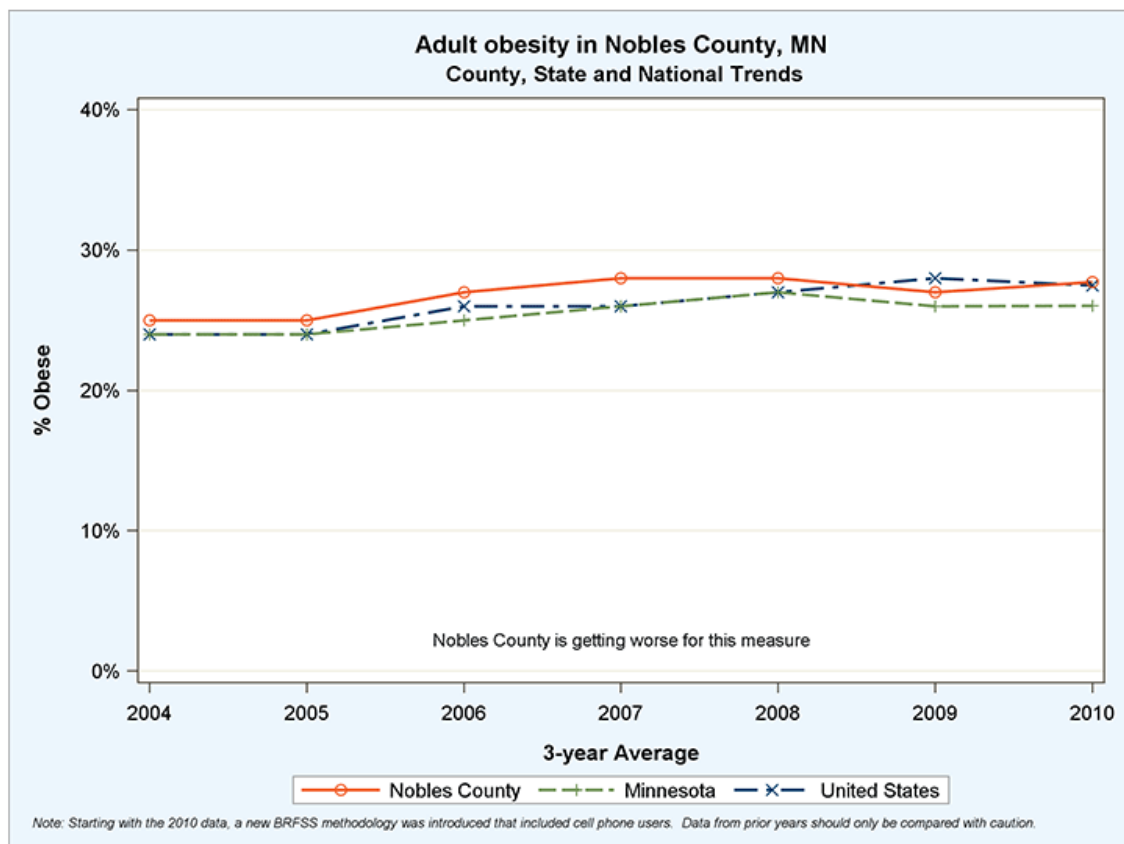
¹⁴ Robert Wood Johnson Foundation. County Health Rankings. Accessed 12/26/14. Available: <http://www.countyhealthrankings.org/app/minnesota/2014/rankings/nobles/county/outcomes/overall/snapshot>

¹⁵ Minnesota Department of Health. Minnesota Student Survey. Accessed: 12/29/14. Available: <http://www.health.state.mn.us/divs/chs/mss/countytables/nobles13.pdf>

¹⁶ Minnesota Department of Health. Minnesota Student Survey. Accessed: 12/29/14. Available: <http://www.health.state.mn.us/divs/chs/mss/countytables/nobles13.pdf>

Table #2

Adult Obesity in Nobles County



17

Being overweight or obese not only increases the risk of premature death and many other diseases and health conditions, but there are substantial economic costs as well. There are both direct and indirect costs associated with being overweight or obese. Direct costs are the higher medical costs associated with diagnosing, treating, and trying to prevent conditions related to being overweight or obese.

Indirect costs of being overweight and obese include morbidity and mortality costs such as lost productivity, absenteeism, and premature death. “Based on national estimates, the overall financial burden of obesity in Minnesota in 2006 was estimated at \$2.8 billion.”¹⁸ The medical costs associated with obesity nationally were estimated at \$147 billion in 2008. This translates into a \$1,429 higher yearly medical cost for people who are obese over those of normal weight.¹⁷

Since the late 1960’s, there has been a dramatic decline in the percentage of students who walked or bicycled to school. Nationally, only 13 percent of students grades Kindergarten through 8th grade reported usually walking or bicycling to school in 2009, while 48 percent of kindergarten students

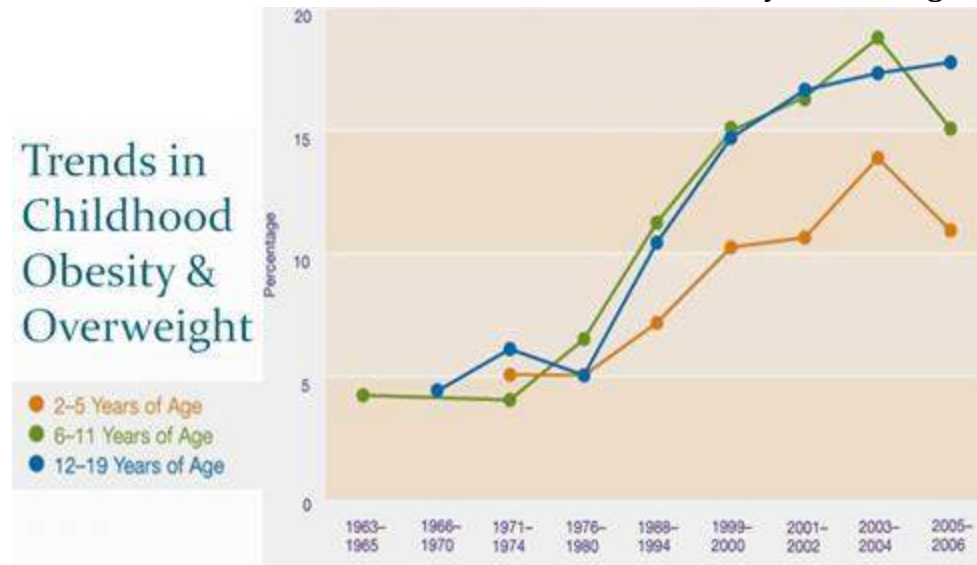
¹⁷ Robert Wood Johnson Foundation. County Health Rankings. Accessed: 12/30/14. Available: <http://www.countyhealthrankings.org/app/minnesota/2014/rankings/nobles/county/outcomes/overall/snapshot>

¹⁸ Minnesota Medicine. The Obesity Challenge. 12/26/14 Available at: <http://www.minnesotamedicine.com/PastIssues/December2012/theobesitychallenge.aspx>

through 8th grade reported usually walking or bicycling to school in 1969. Distance is a strong indicator associated with how children get to school, but only 35 percent of kindergarten through 8th grade students nationally, who lived within a mile of school, reported usually walking or bicycling to school once a week. In 1969, 89 percent of kindergarten through 8th grade students, who lived within a mile of school, reported usually walking or bicycling to school once a week.¹⁹

Table #3

Trends in Childhood Obesity & Overweight



CDC, Nation Center for Health Statistics

Just a decade or two ago, a large number of children were free-range children. These children walked or bicycled around the neighborhood and community being more independent. Increasingly children are dependent on their parents for transportation. Instead of walking or bicycling children are getting rides. Five to seven percent of vehicle miles traveled and 10 to 14 percent of all personal vehicle trips made in high traffic times in the morning are personal vehicles taking Kindergarten through 8th grade students to school.²⁰

Reducing the number of vehicle trips would create a more efficient, safe, and connected community. Getting children walking and bicycling again is one way to combat inactivity and the dramatic rise in obesity. Addressing obesity through Safe Routes to School and promoting an active lifestyle has a number of positive externalities.

¹⁹ The National Center for Safe Routes to School. How Children Get to School: School Travel Patterns from 1969 to 2009. Accessed: 12/26/2012. Available:

http://saferoutesinfo.org/sites/default/files/resources/NHTS_school_travel_report_2011_0.pdf

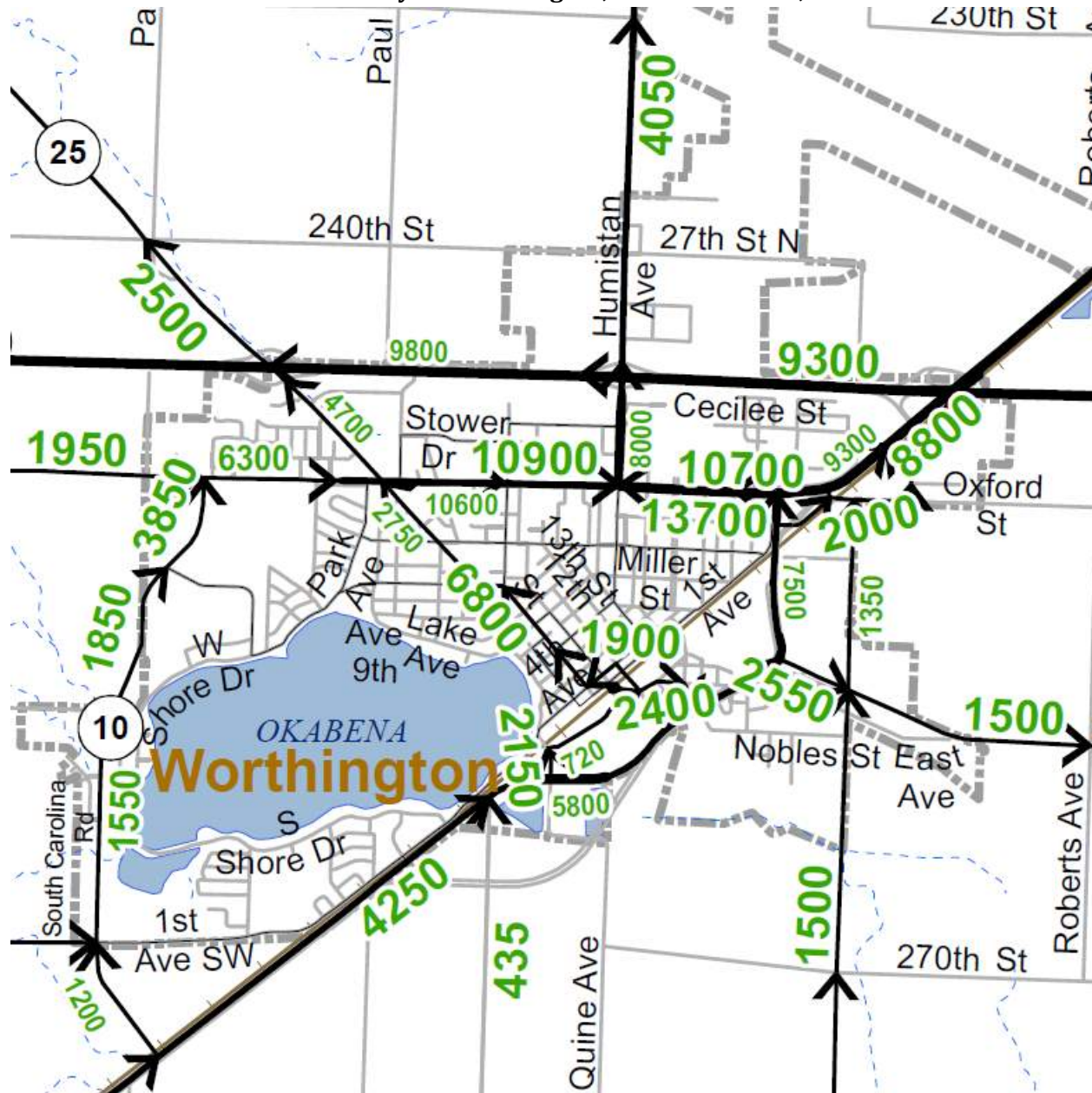
²⁰ The National Center for Safe Routes to School. How Children Get to School. Accessed 12/31/2014. Available:

http://saferoutesinfo.org/sites/default/files/resources/NHTS_school_travel_report_2011_0.pdf

Traffic Volumes

The Worthington Active Living Planning Process analyzed traffic volumes from 2013.

Table #4 City of Worthington, Traffic Volumes, 2013



Crash Data

The Worthington Active living Planning Process analyzed crash data within the City of Worthington. Within the City of Worthington, there were 1676 reported crash occurrences from 2004 through October 2014. Thirty-four of the outlined crashes involved a pedestrian or bicyclist (none of the crashes were fatal). There were no reported crashes involving pedestrians.

Table #5

City of Worthington Crash Data, 2004 through October 2014

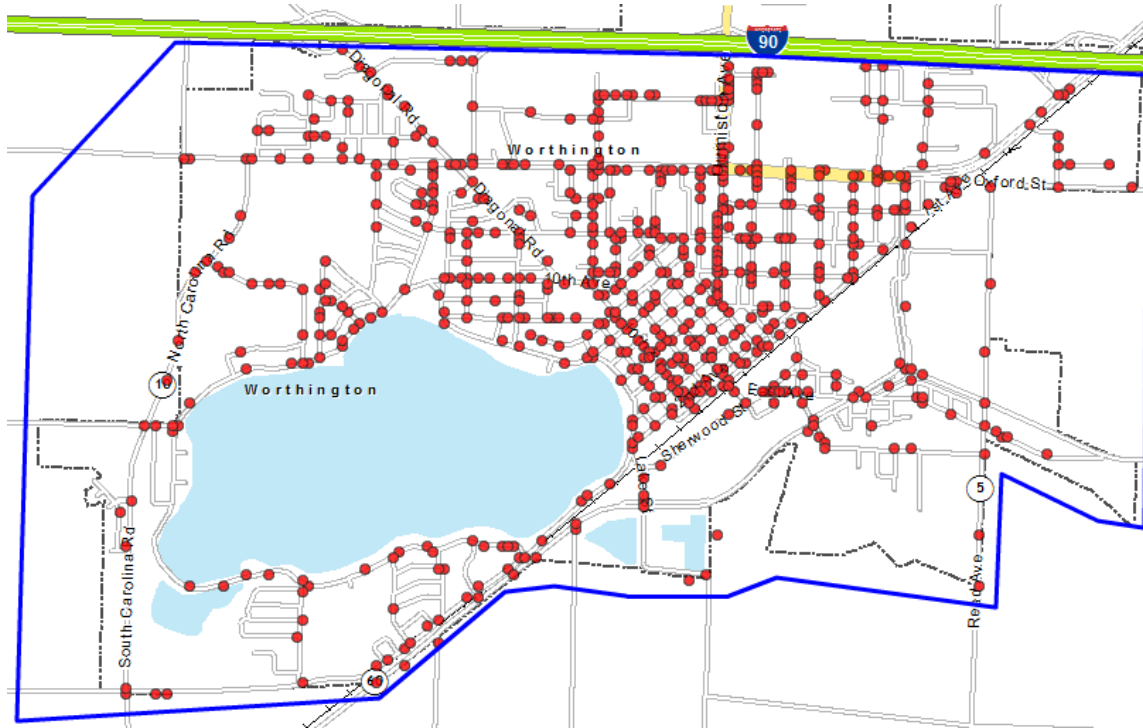


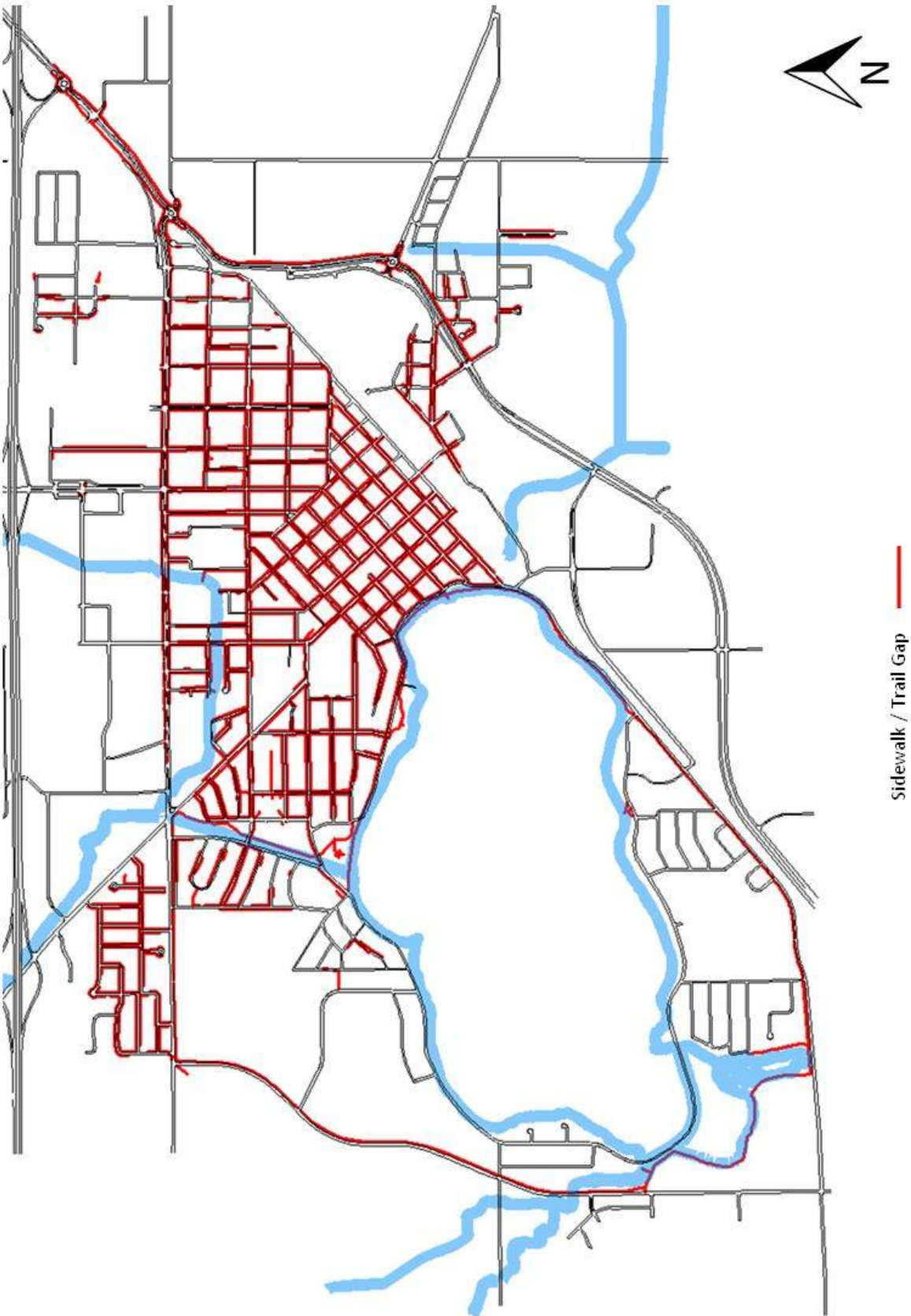
Table #6

Crash Data - Bike



Table #7

City of Worthington Sidewalk Map



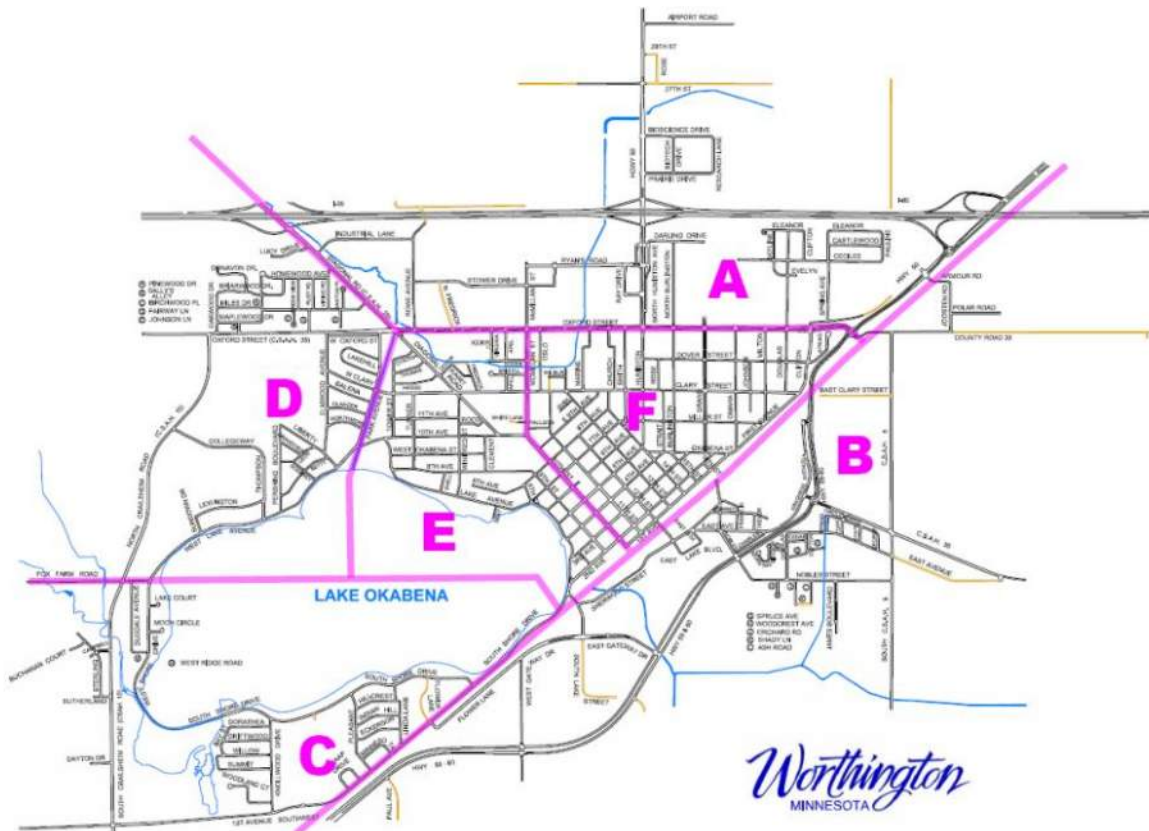
Walkability & Bikeability

Community members in the City of Worthington had an opportunity to rank the existing conditions in regards to walking and biking on a scale of one to 10 (10 being best and 1 being the worst). The categories that were ranked include: general atmosphere, did you have room to walk, was it easy to cross streets, did drivers behave well, could you follow safety rules, and was your walk pleasant. Refer to Appendix for the survey and the variables that impact each category. Below is a summary of the ranking for arrival and departure.

Table #8 A Community Survey Summary

Section of City	1. General Atmosphere	2. Room to walk	3. Easy to cross street	4. Drivers behave well	5. Follow safety rules	Average 1,2,3
A	4	3	3	4	5	3
B	6	4	5	6	7	5
C	8	8	8	7	8	8
D	7	6	7	7	8	7
E	8	7	7	7	8	7
F	7	6	7	6	9	7
Average	7	6	6	6	8	

Table #8 B City of Worthington - Neighborhood Sections Map



Main Themes

- Section A of Worthington is underserved in regards to pedestrian infrastructure (Oxford / I-90 / Highway 59 Corridor).
- Trail is an asset.
- Gaps in the sidewalk network - "Can't walk on sidewalks, they are too sporadic, don't go to curbs and people don't shovel snow," Community Survey.
- Lack of esthetic appeal / curb appeal - "It is not nice to walk around most of Worthington," Community Survey.
- Sidewalks:
 - Need to be cleared of snow.
 - Need to be repaired when in poor condition
- Parked cars blocking sidewalks.
- Vehicles not stopping for pedestrians in crosswalks.

Below is a summary of comments – this is not an exhaustive list of comments. The list of comments highlights issues and needs that were identified in the Community Survey.

General Atmosphere:

- "I feel safe walking in my neighborhood and other places I walk," Community Survey.
- "There is definitely room for improvement throughout Worthington in all areas," Community Survey.

Room to walk:

- Ryan's Road – "We need sidewalks to Walmart / Ryan's Rd," Community Survey.
- Snow removal – "People on Oxford don't clear away their snow," Community Survey. "Have her enforce the clearing of snow on sidewalks," Community Survey.
- Grade / Causeway – "We love our walk around Sunset Bay but are concerned about safety on the grade," Community Survey. "We walk along the grade and would like a wider shoulder," Community Survey.
- Trail – "Love the new bike path - More access points needed to it," Community Survey.
- South Shore Drive – "Difficult to drive along S. Shore due to many walkers/etc./run/here - no place for car to move over," Community Survey.

- Cars often park blocking the sidewalk – “Some driveways have too many cars that block sidewalk - have to walk through or into street around them,” Community Survey.
- Branches and Debris – “Some places no sidewalks, also branches and debris on walks,” Community Survey.
- Homewood Hills Neighborhood – “Sidewalks in Homewood Hills Addition don't all extend to street. You have to walk on the grass boulevard to enter street to cross to next block,” Community Survey. “Lots of missing sidewalks, sidewalks don't go to curbs, sidewalks not shoveled, and sidewalks start and stop abruptly. No ramps, People have to walk in the street mostly. More bikes on sidewalk than walkers. Six cars parked in driveways over sidewalks.”
- Cracked and crumpled sidewalks need to be repaired – “Lots of toe catchers,” Community Survey. “Mostly cracked, broken, or uneven - Hard to push a stroller,” Community Survey.
- Incomplete sidewalk network – “We cannot take a 20 minute walk in our area without using the street for part of it,” Community Survey. “Children are walking on the street many times, which is not safe,” Community Survey.
- Bridge over Whisky Ditch on 10th Avenue – “Bridge has been closed forcing people to walk in the street,” Community Survey.
- Roundabouts – “I work @ JBS & would like to walk/bike to work but I have trouble navigating the roundabouts since there are no space,” Community Survey.

Easy to Cross Street:

- “Congestion by auditorium & St. Mary's Church low visibility - high traffic,” Community Survey. People don't pay attention to signs, kids driving too fast in warm months.
- 5th Avenue – “Crossing 5th Ave is sometimes difficult due to traffic/traffic speed,” Community Survey.
- Ramps – “Ramps needed at all crossing,” Community Survey.
- Drivers not stopping for pedestrians – “I walked once & a car let me walk across & the next car the man yelled at me for making him stop,” Community Survey. “Cars do not stop for pedestrians,” Community Survey.
- Crosswalks – “Need crosswalks to be marked on the street,” Community Survey. “There should be a crosswalk for people to take Nursing Home residents across South Shore Drive to Freedom Park,” Community Survey.
- “Corner of Pinewood & Miles Drive - Very Dangerous for Kids!” Community Survey.
- “Hard to cross by post office,” Community Survey.

- It is difficult to cross on the north side of the lake.
- Crailsheim Road – “It’s very scary crossing Crailsheim when cars travel fast,” Community Survey.
- 10th Street – “This is especially true by Johnson Eye Clinic trying to cross 10th Street & the next block down by the Atrium & appt. buildings,” Community Survey.
- Lake Avenue & Tower Street – “Crosswalk needed at corner of Lake Ave & Tower Street. Signs are ignored. Lot of pedestrian traffic heading to park/beach. Cars speeding around corner at 40 mph+,” Community Survey.
- Crossing McMillan Street at Clary Street is dangerous.
- Visibility is an issue – “Too many cars allowed to park on street,” Community Survey.

Drivers Behave Well:

- Drivers not stopping for pedestrians – “I walked once & a car let me walk across & the next car the man yelled at me for making him stop,” Community Survey. “Cars do not stop for pedestrians,” Community Survey.
- Bikers – “Bicycles on wrong side of road most of the time, don’t stop, yield, or signal,” Community Survey.
- Traffic speeds – “Driving a little fast on Maplewood & Miles Drive,” Community Survey. “Cars are always driving way to fast in Homewood Hills. Sometimes it gets real scary out here. There are a lot of children and pets,” Community Survey. “Drivers speed up a bit; especially by Slater Park,” Community Survey.
- Sharing the road – “As a driver comment - walkers walking down middle of road around lake & down the road on the grade. None want to move out of the way. I feel road is for drivers. Paths & sidewalks for walkers. Sometime 3 & 4 wide blocking whole lane,” Community Survey. “(Some) Drivers (not all!) Do not move over for people walking around lake Okabena (specifically by the ----Farm on West Lake Avenue),” Community Survey.
- “No one stops at stop signs!” Community Survey.
- “People don’t pay attention to signs, kids driving too fast in warm months,” Community Survey.
- Licenses – “Too many without licenses!” Community Survey.
- “Races down Lake Ave & young people tearing out of park,” Community Survey.
- Alleys – “Drivers from alleys don’t look for pedestrians,” Community Survey.
- Distracted driving – “Have to assume that all drivers are either texting or talking on cell phones. Most are,” Community Survey.

- Grade / Causeway – “Drivers are courteous but often have to swerve into the other lane to make room for us on the grade,” Community Survey.
- Lake Avenue – “Cars drive way too fast,” Community Survey. “Speed limit around the lake is not obeyed,” Community Survey.

Follow Safety Rules:

- “Have to watch for my own safety,” Community Survey.
- Crosswalks needed – “No crosswalk from Olson Park bicycle to grade,” Community Survey. “No crosswalks in Homewood Hills Neighborhood,” Community Survey. “South Shore Drive where the walking path crosses along 1st Ave. Need to mark the crosswalk area,” Community Survey.
- “It's unclear who has the right of way where the bike path crosses 10th St. at Centennial Park,” Community Survey.
- “Can’t walk on sidewalks, they are too sporadic, don't go to curbs and people don't shovel snow,” Community Survey.
- “Some people don't look before crossing the street! They expect drivers to see them. A lot of jay walking downtown,” Community Survey.
- “Cannot walk on shoulder. Too many cars allowed to park on street. This makes Grand Ave a one lane road on both sides,” Community Survey.
- Pedestrian education – “People need instruction on which side to walk & bike,” Community Survey.
- Clearing sidewalks of snow – “whoever is in charge of supervising the sidewalk clearing of snow is doing a poor job of it.”

Walk Pleasant:

- Trash – “Trash in the streets and yards! It looks shabby,” Community Survey. “Individuals can't make it to garbage containers just a few yards away,” Community Survey. “Always pile of trash at east end of Miles Drive and Viking where stop signs are at intersection of old Highway 266,” Community Survey.
- Yards & lawns - “Not pleasant with all the garbage, junk cars and run down houses,” “Some homes not well cared for and yards not kept up,” Community Survey. “Some people are not mowing their yards – eyesore,” Community Survey.
- “These Issues could be resolved if we had a police presence in the neighborhood,” Community Survey.

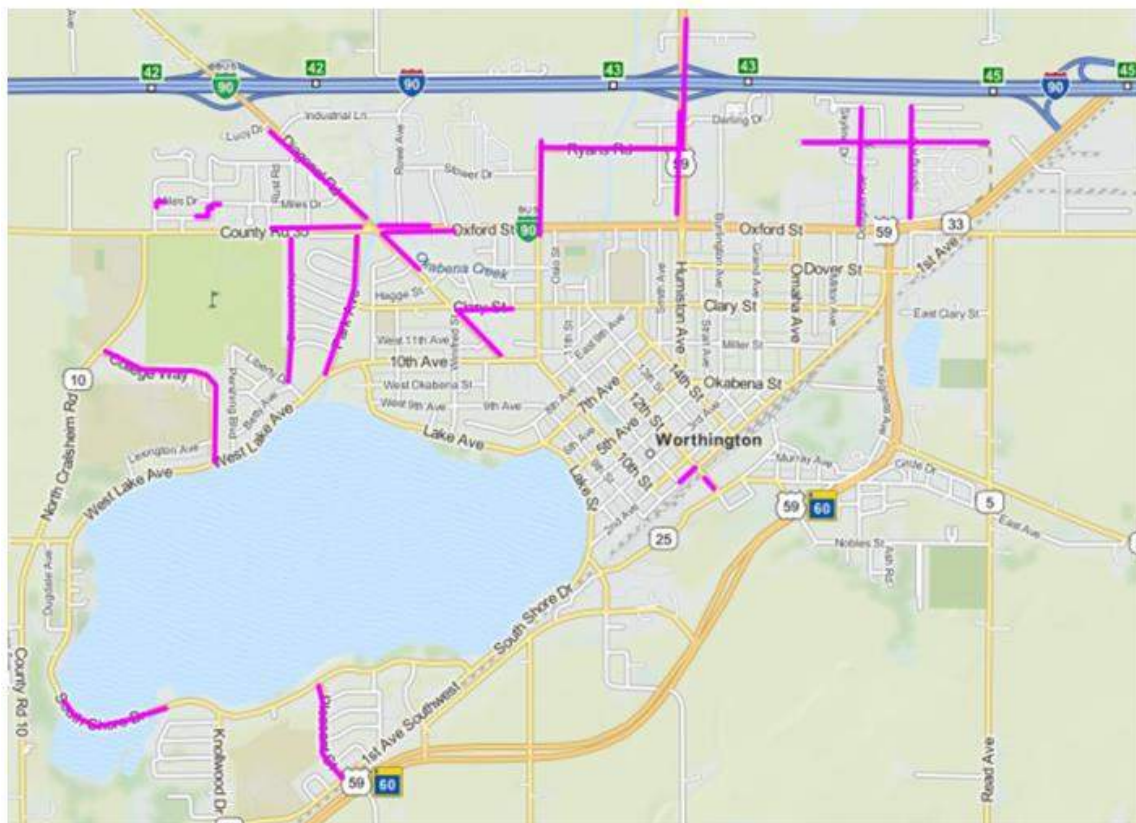
- “The only good walking path is around the lake. To get to it you have to drive,” Community Survey.
- “People want to walk around lake not out away from lake,” Community Survey.
- Lighting is needed in some spots to make it safe.
- Clean up after pets – “Get people to pick up their dog poop!” Community Survey.
- Trail – “Beautiful trail around the bay!” Community Survey. “The route around the lake and the new bike path is beautiful,” Community Survey.
- Dogs – Need to have dogs on a leash.
- Lighting – “Not well lit through Olson Park or on edge of town on the path so we chose to stay by the lake,” Community Survey. “Not well lit - especially the neighborhood around East Ave (Franklin, Murray, Trevor, etc.) Scary!!” Community Survey.

Gaps in the Sidewalk / Trail Network

A number of gaps in the sidewalk / trail network were discussed as part of the Worthington Active Living Planning Process.

Table #9

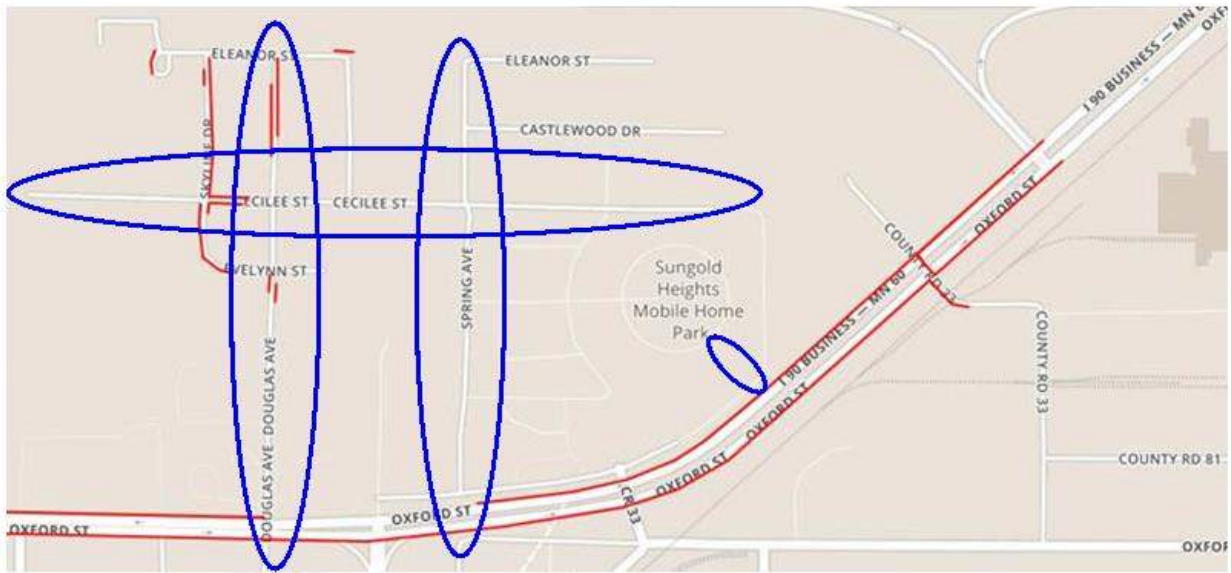
Input from Wikimapping – Sidewalk Gap



No Sidewalk

Table #10

Cecilee Street Neighborhood Sidewalk / Trail Gap




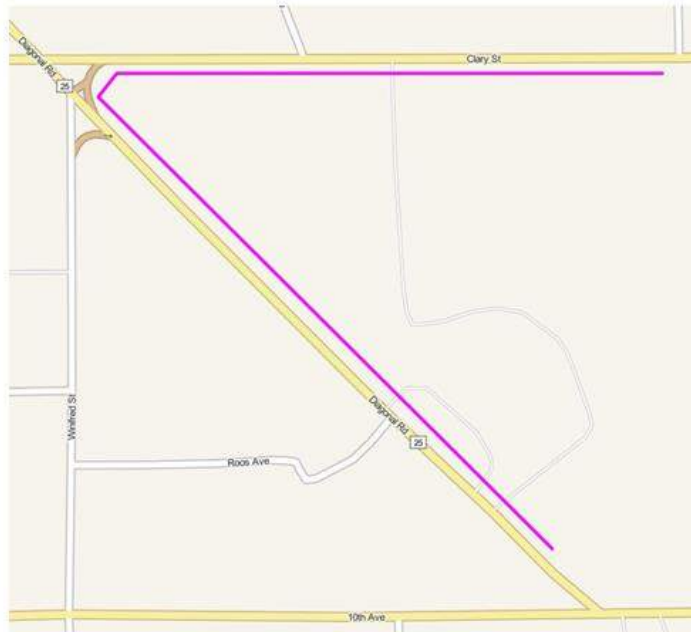
Sidewalk / Trail Gap 

Table #11

Input from Wikimapping – Sidewalk Gap



No Sidewalk 

Table #12

Oxford Street / Diagonal Road Sidewalk Gap



Table #13

Elmwood Avenue & Park Avenue Sidewalk Gap

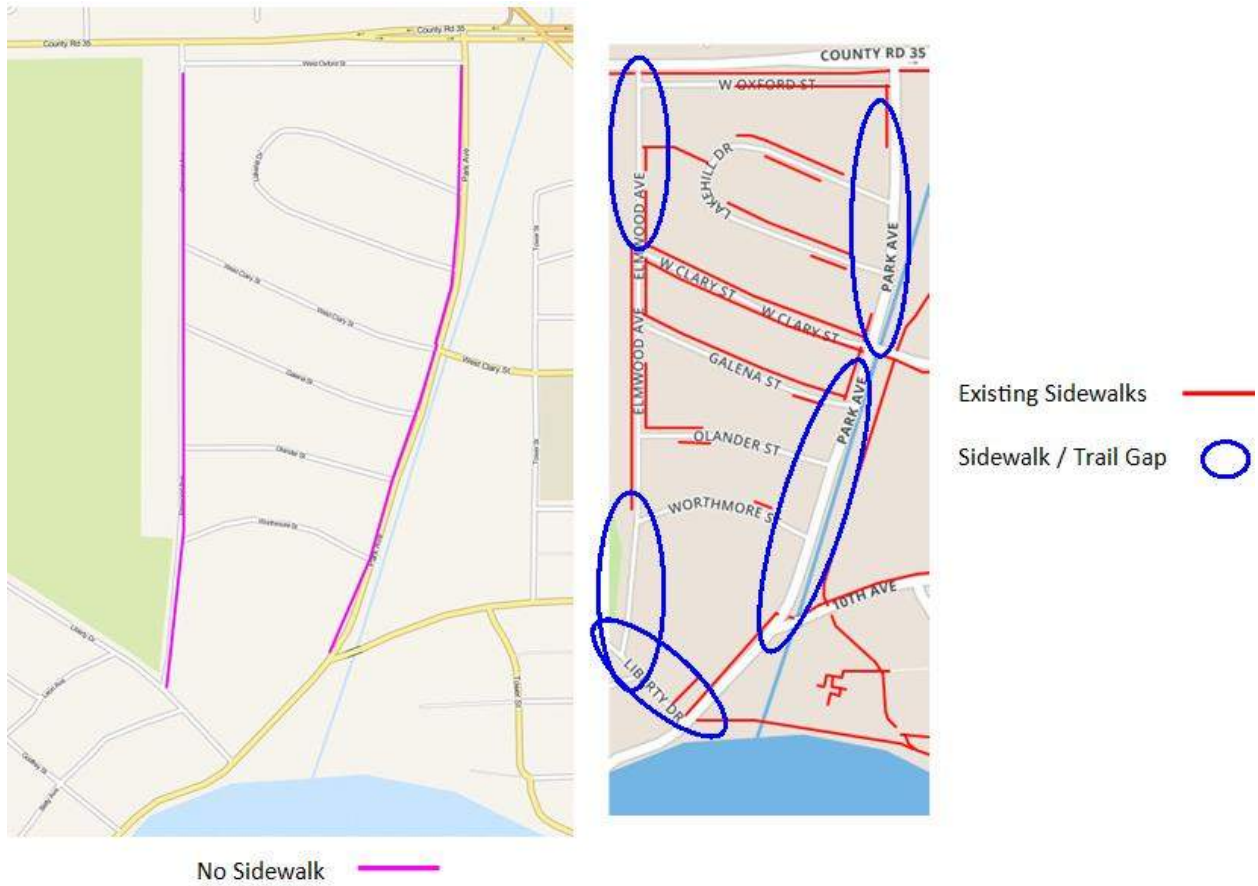


Table #14

College Way & Thompson Avenue Sidewalk Gap

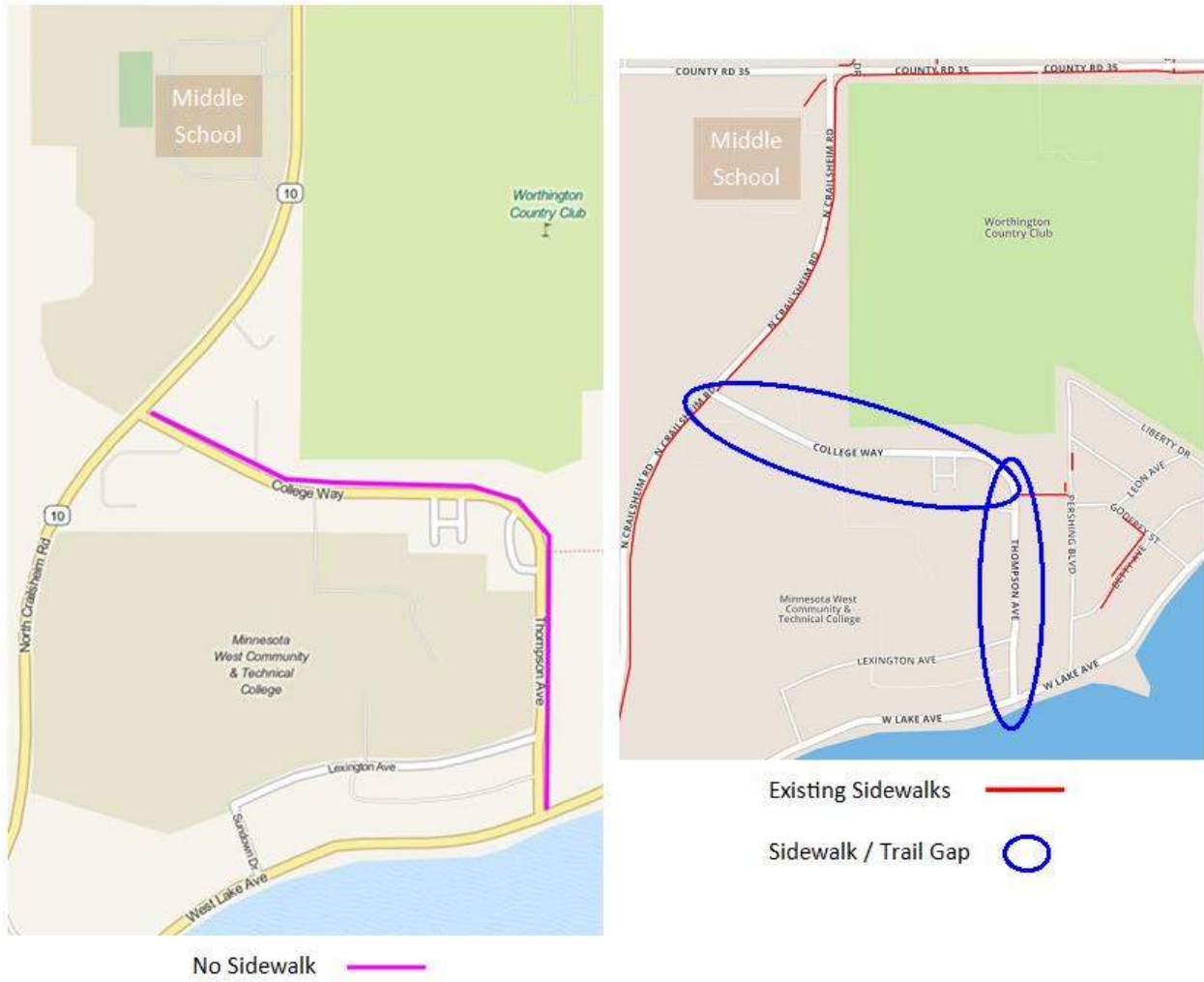


Table #15

Homewood Hills Sidewalks

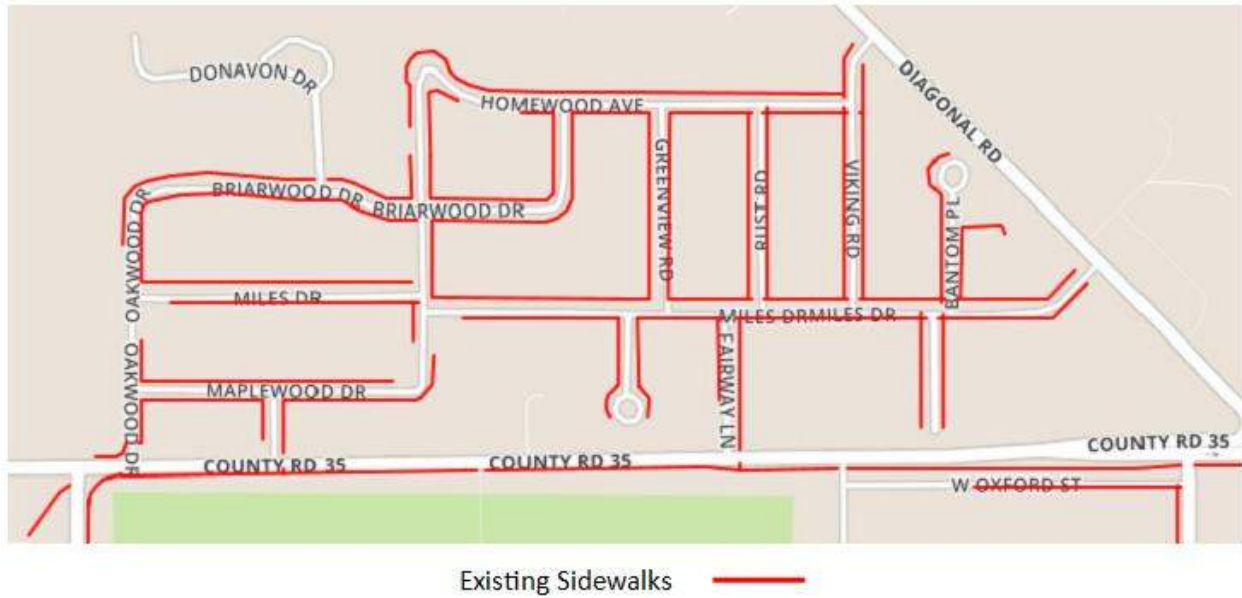


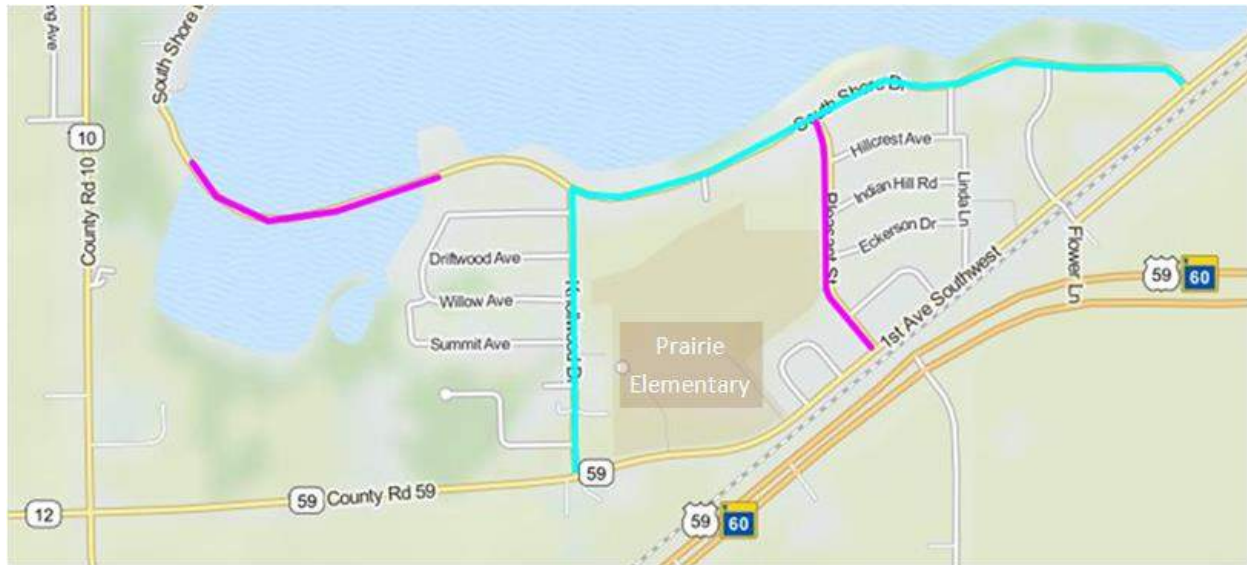
Table #16

South Lake Okabena - Existing Sidewalks



Table #17

South Lake Okabena - Route I'd Like to Use



No Sidewalk ———

Route I'd like to use ———

Table #18

Sally's Alley - Connectivity Issue



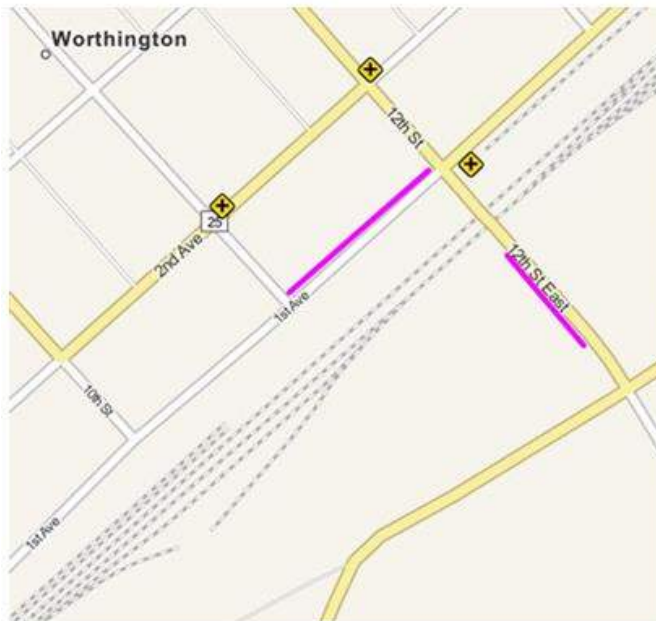
Sally's Alley—Connectivity Issue



Oakwood Drive—Example

Table #19

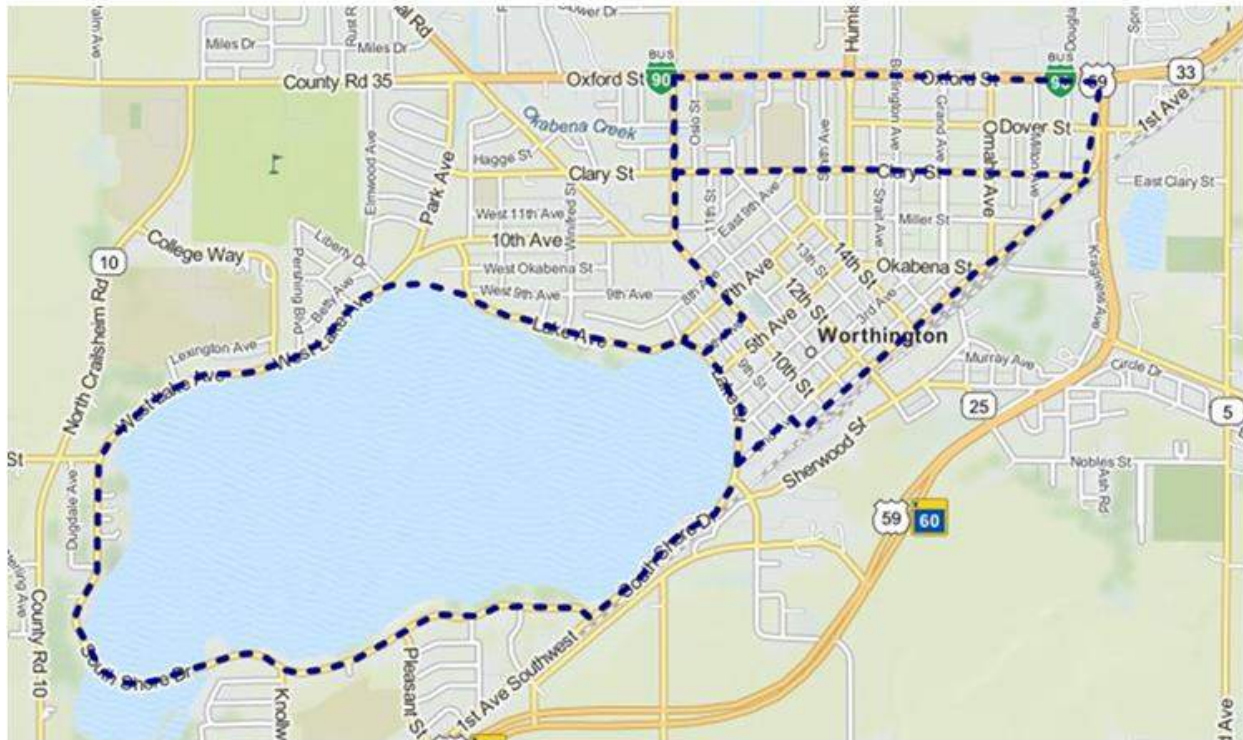
1st Avenue & 12th Street East Sidewalk Gap



1st Avenue

Table #20

Proposed On Street Bike Route

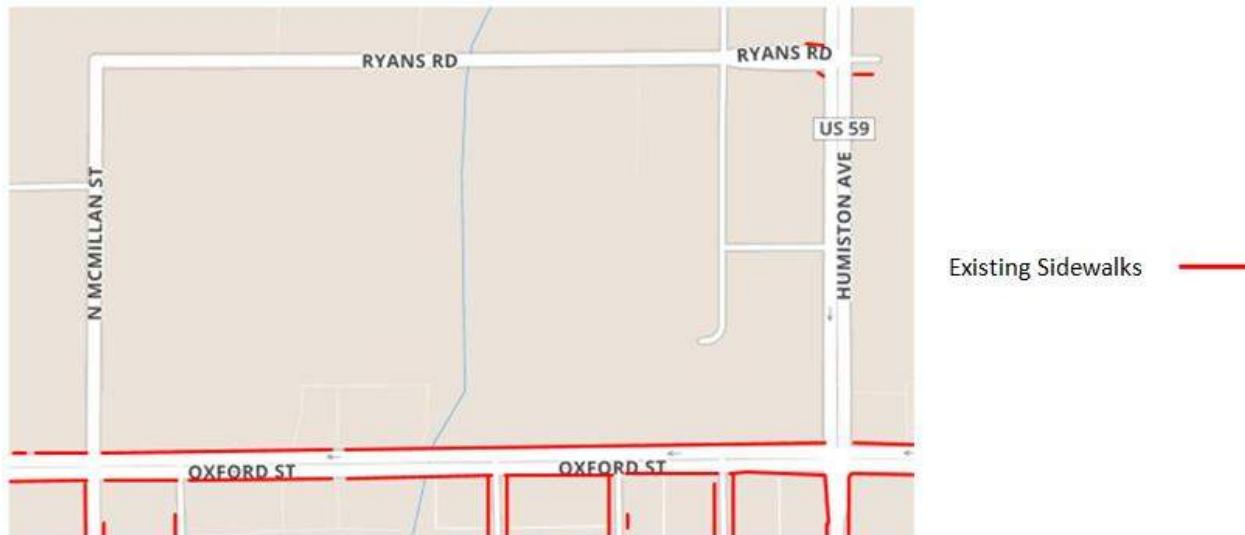


Proposed On Street Bike Route

Table #21 Ryan's Road to Oxford Street – Route I'd Like to Use



Table #22 Ryan's Road to Oxford Street – Sidewalk Map



Community Input – Ryan's Road to Oxford Street Corridor:

- This area is not safe for people who do not have a car.
- No sidewalk or trail to Wal-Mart - lots of foot traffic.
- Something needs to be done in this area to make it safer.
- Trail along Okabena Creek would be a safe route to business on Ryan's Rd.
- I should be able to walk to those businesses, but there is not a safe route for walkers now. The sidewalk on Oxford has light poles in the middle of them.

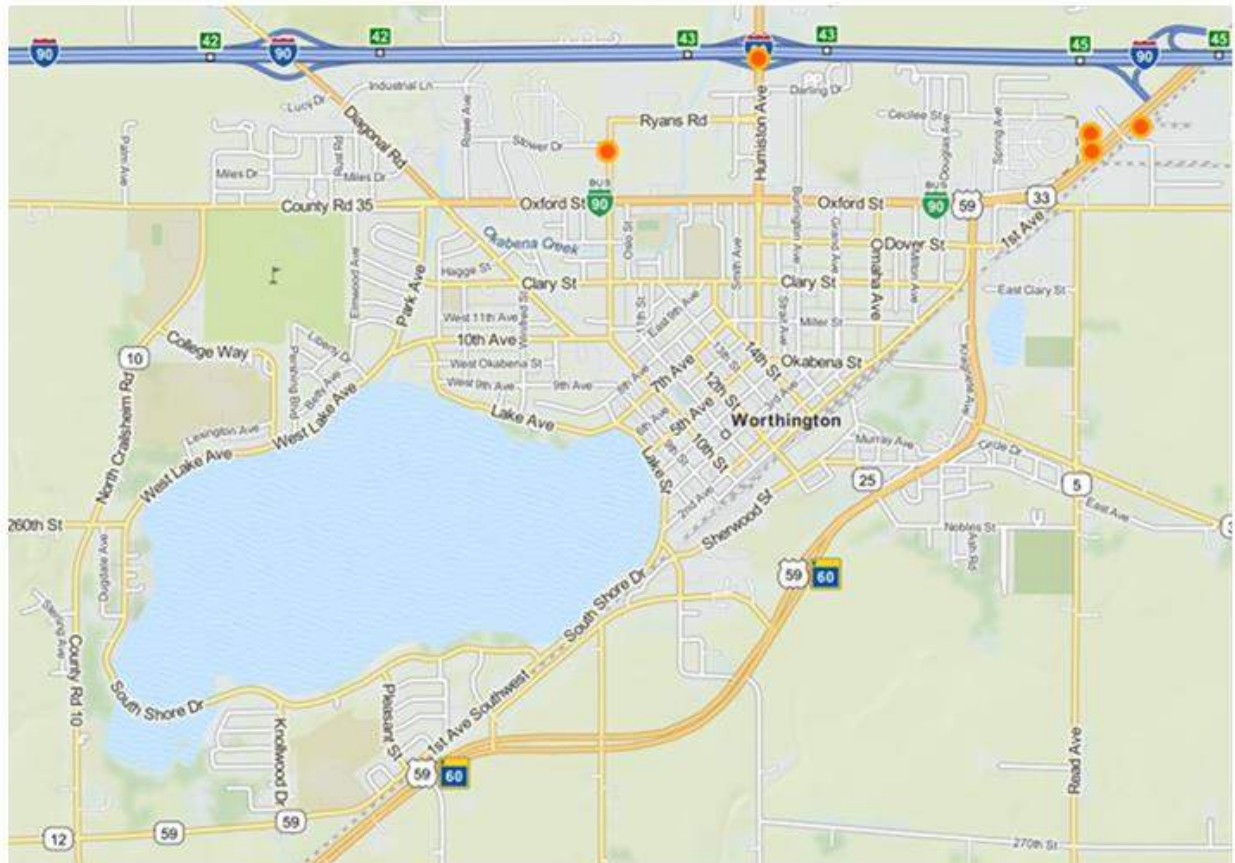
Other Pedestrian Issues


A number of other pedestrian issues were discussed as part of the Worthington Active Living Planning Process.

Problem Intersections:

- Park Avenue and 10th Avenue
- West Oxford Street and North Crailsheim Road
- Clary Street and McMillian Street
- This is not an exhaustive list

Table #23 Pedestrian Lighting Issues



Pedestrian Lighting Issue 

IV Goals & Strategies

Goals are general guidelines that explain what the City of Worthington wants to achieve. Strategies narrow the general guidelines and define in more detail how the goal will be achieved. Strategies are the actual steps to be taken to achieve the goals. A strategy may just be the first step, but the general need for the project is outlined.

The identified Goals and Strategies were created throughout the planning process with input from the community residents. It should be noted that not every existing issue identified within the Existing Conditions Chapter has a goal outlined below. Goals were only developed for certain existing conditions and some issues did not have a definite solution. Identifying the existing condition is the first step in working towards a solution.

The city council ranked the infrastructure projects and the non-infrastructure. This ranking or prioritization will help with directing time and money. Prioritization does not mean that the first goal has to be accomplished before moving onto another goal.

The purpose of the prioritization is to show that there was a discussion about the possible options and with unlimited resources this is what they would choose to accomplish first. Due to scarce resources, it may be necessary to start with a goal that has less upfront costs and is relatively easier to implement. The goals and strategies being outlined in the Worthington Active Living Plan are recommendations, so during implementation modifications can take place. Additional engineering work may also be needed before implementation can take place.

Goal

To enable the City of Worthington to become a more walkable community through strategic pedestrian improvements outlined in the Worthington Active Living Plan.

Objective

Identify destinations, gaps, areas of concern, and prioritize projects for addressing pedestrian infrastructure in the City of Worthington.

Proposal

Think about walkability and bikeability when considering decisions that influence the built environment.



Worthington Goals & Strategies

Complete Streets

Goal: Ensure all streets in Worthington are Complete Streets.

Strategy:

Implement a Pedestrian Functional Classification System.

When constructing a new street or resurfacing an existing street, consider and discuss the function of that street and the pedestrian amenities needed to make that street safe for pedestrians.

5 E(s): Engineering and Encouragement

Existing Conditions:

Some community members may not have access to a motor vehicle, so walking, biking, and transit are their primary transportation modes. Sidewalks and trails have a community benefit and help to increase connectivity within the community. If there are gaps in the sidewalk and trail network, it is not convenient to walk and bike.

Different streets require different pedestrian amenities. A Complete Street does not have a singular definition. A Complete Street is any street you feel safe walking or biking on. A Complete Street does not have to have a sidewalk on both sides of the street, but you have to consider all users when deciding if it is safe for pedestrians.

Younger children may need a sidewalk to separate them from vehicle traffic. Younger children may be learning how to ride a bike, so it is not safe for them to share the road with vehicle traffic. When making a decision whether a street needs sidewalks or not, the function of the street needs to be considered.

The City of Worthington adopted a Complete Streets Policy on March 25th, 2013. During the planning process for the Worthington Active Living Plan the Complete Streets Policy was discussed. The discussion centered on how to implement the Complete Streets Policy.

A Pedestrian Functional Classification System was discussed and proposed as one way to implement Worthington's Complete Streets Policy. A Pedestrian Functional Classification System would provide direction regarding consistence implementation of pedestrian infrastructure. Pedestrian infrastructure would be based on the function of the street, traffic volumes, and traffic speeds. Pedestrian infrastructure decisions will no longer be on a case by case basis.

The Worthington City Council should classify streets in Worthington, so a discussion will occur around the function of the street. Below are three classifications that can be used to identify the pedestrian amenities a street needs.

- ▶ Connector Streets
 - Connects primary destinations

- Highest traffic volumes streets
- Require the highest level of pedestrian amenities - Sidewalks on both sides of the street or a trail conveniently located along the corridor that connects key locations.
- ▶ Neighborhood Connector Streets
 - Connects Residential Streets to Connector Streets
 - Medium level traffic volume streets
 - Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other.
- ▶ Residential Streets
 - All other streets
 - Lower traffic speeds
 - Lower traffic volumes
 - Sidewalks are encouraged but there is no sidewalk requirement

New Developments

For new developments the developer should have to get an exemption from the Worthington City Council for why a sidewalk is not needed (street is wide, low traffic volumes, it is reasonable to walk on the street...).

Existing Sidewalks

To remove an existing section of sidewalk the landowner should be required to get approval from the Worthington City Council. This will help to prevent gaps in the sidewalk and trail network.

There was support at the Worthington Active Living Community Meetings for the Pedestrian Functional Classification System. This Pedestrian Functional Classification System was also discussed at the Worthington Traffic and Safety Committee Meeting on April 28, 2015. The Traffic and Safety Committee officially approved to move forward with the Pedestrian Functional Classification System.

It is recommended that City of Worthington Engineering and Planning staff finalize the Pedestrian Functional Classification Map with the Planning and Zoning Committee. This map will serve as a vision of sidewalk and trail implementation in the City of Worthington. Once the Pedestrian Functional Classification Map is finalized, the Pedestrian Functional Classification System will be presented to the

Worthington City Council. The Pedestrian Functional Classicization System will go through the adoption process and will be added as part of the Worthington Comprehensive Plan.

It is recommended that the Worthington Pedestrian Functional Classification System be implemented by need and project basis. There are Connector Streets and Residential Connector Streets within the City of Worthington that have no pedestrian infrastructure. It is not safe to walk or bike along these streets.

A community survey was conducted as part of the Worthington Active Living Plan. The survey asked community members to rank different sections of the city based on the general atmosphere, room to walk, and ease of crossing the street. The results of the community survey suggest that the first section of the city to address is A. Below is the map identifying the different areas of the city and survey results.

Table #24 A City of Worthington – Neighborhood Sections Map

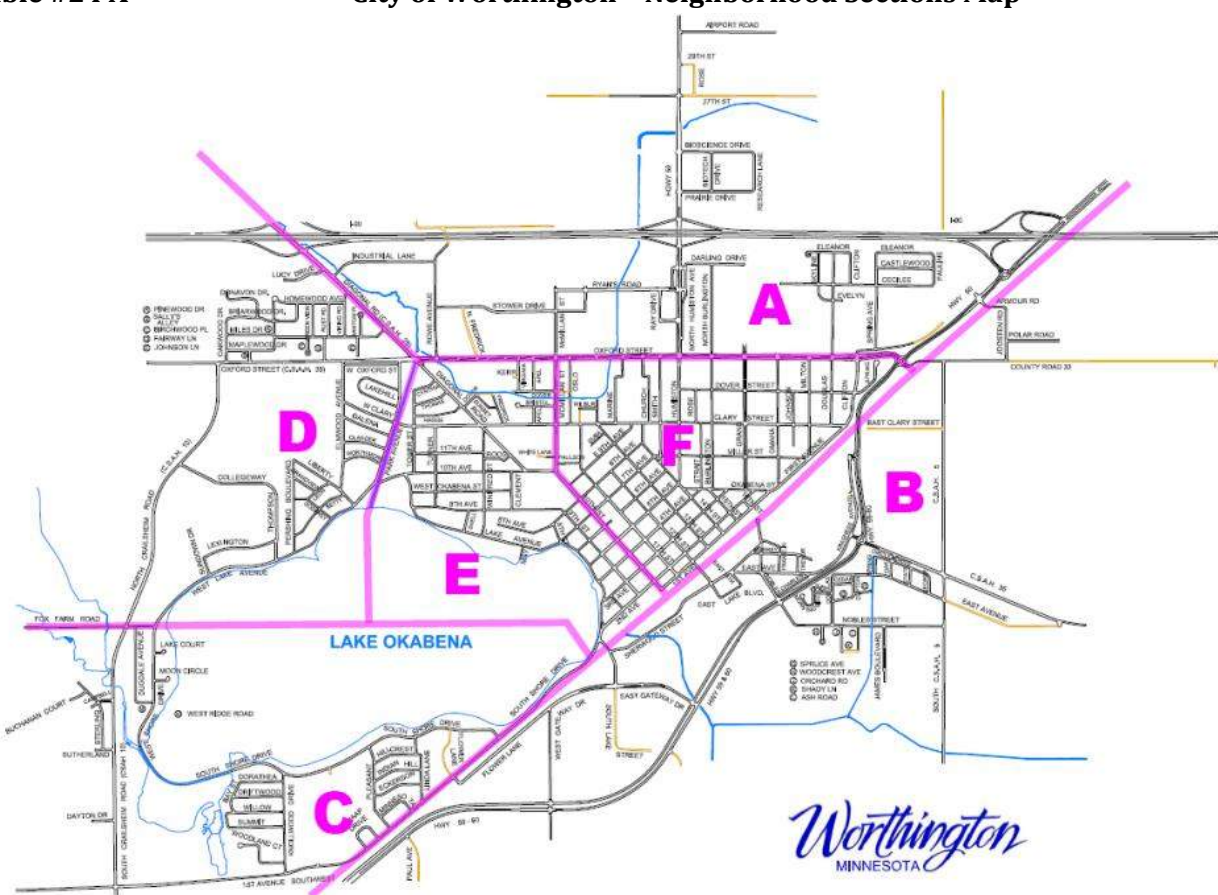


Table #24 B

Community Survey Summary

Section of City	1. General Atmosphere	2. Room to walk	3. Easy to cross street	4. Drivers behave well	5. Follow safety rules	Average 1,2,3
A	4	3	3	4	5	3
B	6	4	5	6	7	5
C	8	8	8	7	8	8
D	7	6	7	7	8	7
E	8	7	7	7	8	7
F	7	6	7	6	9	7
Average	7	6	6	6	8	

Worthington Infrastructure Goals & Strategies

Lake Okabena Loop Segment #1 – South Shore Drive

Goal: Increase pedestrian safety and access around Lake Okabena.

Strategy:

Long Term – Establish sidewalks along South Shore Drive, West Shore Drive, and West Lake Avenue.

Short Term – Paint fog lines and allow additional space for pedestrians and bicyclists.

Complete a safe pedestrian and bicyclist route around Lake Okabena.

5 E(s): Encouragement and Engineering

Existing Conditions:

Space is the main issue along Lake Okabena. There were multiple comments in the Community Surveys regarding safety along the lake and how it is only a matter of time before a pedestrian gets hit. Community input generally indicates that it is not safe to walk and bike on the road due to vehicle traffic and the narrower width of the streets.

The Streets around Lake Okabena have both residential traffic and through traffic. These streets function as Neighborhood Connectors. Separated pedestrian infrastructure is needed to provide a safe space for pedestrians. A continuous sidewalk or trail is needed to provide adequate pedestrian infrastructure.

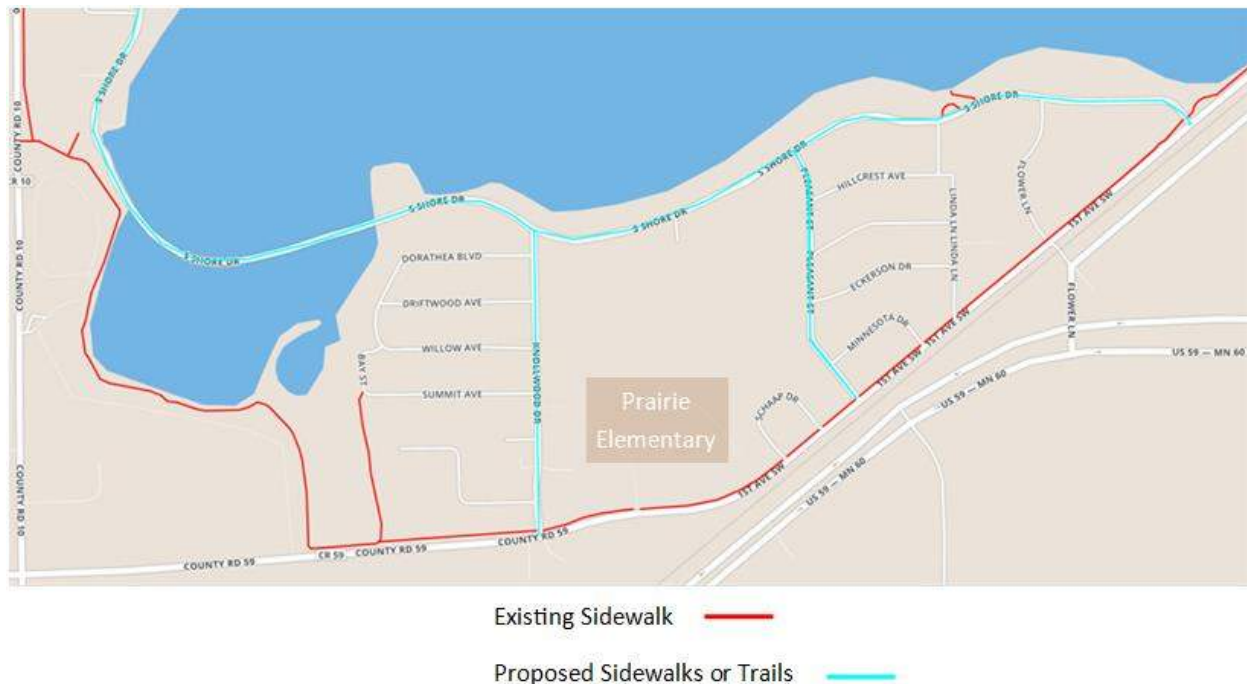
► Neighborhood Connector Streets

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets

- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other. (Refer to the Complete Streets Goal for a description of the function of different streets and the pedestrian amenities needed to make that street safe for pedestrians).

Table #25

Segment #1 – South Shore Drive



Short Term Option

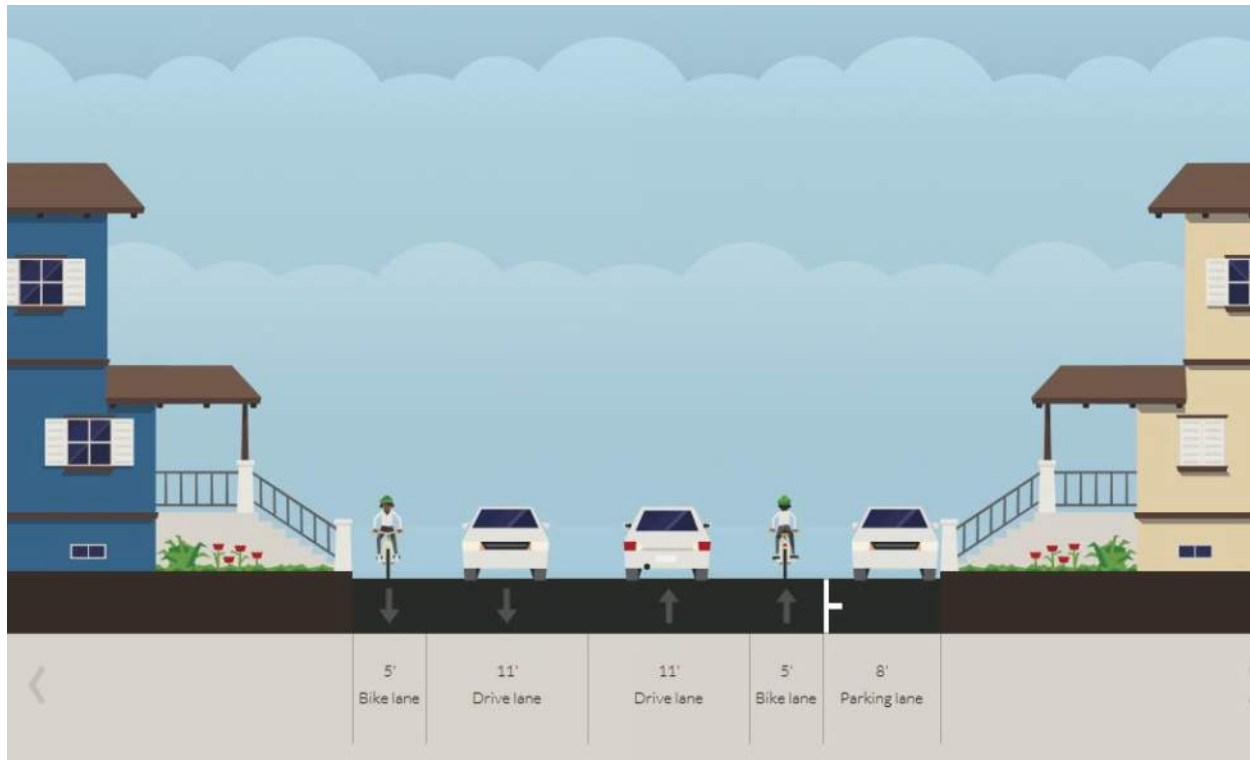
During the summer of 2014, local public health and the City of Worthington implemented a tactical urbanism demonstration project. The demonstration project showed how shifting the center line and eliminating parking on one side of the South Shore Drive could provide a safer space for pedestrians and bicyclists. Fog lines were painted to allow for more space for pedestrians and bicyclists. Below are two pictures of the demonstration project.

Table #26 Tactical Urbanism Demonstration Project – South Shore Drive



South Shore Drive is 40 feet wide. The road dimensions change as you drive around the lake, so the demonstration project as conducted on South Shore Drive will not be feasible all the way around the lake. Lane widths will have to change as you move clockwise around the lake. Below is a StreetMix example of South Shore Drive. Additional engineering work will be needed to finalize lane widths and parking.

Table #27 Example -South Shore Drive – 40' Width (11' Vehicle Travel Lane)



Our short term solution is to paint fog lines and establish an additional space for pedestrians and bicyclists. Since we are encouraging pedestrian and bicyclists to share the road with vehicle traffic,

traffic calming measures should be discussed. Vehicle traffic lane widths can help to calm traffic and create a safer environment for pedestrians and bicyclists. An 11 foot vehicle travel lane is one option and an 11 foot vehicle travel lane is narrower than most streets in Worthington. A 10 foot vehicle travel lane could also be discussed as an option to further calm traffic and increase pedestrian safety. A narrower travel lane acts as a traffic calming device by visually making the road feel smaller.

“Lane widths of 10 feet are appropriate in urban areas and have a positive impact on a streets safety without impacting traffic operation.”²¹

Lake Okabena Loop Segment #2 –Causeway / Grade

Goal: Increase safety for all users on the Causeway.

Strategy:

Implement one-way only traffic on the Causeway.

Complete a safe pedestrian and bicyclist route around Lake Okabena.

5 E(s): Encouragement and Engineering

Existing Conditions:

Space is the main issue along the Causeway. Currently, there are two travel lanes, a parking lane on the south side of the street, and virtually no shoulder on the north side of the street. There are walkers, bikers, people fishing, and motor vehicle traffic. A compromise is proposed to be made to make it safer for all users.

Public comments regarding the Causeway

- ▶ “Need bike/sidewalk around the grade”
- ▶ “Grade - This needs to be wider. When it's nice you have lots of foot/bicycle traffic along with everyone fishing here. Add all the people driving around the lake in the summer and it's very dangerous.”
- ▶ “Add a partitioned path the entire length of the grade.”
- ▶ “Every time I have to pass through this area, I think, ‘Are they going to wait until someone dies here to fix this?’ Please make improvements to allow for safe use of road by PEDs, cars, and people fishing. It's only a matter of time.”

²¹ NACTO. Urban Street Design Guide. Accessed: 6/18/15. Available: <http://nacto.org/usdg/street-design-elements/lane-width/>

- “Drivers are courteous but often have to swerve into the other lane to make room for us on the grade.”

Table #28**Causeway / Grade**

Making the Causeway a one-way street will help to increase safety for all users. Pedestrians will have space to walk and fish. There will be space for parking, so elderly community members can drive out onto the Causeway and park where they would like to fish.

Space for emergency vehicles will be less of an issue. There will no longer be a chance of getting backed up behind motor vehicles that are three wide. A current issue affecting emergency vehicles is having space to get through along the Causeway. An emergency vehicle can be blocked by a vehicle traveling the same direction, meeting another vehicle traveling the opposite direction, and a parked vehicle along the Causeway. Have three vehicles side by side leaves no extra room for a vehicle to pull over.

If the Causeway is one-way only, there will be more space for emergency vehicles. There is only the possibility of having two vehicles side by side with pedestrians. Pedestrians can easily step off of the pedestrian lane onto the parking lane between parked cars and on the shoulder and wait for emergency vehicles to pass.

It is recommended that one-way traffic flow be established on the grade. The direction of the one-way traffic has to be discussed. There will be a turnaround loop established in Slater Park or in the parking lot by the boat landing. The location of the turnaround loop depends which way the one-way traffic flows.

Table #29 A

One-way Turn Around Loop in Slater Park



Curb or Painted Turn Around Loop ———

Table #29 B

One-way Turn Around Loop by Boat Landing



Curb or Painted Turn Around Loop ———

Lake Okabena Loop Segment #3 – West Shore Drive (Causeway to Foxfarm Road)

Goal: Increase pedestrian safety and access around Lake Okabena.

Strategy:

Long Term – Establish sidewalks along South Shore Drive, West Shore Drive, and West Lake Avenue.

Short Term – Paint fog lines and allow additional space for pedestrians and bicyclists.

Complete a safe pedestrian and bicyclist route around Lake Okabena.

5 E(s): Encouragement and Engineering

Existing Conditions:

Space is the main issue along Lake Okabena. There were multiple comments in the Community Surveys regarding safety along the lake and how it is only a matter of time before a pedestrian gets hit. It is generally not safe to walk and bike on the road due to vehicle traffic and the narrower width of the streets.

The streets around Lake Okabena have both residential traffic and through traffic. These streets function as Neighborhood Connectors. Separated pedestrian infrastructure is needed to provide a safe space for pedestrians and bicyclists. A continuous sidewalk or trail is needed to provide adequate pedestrian infrastructure.

► **Neighborhood Connector Streets**

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other. (Refer to the Complete Streets Goal for a description of the function of different streets and the pedestrian amenities needed to make that street safe for pedestrians).

Short Term Option

During the summer of 2014, local public health and the City of Worthington implemented a tactical urbanism demonstration project. The demonstration project showed how shifting the center line and eliminating parking on one side of the South Shore Drive could provide a safer space for pedestrians and bicyclists. Fog lines were painted to allow for more space for pedestrians and bicyclists. Below are two pictures of the demonstration project.

Table #30 Tactical Urbanism Demonstration Project – South Shore Drive



West Shore Drive is 42.5 feet wide from the Causeway to Foxfarm Road. The road dimensions change as you drive around the lake, so what will work along South Shore Drive will not be feasible all the way around the lake. Lane widths will have to change as you move clockwise around the lake. Below is a StreetMix example of West Shore Drive. Additional engineering work will be needed to finalize lane widths and parking.

Table #31 Example -West Shore Drive – 42.5' Width (11' Vehicle Travel Lane)



Lake Okabena Loop Segment #4 – West Shore Drive (Foxfarm Road to Sundown Drive)

Goal: Increase pedestrian safety and access around Lake Okabena.

Strategy:

Long Term – Establish sidewalks along South Shore Drive, West Shore Drive, and West Lake Avenue.

Complete a safe pedestrian and bicyclist route around Lake Okabena.

*5 E(s): Encouragement and Engineering**Existing Conditions:*

Space is the main issue along Lake Okabena. There were multiple comments in the Community Surveys regarding safety along the lake and how it is only a matter of time before a pedestrian gets hit. It is generally not safe to walk and bike on the road due to vehicle traffic and the narrower width of the streets.

The Streets around Lake Okabena have both residential traffic and through traffic. These streets function as Neighborhood Connectors. Separated pedestrian infrastructure is needed to provide a safe space for pedestrians. A continuous sidewalk or trail is needed to provide adequate pedestrian infrastructure.

► Neighborhood Connector Streets

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other. (Refer to the Complete Streets Goal for a description of the function of different streets and the pedestrian amenities needed to make that street safe for pedestrians).

Short Term Option

West Shore Drive is 23.5 feet wide with four foot paved shoulders from Foxfarm Road to Sundown Drive. The road dimensions change as you drive around the lake, so what will work along West Shore Drive will not be feasible all the way around the lake. This section of the loop does not have a short term solution.

West Shore Drive from Foxfarm Road to Sundown Drive does have a five foot paved shoulder. Without a complete road reconstruct there are no short term painting solutions. Below is a picture of this segment of the loop around the lake.

Table #32

Foxfarm Road to Sundown Drive – paved shoulder

*Lake Okabena Loop Segment #5 – West Shore Drive (Sundown Drive to Thompson Avenue)*

Goal: Increase pedestrian safety and access around Lake Okabena.

Strategy:

Long Term – Establish sidewalks along South Shore Drive, West Shore Drive, and West Lake Avenue.

Short Term – Paint fog lines and allow additional space for pedestrians and bicyclists.

Complete a safe pedestrian and bicyclist route around Lake Okabena.

5 E(s): Encouragement and Engineering

Existing Conditions:

Space is the main issue along Lake Okabena. There were multiple comments in the Community Surveys regarding safety along the lake and how it is only a matter of time before a pedestrian gets hit. It is generally not safe to walk and bike on the road due to vehicle traffic and the narrower width of the streets.

The Streets around Lake Okabena have both residential traffic and through traffic. These streets function as Neighborhood Connectors. Separated pedestrian infrastructure is needed to provide a safe space for pedestrians. A continuous sidewalk or trail is needed to provide adequate pedestrian infrastructure.

► **Neighborhood Connector Streets**

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other. (Refer to the Complete Streets Goal for a description of the function of different streets and the pedestrian amenities needed to make that street safe for pedestrians).

Short Term Option

During the summer of 2014, local public health and the City of Worthington implemented a tactical urbanism demonstration project. The demonstration project showed how shifting the center line and eliminating parking on one side of the South Shore Drive could provide a safer space for pedestrians and bicyclists. Fog lines were painted to allow for more space for pedestrians and bicyclists. Below are two pictures of the demonstration project.

Table #33 Tactical Urbanism Demonstration Project – South Shore Drive



West Shore Drive is 42.5 feet wide from Sundown Drive to Thompson Avenue. The road dimensions change as you drive around the lake, so what will work along South Shore Drive will not be feasible all the way around the lake. Lane widths will have to change as you move clockwise around the lake. Below is a StreetMix example of South Shore Drive. Additional engineering work will be needed to finalize lane widths and parking.

Table #34 Example -West Shore Drive – 42.5' Width (11' Vehicle Travel Lane)



Lake Okabena Loop Segment #6 – West Shore Drive (Pershing Boulevard to Liberty Drive)

Goal: Increase pedestrian safety and access around Lake Okabena.

Strategy:

Long Term – establish sidewalks along South Shore Drive, West Shore Drive, and West Lake Avenue.

Short Term – Paint fog lines and allow additional space for pedestrians and bicyclists.

Complete a safe pedestrian and bicyclist route around Lake Okabena.

5 E(s): Encouragement and Engineering

Existing Conditions:

Space is the main issue along Lake Okabena. There were multiple comments in the Community Surveys regarding safety along the lake and how it is only a matter of time before a pedestrian gets hit. It is

generally not safe to walk and bike on the road due to vehicle traffic and the narrower width of the streets.

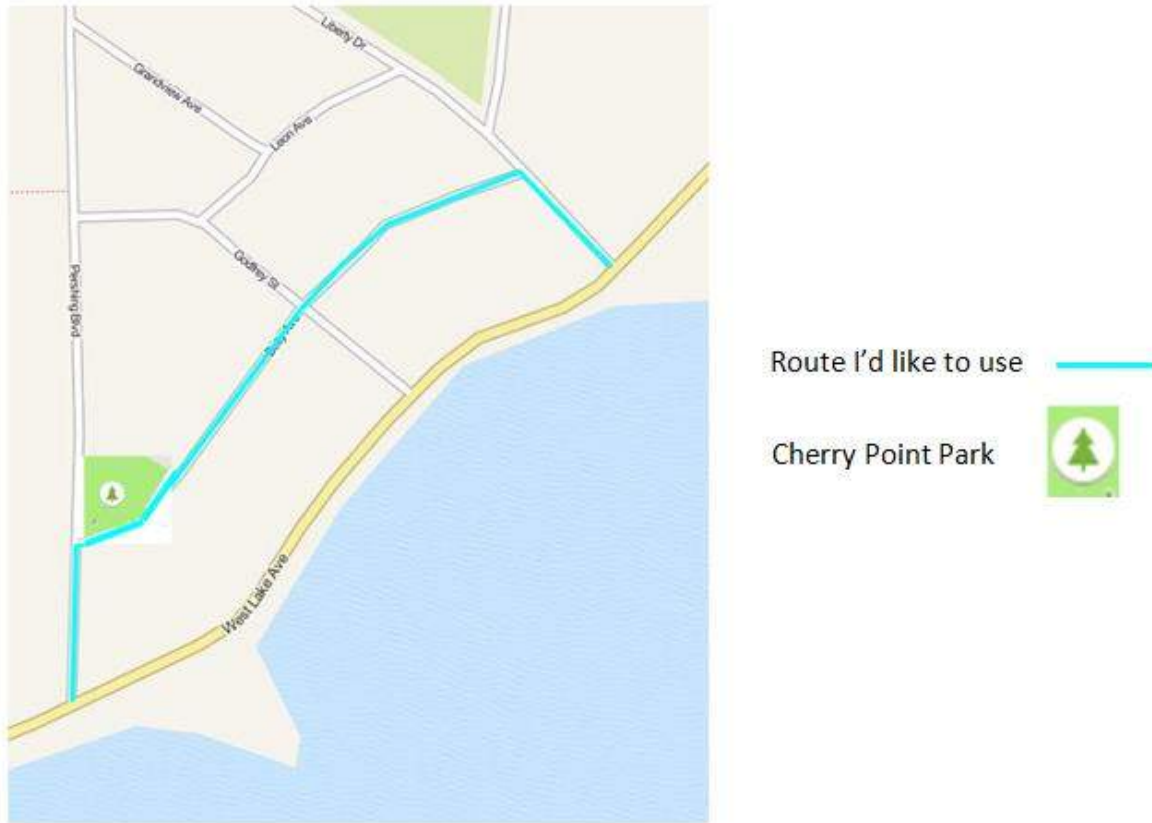
The Streets around Lake Okabena have both residential traffic and through traffic. These streets function as Neighborhood Connectors. Separated pedestrian infrastructure is needed to provide a safe space for pedestrians. A continuous sidewalk or trail is needed to provide adequate pedestrian infrastructure.

► Neighborhood Connector Streets

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other. (Refer to the Complete Streets Goal for a description of the function of different streets and the pedestrian amenities needed to make that street safe for pedestrians).

Possible Sidewalk Route

Due to limited space along West Lake Avenue, a possible sidewalk route could be to connect to Cherry Point Park. This would provide a convenient resting place along the western stretch of the loop around the lake. Below is the possible route. A walk does appear to be feasible on the south side of West Lake Avenue from just east of Pershing Boulevard to Liberty Drive. The most difficult segment to fit a walk in is between Thompson Avenue and Pershing Boulevard.

Table #35**West Lake Avenue – Route I'd Like to Use***Short Term Option*

During the summer of 2014, local public health and the City of Worthington implemented a tactical urbanism demonstration project. The demonstration project showed how shifting the center line and eliminating parking on one side of the South Shore Drive could provide a safer space for pedestrians and bicyclists. Fog lines were painted to allow for more space for pedestrians and bicyclists. Below are two pictures of the demonstration project.

Table #36**Tactical Urbanism Demonstration Project – South Shore Drive**

West Shore Drive is 32 feet wide from Pershing Boulevard to Liberty Drive. The road dimensions change as you drive around the lake, so what will work along West Shore Drive will not be feasible all the way around the lake. Lane widths will have to change as you move clockwise around the lake. Below is a StreetMix example of South Shore Drive. Additional engineering work will be needed to finalize lane widths and parking.

Table #37 **Example -West Shore Drive – 32’ Width (11’ Vehicle Travel Lane)**



Lake Okabena Loop Segment #7 – Centennial Park (10 Avenue)

Goal: Increase pedestrian safety and access around Lake Okabena.

Strategy:

Complete a safe pedestrian and bicyclist route around Lake Okabena.

Establish a trailhead at Centennial Park.

5 E(s): Encouragement, Education, and Engineering

Existing Conditions:

There is a sidewalk and trail in Centennial Park. The sidewalk starts by the intersection of West Lake Avenue and Liberty Street and navigates along the lake. The trail runs along Whisky Ditch and connects to the sidewalk along Lake Avenue.

The City of Worthington is developing plans to improve the bridge crossing on 10th Avenue by the intersection with Park Avenue. Part of this project is improving the trail crossing and pedestrian infrastructure connections. It is recommended to create a trailhead in Centennial Park that connects the two trails and provides information regarding walking, biking, and destinations in Worthington.

It is essential that trail users and potential trail users have access to information regarding trails to enhance their experience. Trail information can be disseminated in a wide variety of formats, including kiosks, brochures, websites, guidebooks, and on-trail signs and blazes. Trail and trailhead signage is an indispensable part.

A brochure is also useful in directing people to other attractions in the City of Worthington.

A trailhead in Centennial Park will help to promote the trail along Whisky Ditch and the loop around the lake. As part of the trailhead a kiosk will be built. The kiosk will display promotional brochures for city parks, downtown businesses, walking routes, Olson Campground, and other annual events in Worthington, like the National Windsurf Regatta.

A balance regarding the appropriate level of signage must be reached between providing adequate signage for users to find their way and avoiding sign pollution. The objectives of trail signing are to:

- Improve the trail user experience
- Attract new trail users
- Enhance the safety of pedestrians
- Increase comfort and confidence in navigating the trail or trail network
- Promote recreational trail use;

Table #38

Trail Head Kiosk Examples



Lake Okabena Loop Segment #8 – Lake Avenue

Goal: Increase pedestrian safety and access around Lake Okabena.

Strategy:

Complete a safe pedestrian and bicyclist route around Lake Okabena.

Improve pedestrian crossing along Lake Avenue and Lake Street.

5 E(s): Encouragement, Education, and Engineering

Existing Conditions:

There are existing sidewalks along Lake Avenue and Lake Street that connect to the trail along South Shore Drive. This is where a number of users access the trail. Lake Street is only two blocks from the downtown business district, so this is a very busy area for pedestrians and motor vehicles.

The intersections along Lake Street from 7th Street to 2nd Avenue should be treated the same as intersections in the downtown area. Crosswalks along Lake Street from 7th Street to 2nd Avenue should be established and maintained at the same level as crosswalks in the downtown area. This will help to increase safety and access to the trail and loop around the lake.

Table #39 Lake Avenue & Lake Street – Existing Sidewalks

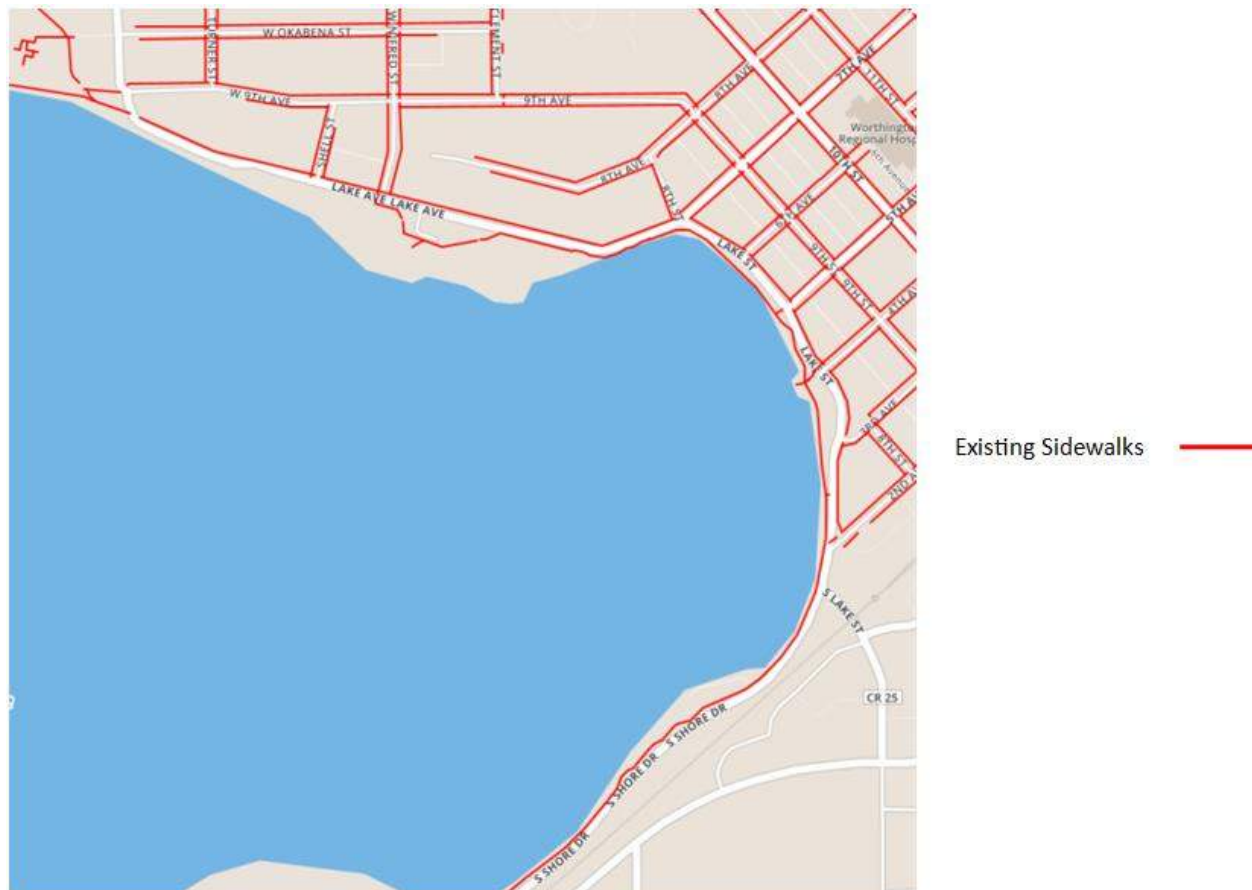


Table #40 B

Prairie Elementary – Pedestrian Connections

*Curb Extensions*

Goal: Increase safety at higher pedestrian volume intersections.

Strategy:

Research curb extensions as a possibility to increase safety at higher traffic volume intersections.

Install a curb extension on the west side of Humiston Avenue at the intersection with Clary Street.

Install curb extensions on Diagonal Road at the intersection with Clary Street.

Install curb extensions in the downtown business district.

Add landscape to curb extensions to increase neighborhood feel and the aesthetic characteristics of the City of Worthington.

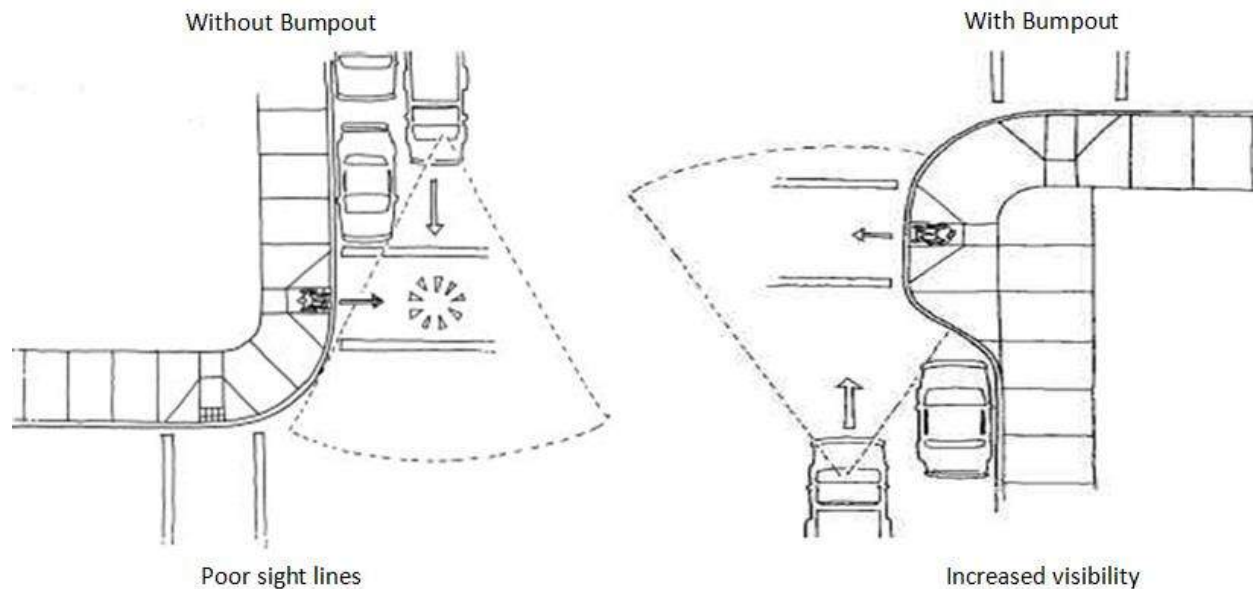
5 E(s): Engineering and Encouragement

Existing Conditions:

Curb extensions are a traffic calming device that helps to slow traffic speeds, increase visibility, and reduce crossing times. Curb extensions narrow the street crossing distance for pedestrians. This helps

to increase safety at the crossing, since pedestrians are in a better position to look past parked vehicles to see oncoming traffic.

Table #41 Bumpout (Curb Extension) Visibility Comparison



Curb extensions can be permanent or removable. Removable curb extensions can be put in to test their effectiveness of slowing traffic and making the environment safer for pedestrians. Removable curb extensions give the city time to evaluate this action before installing permanent curb extension. If the removable curb extension is effective, a more permanent curb extension can be installed.

Curbs and storm water drainage do not have to be altered with removable curb extensions. Rebuilding curbs and modifying storm water drainage can be very expensive. A removable curb extension is an add-on to the existing street.

Installing removable curb extensions, in the form of planters, not only makes it safer for pedestrians, but they help to make the street more aesthetically pleasing. During the summer months these planters will be green and in winter months the planters can be removed for easy snow removal. The area within the bump-out and the crosswalk can also be dyed red to make the area stand out.

Table #42 A Removable Curb Extension - Planters



Table #42 B**Removable Curb Extension***Humiston Avenue and Clary Street*

Humiston Avenue has been identified as a higher risk area due to the higher traffic volumes. The intersection of Humiston Avenue and Clary Street is a higher pedestrian traffic area, since it is a primary crossing to get to the Worthington High School. A curb extension on the west side of Humiston Avenue will help to make the crossing safer for pedestrians. An example of a curb extension on only one side of the street is below.

Table #42 C**Curb Extension –One Side Only**

22

²² City of Shoreview, MN. Accessed: 6/18/15. Available:
<http://www.shorelinewa.gov/government/departments/public-works/transportation-services/traffic-calming-and-pedestrian-bicycle-safety-in-shoreline>

Diagonal Road and Clary Street

Diagonal Road has been identified as a higher risk area due to the higher traffic volumes. The intersection of Diagonal Road and Clary Street is especially difficult for vehicle traffic and pedestrians, since Winfred Street also contributes to congestion in this area. Winfred Street intersects with Diagonal Road just south of Clary Street. Curb extensions in this area could help to increase safety for pedestrians.

Downtown Worthington

Downtown Worthington is a very busy area with higher volumes of vehicle traffic and pedestrian traffic. There are four way stops at a number of intersections in the downtown area, but crossing the street can be difficult for pedestrians. Streets in the downtown business district are wide and on street parking can reduce visibility. Curb extensions will decrease crossing times and will help to increase visibility.

Landscaping

Curb extensions can help to increase the aesthetic characteristic of the City. Curb extensions can also provide a place to increase drainage for water. It is recommended that different landscape designs be considered as part of curb extension projects. Refer to the goal Adopt a Curb Extension for information regarding the maintenance of curb extension landscaping. Below are examples of landscaping that can be considered.

Table #42 D Curb Extension – Landscaping Examples



Oxford-59 – I-90 Corridor

Goal: Increase pedestrian safety in the Oxford – 59 – I-90 Corridor

Strategy:

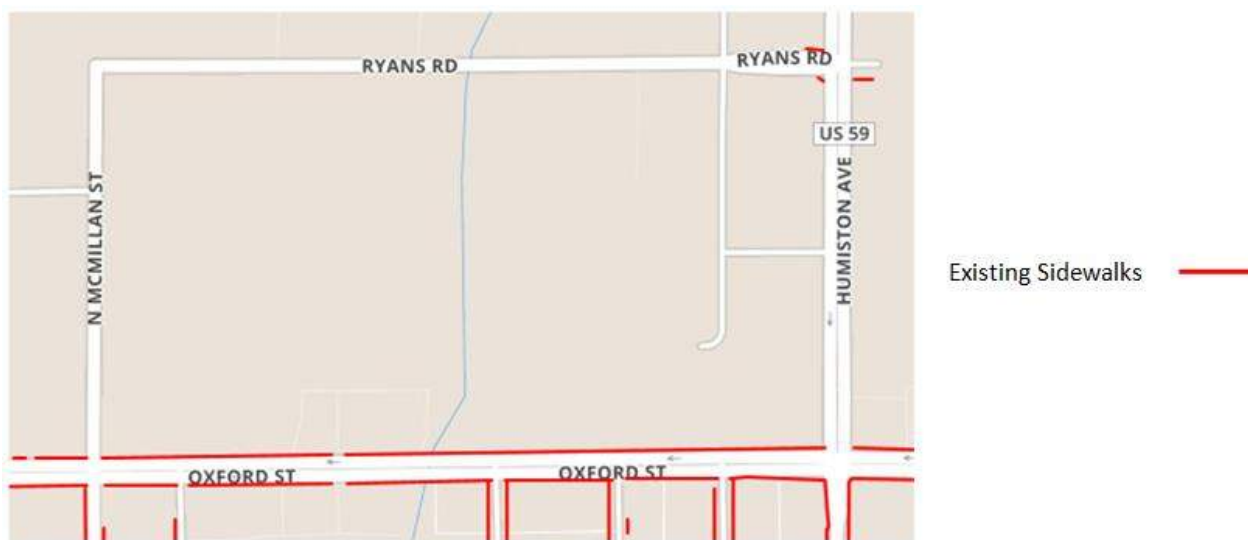
Add pedestrian infrastructure to the Oxford – 59 – I-90 Corridor

5 E(s): Engineering and Encouragement

Existing Conditions:

The Oxford – 59 – I-90 Corridor is the area between Oxford Street, Humiston Avenue (Highway 59), Interstate 90, and McMillian Street. This area was developed to serve motor vehicle traffic only. There is little or no pedestrian infrastructure in this corridor. There is no pedestrian connectivity in this corridor. Below is a map of the existing pedestrian infrastructure around the Oxford – 59 – I-90 Corridor.

Table #43 Oxford – 59 – I-90 Corridor – Existing Sidewalks



During the spring of 2015, Short Elliott Hendrickson Inc. (SEH) completed a Pedestrian and Trail Assessment for the Oxford – 59 – I-90 Corridor. The assessment outlined multiple possible paint and pedestrian infrastructure projects. Since there are multiple options for adding pedestrian infrastructure in this corridor, a public meeting is needed to finalize the plan regarding pedestrian infrastructure in the Oxford – 59 – I-90 Corridor.

Through the Planning Process for the Worthington Active Living Plan, there was support for adding pedestrian infrastructure in the Oxford – 59 – I-90 Corridor. The Community Survey highlighted that Section A, the area north of Oxford Street, was severely underserved in regards to pedestrian infrastructure. Section A averaged a three out of 10 for the survey categories: general atmosphere, did you have room to walk, and was it easy to Cross Street. The results of the community survey suggest

that the first section of the city to address is A. Below is the map identifying the different areas of the city and survey results.

Table #44 A City of Worthington – Neighborhood Sections Map

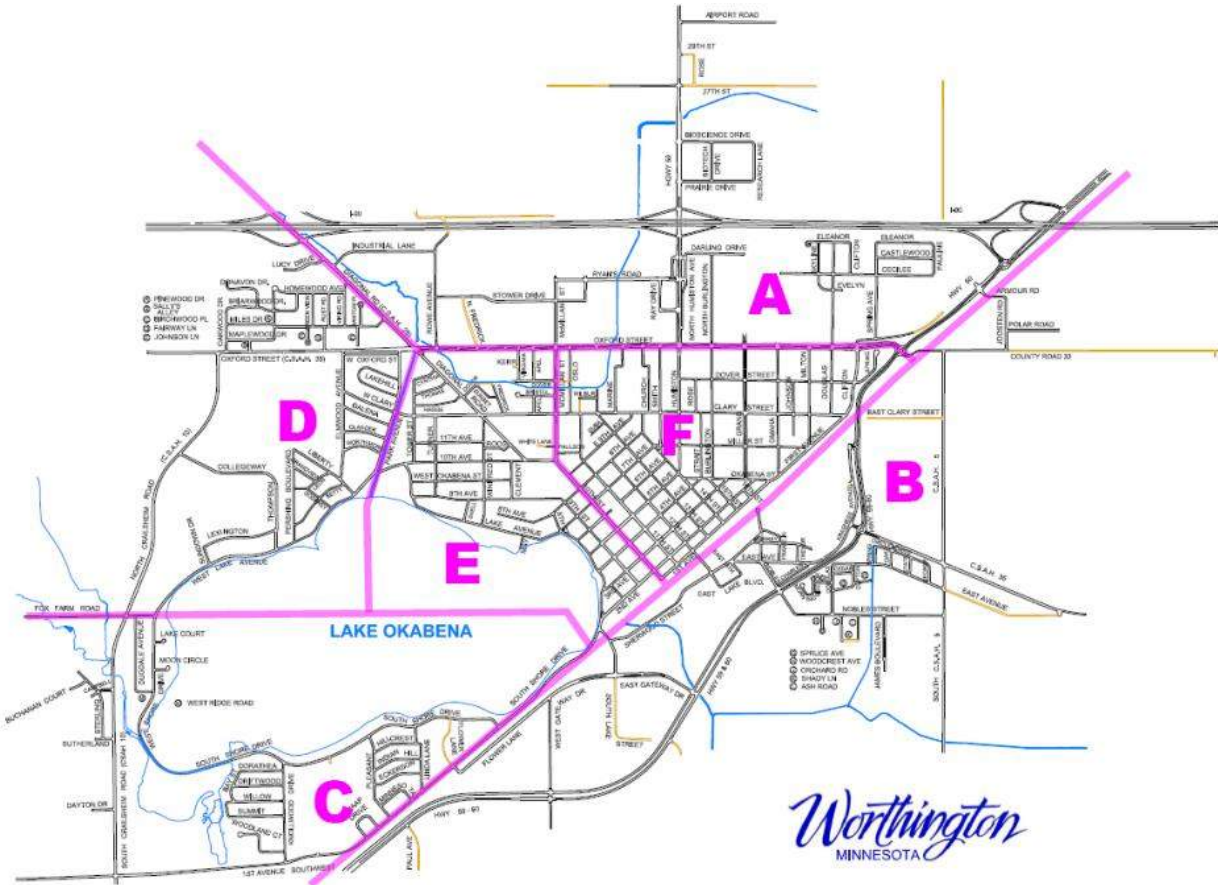


Table #44 B Community Survey Summary

Section of City	1. General Atmosphere	2. Room to walk	3. Easy to cross street	4. Drivers behave well	5. Follow safety rules	Average 1,2,3
A	4	3	3	4	5	3
B	6	4	5	6	7	5
C	8	8	8	7	8	8
D	7	6	7	7	8	7
E	8	7	7	7	8	7
F	7	6	7	6	9	7
Average	7	6	6	6	8	

One of the main themes of the Worthington Active Living Plan Planning Process was:

- Section A of Worthington is underserved in regards to pedestrian infrastructure (Oxford – 59 – I-90 Corridor).

There are a number of community members in Worthington whose primary modes of transportation are walking and biking. When discussing and analyzing primary destinations in the City of Worthington, a number of the primary destinations are in the Oxford – 59 – I-90 Corridor. Community members are walking in the Oxford – 59 – I-90 Corridor, but it is not safe or convenient. Community members are walking on shoulders, on boulevards that are covered by snow in the winter, and between destinations through parking lots and open greenspace.

Oxford Street, Humiston Avenue (Highway 59), and McMillian Street all function as a Connector Street or a Neighborhood Connector. There are a number of destinations in the Oxford – 59 – I-90 Corridor that are traffic generators, so some of the highest traffic volume streets in Worthington are in this corridor. It is generally not safe to walk and bike on the road in the Oxford – 59 I-90 Corridor due to vehicle traffic and traffic speeds. Below is a description of a Connector Street and Neighborhood Connector.

▶ Connector Streets

- Connects primary destinations
- Highest traffic volumes streets
- Require the highest level of pedestrian amenities - Sidewalks on both sides of the street or a trail conveniently located along the corridor that connects key locations.

▶ Neighborhood Connector Streets

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other.

Safety for pedestrians is the primary concern. Pedestrian connections need to be made to Walmart, Fareway Grocery, and other destinations. Community members are walking and biking, so it is only a matter of time before there is a serious incident.

Public input throughout the Planning Process for the Worthington Active Living Plan shows that there is a need for safe pedestrian connections to destinations in the Oxford – 59 I-90 Corridor. There has been an engineering study showing possible pedestrian infrastructure improvements. The Worthington City Council, Community Development Department Staff, Engineering Department Staff, and Public Works Staff need to hold a public hearing to discuss possible pedestrian infrastructure improvements to make

Cecilee Street Connection

Goal: Improve walkability in the neighborhood around Cecilee Street.

Strategy:

Establish sidewalks on Neighborhood Connectors in the Cecilee Street neighborhood.

Create a sidewalk or trail connection between Sungold Heights Mobile Home Park and the trail along Oxford Street.

5 E(s): Encouragement and Education

Existing Conditions:

There are currently no sidewalk or trail connections between the sidewalks along Oxford Street and the neighborhoods around Cecilee Street. There are a number of residents who live in the neighborhoods around Cecilee Street who walk on the street to get to work and to other destinations along Oxford Street. Cecilee Street, Douglas Avenue, and Spring Avenue all function as Neighborhood Connectors. It is generally not safe to walk or bike on Neighborhood Connectors.

► **Neighborhood Connector Streets**

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other.

Douglas Avenue and Spring Avenue are the only two routes into the neighborhoods around Cecilee Street. Cecilee Street is the main connection to the majority of residential streets in this area. Cecilee Street, Douglas Avenue, and Spring Avenue have higher traffic volumes and traffic speeds, since they function as Neighborhood Connector Streets. There are also a number of businesses in this area that generate semi-truck traffic.

Cecilee Street, Douglas Avenue, and Spring Avenue are narrower streets, so there is not a safe place for pedestrians. A sidewalk is needed to create a safe place for pedestrians. A sidewalk would provide a safe and convenient place for pedestrians.

“North Douglas Avenue at Oxford by Jay Cox is a very busy intersection,” Community Survey.

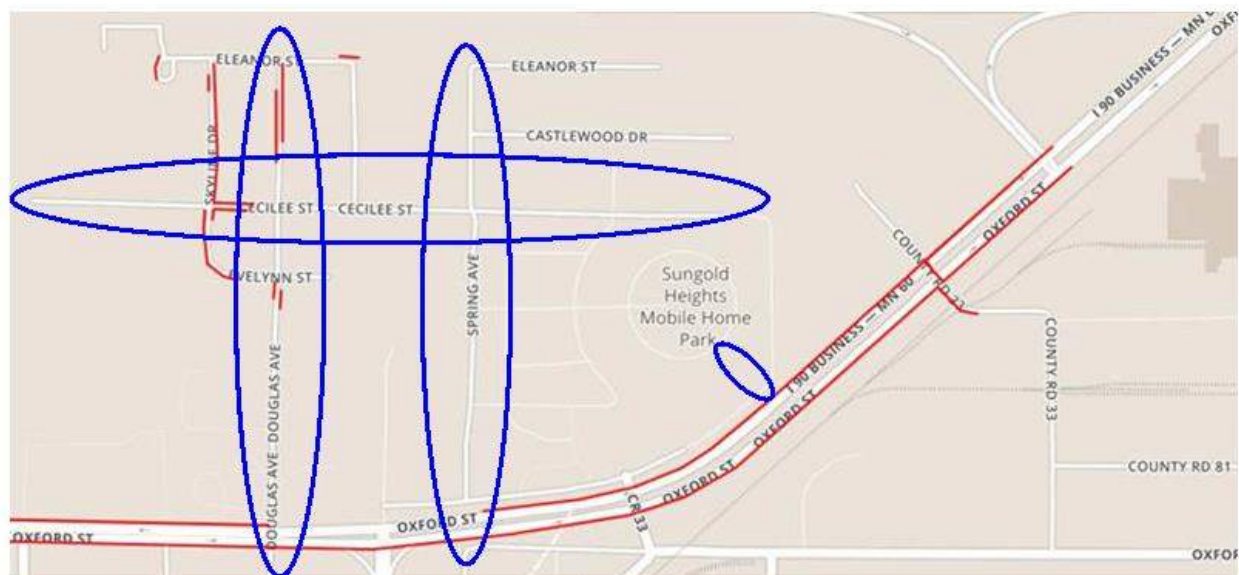
“Some drivers coming out of Jay Cox don’t even look,” Community Survey.

Table #46 Douglas Avenue – Narrow Street



There are a number of destinations within walking distance of Cecilee Street. JBS is one of these destinations. There is a trail leading to JBS, but a convenient connection is needed. A connection between Sungold Heights Mobile Home Park and the trail leading to JBS would vastly increase the connectivity in this area. Below is a map outlining the key sidewalk gaps in the Cecilee Street neighborhoods.

Table #47 Cecilee Street Sidewalk Connection



Sidewalk / Trail Gap ○

Oxford Street & Diagonal Road Sidewalk Connection

Goal: Improve walkability along Oxford Street.

Strategy:

Fill in sidewalks along Oxford Street leading to Diagonal Road and the trail along West Oxford Street.

5 E(s): Engineering and Encouragement

Existing Conditions:

Oxford Street and Diagonal Road are two of the busiest streets in Worthington. Oxford Street and Diagonal Road both function as Connector Streets. Connector Streets require the highest level of pedestrian infrastructure, since it is generally not safe to walk and bike on the street.

► Connector Streets

- Connects primary destinations
- Highest traffic volumes streets
- Require the highest level of pedestrian amenities - Sidewalks on both sides of the street or a trail conveniently located along the corridor that connects key locations.

Below is a map outlining the sidewalk gaps along Oxford Street and Diagonal Road. These are two critical connections. Not having pedestrian infrastructure in this area decrease connectivity and discourages walking and biking.

Table #48 Oxford Street & Diagonal Road Sidewalk Connection



Oxford Street Sidewalks – Walkability

Goal: Improve walkability along Oxford Street

Strategy:

Address the sidewalk access issue along Oxford Street.

5 E(s): Engineering and Encouragement

Existing Conditions:

There are sidewalks along the majority of Oxford Street. These sidewalks are generally in good condition, but access is an issue. There are street light poles that block the sidewalk. A person in a wheelchair would not be able to stay on the sidewalk and get around the light poles.

There are a number of community members in Worthington whose primary mode of transportation is walking and biking. Oxford Street does function as a Connector Street. There are higher volumes of through traffic and Oxford Street connects destinations in Worthington. Generally it is not safe to walk on Oxford Street, so sidewalks or a trail is needed to provide a safe place for pedestrians.

► **Connector Streets**

- Connects primary destinations
- Highest traffic volumes streets
- Require the highest level of pedestrian amenities - Sidewalks on both sides of the street or a trail conveniently located along the corridor that connects key locations.

When the sidewalk access issue is addressed, a buffer should be established between the sidewalk and vehicle traffic along Oxford Street. A grass buffer helps to increase the safety of pedestrians. A grass buffer along with street trees and landscaping also helps to increase the aesthetic character.

Table #49

Oxford Street Sidewalk Walkability



Light Pole Blocking Sidewalk



High Number of Driveways on Oxford Street

Oxford Street – Midblock Crossing

Goal: Improve safety and connectivity along Oxford Street.

Strategy:

Add a midblock crossing on Oxford Street by the football field Pizza Ranch area.

Add midblock crossings where needed along Oxford Street.

5 E(s): Engineering and Encouragement

Existing Conditions:

There are sidewalks along the majority of Oxford Street. There are also controlled crossings at the following intersections with Oxford Street:

- Diagonal Road – four way stop
- McMillian Street – stop light
- Humiston Avenue – stop light
- Omaha Avenue – stop light

There are several blocks between these controlled crossings, so it may not be convenient to walk to one of these crossings. People may decide to cross midblock, which can be a safety risk. Additional midblock crossings should be considered along Oxford Street to make crossing safer and in turn walking more convenient.

What are Mid-Block Pedestrian Crossings?

“Mid-block pedestrian crossings are marked crosswalks placed between intersections. They look similar to intersection crosswalks, but often incorporate several design features to increase safety. Mid-block crossings frequently include pedestrian islands, which provide a safe refuge for pedestrians crossing two-way traffic. Users can check traffic one way, cross to the island, and check traffic in the other direction before continuing to cross. Yield lines can be set back to require vehicles to stop farther away from the intersection. Bulb-outs (curb extension / bumpout) that narrow the roadway can be used to calm traffic by slowing speeds, and can make pedestrians more visible to drivers. Where traffic volumes are heavy, mid-block crossings can be signalized to further increase safety and comfort.”²³

Why Use Mid-Block Pedestrian Crossings?

“Many people do not go out of their way to cross at established intersections. Instead, they choose to cross the street using the most direct route, even if that means crossing several lanes of busy traffic. Mid-block pedestrian crossings decrease random and unpredictable crossings associated with a high risk

²³ Michigan Complete Street Coalition. Mid-Block Pedestrian Crossings Explained. Accessed: 6/24/15. Available: <https://michigancompletestreets.wordpress.com/2014/01/21/mid-block-pedestrian-crossings-explained/>

of collisions, especially in areas that are heavily travelled by pedestrians or where block lengths are long.”²⁴

When to Use Mid-Block Pedestrian Crossings:

“An engineering study should be completed to determine the need for a mid-block crossing, which incorporates roadway width, traffic volume, traffic speed and type, desired lines for pedestrian movement and adjacent land use. Heavily traveled areas that have high incidences of random crossings are good candidates for mid-block crossings, including schools, shopping centers, transit centers, and college campuses.”²⁴

Table #50 Oxford Street – random crossing by football field area



Safety Benefits:

“Mid-block locations account for more than 70% of pedestrian fatalities. Mid-block pedestrian crossings increase safety by decreasing random and unexpected pedestrian crossings. As stated before, people tend to cross where it is most convenient for them to cross, creating a high risk of collisions. Mid-block pedestrian crossings consolidate pedestrian traffic and allow drivers to predict and expect pedestrian traffic. Around 83% of pedestrians surveyed in an East Lansing, MI study changed their crossing behaviors where mid-block crossings were present. Where pedestrian islands have been included in the crosswalk design, pedestrian crashes were reduced by 46% and vehicle crashes were reduced by 39%.

²⁴ Michigan Complete Street Coalition. Mid-Block Pedestrian Crossings Explained. Accessed: 6/24/15. Available: <https://michigancompletestreets.wordpress.com/2014/01/21/mid-block-pedestrian-crossings-explained/>

Because mid-block crosswalks can be difficult to use for individuals with visual impairments, adding a crosswalk signal to the crossing will help make the treatment safer for all users.”²⁵

Table #51 **Mid-Block Crossing – Example**



Elmwood Avenue & Park Avenue Neighborhood Connection

Goal: Improve walkability in the Elmwood and Park Avenue neighborhood.

Strategy:

Fill in sidewalk gaps along Elmwood Avenue and Park Avenue.

5 E(s): Engineering and Encouragement

Existing Conditions:

Elmwood Avenue and Park Avenue are the primary connector streets to Oxford Street and West Lake Avenue from the residential streets in this area. There is also through traffic from Oxford Street to West Lake Avenue. Elmwood Avenue and Park Avenue function as Neighborhood Connectors. It is generally not safe to walk or bike on Neighborhood Connectors.

- ▶ Neighborhood Connector Streets
 - Connects Residential Streets to Connector Streets
 - Medium level traffic volume streets

²⁵ Michigan Complete Street Coalition. Mid-Block Pedestrian Crossings Explained. Accessed: 6/24/15. Available: <https://michigancompletestreets.wordpress.com/2014/01/21/mid-block-pedestrian-crossings-explained/>

- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other.

There are sidewalks along segments of Elmwood Avenue and Park Avenue. There are also sidewalks along some of the residential streets in this area. To create a safe and convenient place for pedestrians, a continuous sidewalk should be established along Elmwood Avenue and Park Avenue.

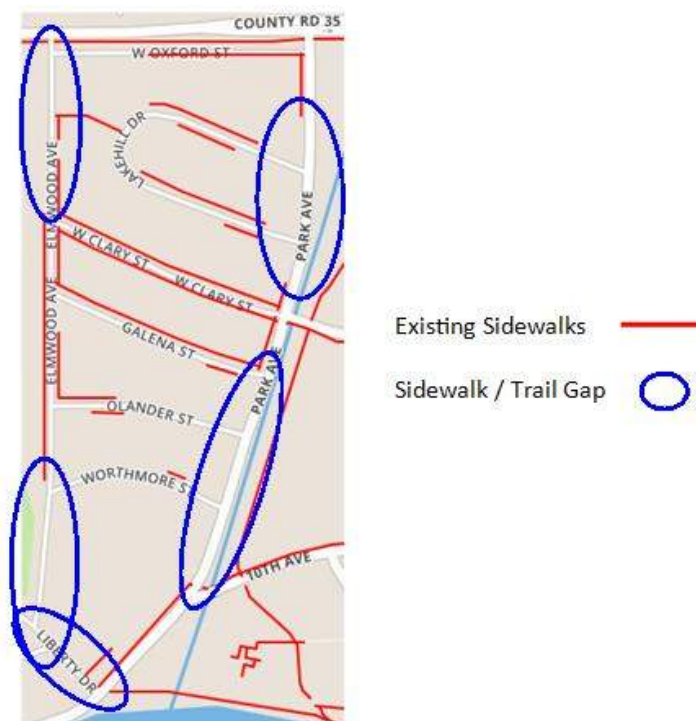
Elmwood Avenue and Park Avenue are within a walkable distance to the Middle School. Establishing a sidewalk along Elmwood Avenue and Park Avenue will increase the connectivity in this area to the existing pedestrian infrastructure. Providing a safer place for pedestrians will help to encourage students to walk and bike to the Worthington Middle School.

The Worthington Middle School is within walkable distance of the neighborhoods along Elmwood Avenue and Park Avenue. There is also a trail along Oxford Street that connects to Elmwood Avenue and Park Avenue. Providing a safe route to the trail will help to create a complete safe route to school, which will encourage walking and biking to school.

There are also a number of people who walk and bike around Lake Okabena. There is also a goal in this plan to improve the pedestrian loop around Lake Okabena. Since there is anticipation of increased pedestrian traffic around Lake Okabena, Elmwood Avenue and Park Avenue are and will be key connections to the lake and pedestrian infrastructure and amenities around the lake. Below is a map outlining the sidewalks gaps along Elmwood Avenue and Park Avenue.

Table #52

Elmwood Avenue & Park Avenue Sidewalk Gaps



College Way & Thompson Avenue Connection

Goal: Improve walkability and connectivity in the College Way and Thompson Avenue neighborhood.

Strategy:

Establish sidewalks along College Way and Thompson Avenue.

5 E(s): Engineering and Encouragement

Existing Conditions:

College Way and Thompson Avenue are primary routes to the Worthington Middle School, the YMCA, and the Minnesota West Community and Technical College Campus. College Way and Thompson Avenue are also the key connection between the trail along North Crailsheim and West Lake Avenue. There are higher traffic volumes and traffic speeds along College Way and Thompson Avenue.

College Way and Thompson Avenue function as a Neighborhood Connector and a Connector Street. College Way and Thompson Avenue are connecting neighborhoods and destinations that generate higher levels of vehicle and pedestrians traffic. Generally, it is not safe to walk and bike on and Connector Streets and Neighborhood Connectors.

► **Connector Streets**

- Connects primary destinations
- Highest traffic volumes streets
- Require the highest level of pedestrian amenities - Sidewalks on both sides of the street or a trail conveniently located along the corridor that connects key locations.

► **Neighborhood Connector Streets**

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other.

Since College Way and Thompson Avenue connect to a number of destinations and neighborhoods, sidewalks or a trail should be established. Sidewalks or a trail along College Way and Thompson Avenue would provide more of a community benefit. There were a number of comments via Wikimapping and the Community Survey regarding the lack of pedestrian infrastructure in this area and the need for it.

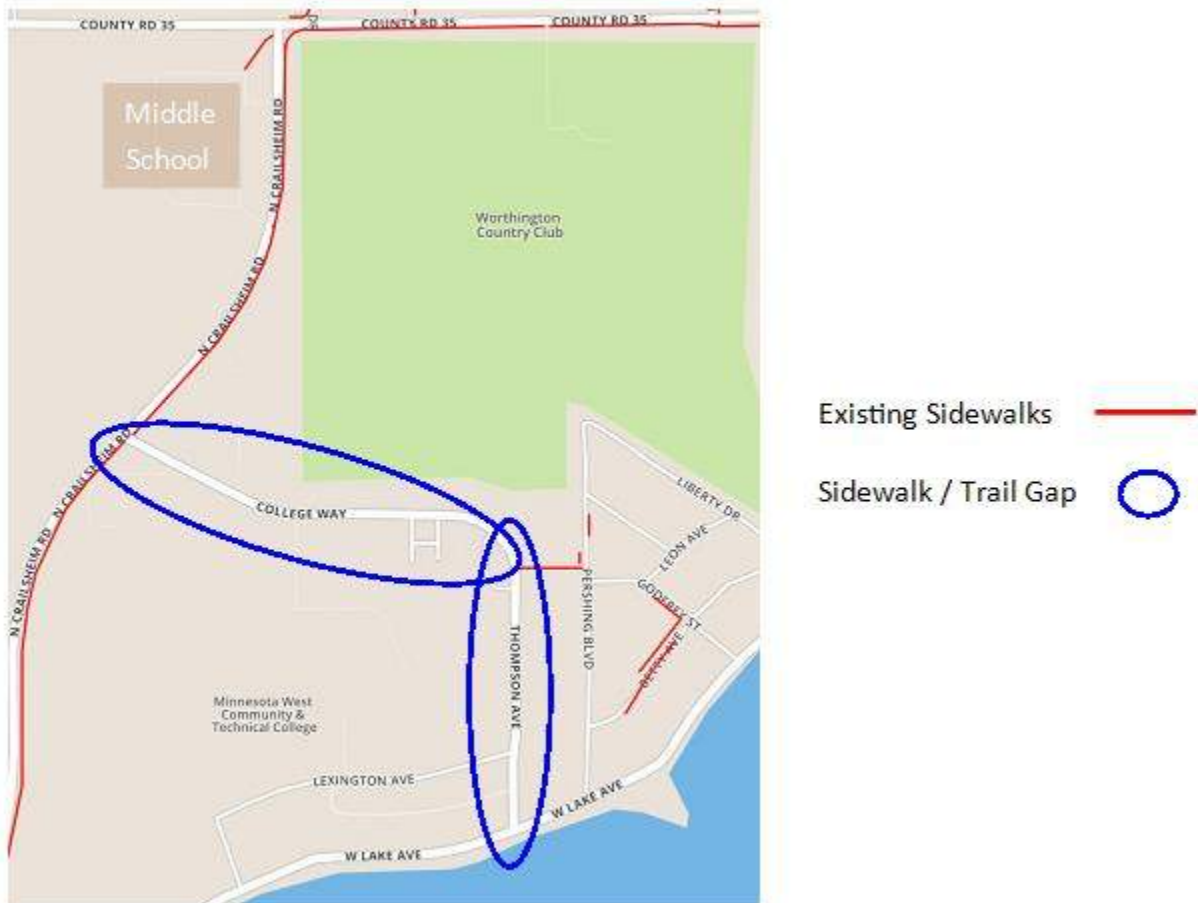
“Children have to walk on the street to get to school and the lake,” Wikimapping.

“This is primary route that should be filled in with sidewalks,” Wikimapping.

“From Centennial Park up the hill to Thompson Avenue, someone is going to get killed,” Community Survey.

“No sidewalk to the YMCA. Is this some kind of a joke?” Wikimapping.

Table #53 College Way & Thompson Avenue Sidewalk Gap



Homewood Hills Pedestrian Connections

Goal: Improve walkability and connectivity in the Homewood Hills neighborhood.

Strategy:

Establish sidewalks along Neighborhood Connectors in the Homewood Hills neighborhood.

Improve access between the sidewalk along Salley’s Alley and the trail.

5 E(s): Engineering and Encouragement

Existing Conditions:

Homewood Hills has a number of streets with sidewalks, but there are a number of sidewalk gaps and areas where the sidewalks do not connect to the street. The Homewood Hills Neighborhood is within walking distance of the Worthington Middle School, but there are only a few walkers and bikers from this area. Key connections need to be filled in to provide a safe place for pedestrians.

“Need sidewalks where none exist in random areas in the newer addition of Homewood hills,” Wikimapping.

“Not all properties have sidewalks and sidewalks end at boulevard and have to walk on grass before getting to street to cross,” Community Survey.

The Complete Streets Goal in this plan discusses one way of implementing Worthington’s Complete Streets Policy. The proposal is to classify streets in Worthington into three different categories. The top two categories require some pedestrian infrastructure. The purpose of this classification system is to research the function of each street and determine the necessary pedestrian infrastructure to make it safe.

Pedestrian infrastructure improvements need to be made in Homewood Hills to create safe connections to the trail and the Middle School. Filling in the sidewalk gaps on Neighborhood Connectors is the first step. Streets that connect neighborhoods and have both residential traffic and through traffic are most likely Neighborhood Connectors. Separated pedestrian infrastructure is needed on streets classified as Neighborhood Connectors and Connector Streets to provide a safe space for pedestrians.

Neighborhood Connector Streets

► Connector Streets

- Connects primary destinations
- Highest traffic volumes streets
- Require the highest level of pedestrian amenities - Sidewalks on both sides of the street or a trail conveniently located along the corridor that connects key locations.

► Neighborhood Connector Streets

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other.

► Residential Streets

- All other streets
- Lower traffic speeds
- Lower traffic volumes
- Sidewalks are encouraged but there is no sidewalk requirement

Classifying the streets in Homewood Hills will help to create a plan in filling in sidewalk gaps and areas where the sidewalks do not connect to the street. The streets that are functioning as Neighborhood Connectors need sidewalks to provide a safe place for pedestrians. These streets serve more of a community benefit, so they should be addressed first.

Sidewalk gaps on Residential Streets should be discussed with homeowners in Homewood Hills to create a plan for the neighborhood. Different streets require different levels of pedestrian infrastructure to create a safe place for pedestrians. Discussing and classifying the streets in Worthington is the first step in creating a consistent sidewalk and trail network throughout the city. Below is a map outlining the existing sidewalks in the Homewood Hills neighborhood.

Table #54 A Homewood Hills Sidewalks

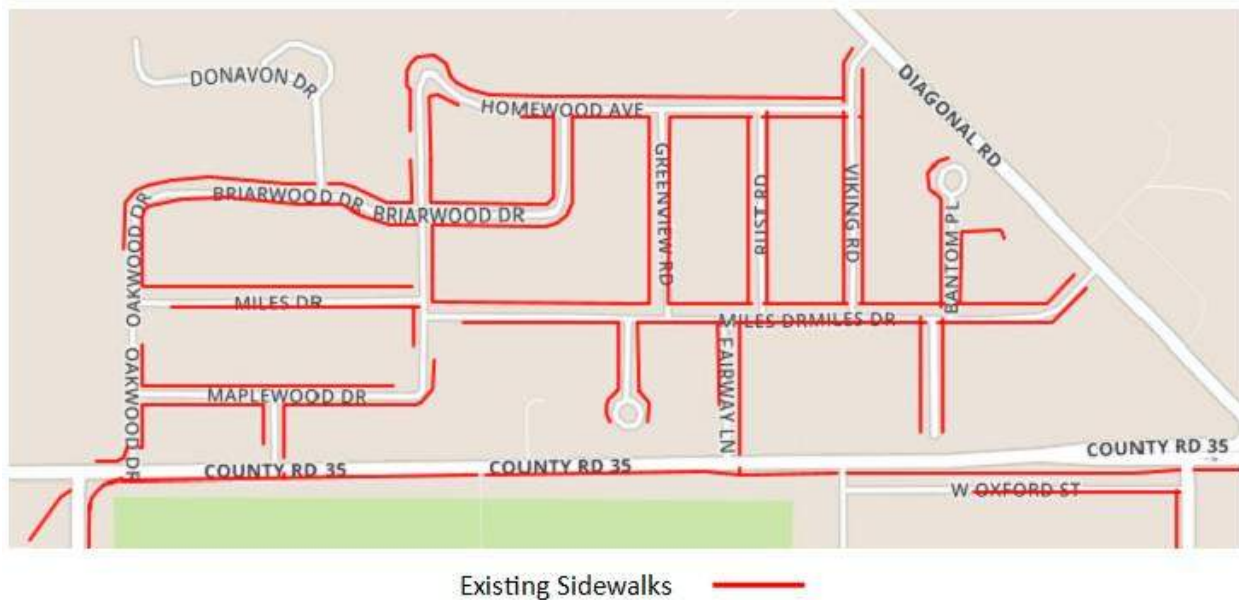


Table #54 B

Homewood Hills Sidewalks – not extending to the street



Sally's Alley—Connectivity Issue



Oakwood Drive—Positive Example

Downtown Walkability - 12th Street East & 1st Avenue

Goal: Improve connectivity between the downtown business district, the neighborhoods east of downtown around Sherwood Street, and the trail along Highway 60.

Strategy:

Fill in the sidewalk gap along 12th Street East between 1st Avenue and Sherwood Street.

Improve the aesthetic along 12th Street East to encourage walk and biking.

Fill in the sidewalk gap along 1st Avenue between 11th Street and 12th Street East.

5 E(s): Engineering and Encouragement

Existing Conditions:

The downtown business district in the City of Worthington is a destination for eating, shopping, and leisure. There are a number of community members who walk and bike to the downtown business district. Businesses in the downtown business district range from restaurants to the Sanford Worthington Medical Center to auto repair.

Twelfth Street is the main connection between the downtown business district and neighborhoods around Sherwood Street. There is a sidewalk gap along 12th Street East, so this connection is incomplete and is not a pleasant place for pedestrians. There are higher traffic volumes and traffic speeds along this route.

Twelfth Street functions as a Connector Street or a Neighborhood Connector, since there is through traffic, and residential traffic, and a higher traffic volume. It is generally not safe to walk and bike on the

road due to vehicle traffic and traffic speeds. Below is a description of a Connector Street and Neighborhood Connector.

► Connector Streets

- Connects primary destinations
- Highest traffic volumes streets
- Require the highest level of pedestrian amenities - Sidewalks on both sides of the street or a trail conveniently located along the corridor that connects key locations.

► Neighborhood Connector Streets

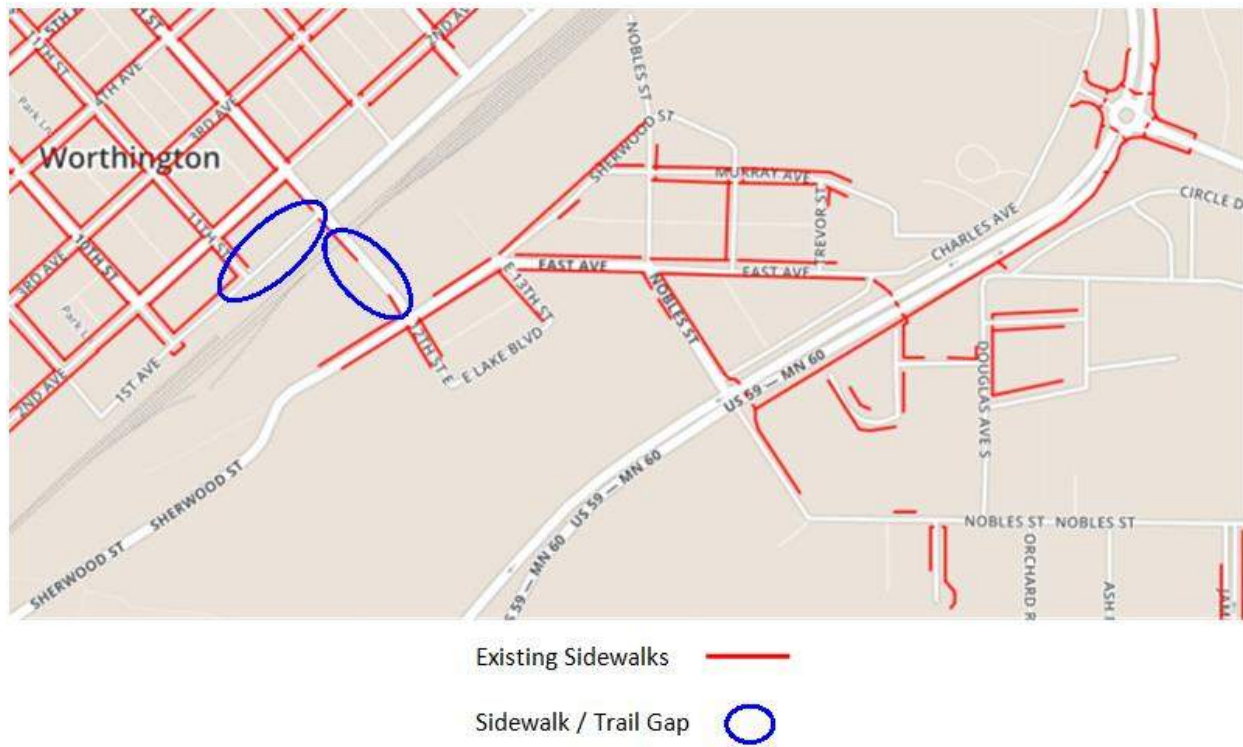
- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other.

There is a paved driveway along 12th Street East where the sidewalk gap exists, but the driveway is cracked and is not in great condition. This sidewalk and paved driveway along 12th Street East is a busy pedestrian route. There are a number of community members in Worthington whose primary modes of transportation is walking and biking. The sidewalk gap along 12th Street East is a critical gap to address. Filling in the sidewalk gap will help to create a safer and more convenient space for pedestrians.

Improving the aesthetic character of 12th Street East will also help to encourage walking and biking. Additional trees could be planted in the public right-of-way. There are a number of businesses in this area, so outreach could be done with these businesses to discuss ways to increase the aesthetic character of 12th Street.

Table #55

12th Street East & 1st Avenue Sidewalk Gap

**Table #56**

12th Street East Sidewalk Gap – Paved Driveway



Table #57

12th Street East Sidewalk Gap – Pedestrian & Vehicle Traffic



Table #58

1st Avenue Sidewalk Gap – Pedestrian Traffic



First Avenue also has high volumes of pedestrian traffic, since it is a primary route to the downtown. People tend to walk and bike using the most direct route. It is natural for pedestrians to use 1st Avenue as the primary route when walking or biking from neighborhoods around Sherwood Street to the downtown business district. Filling in the sidewalk gap along 12th Street East and 1st Avenue will create a safe and convenient connection between the neighborhoods around Sherwood Street to the downtown business district.

Highway 60 Trail & Lake Trail Connection

Goal: Improve connectivity between the trail along Highway 60, the neighborhoods east of downtown around Nobles Street, and the trail along Lake Okabena.

Strategy:

Engineer a trail connection between the trail along Highway 60 and the trail along Lake Okabena.

Construct a trail connection between the trail along Highway 60 and the trail along Lake Okabena.

5 E(s): Engineering and Encouragement

Existing Conditions:

There is an existing trail along Highway 60 and Lake Okabena. The existing route between these two trails is to use the sidewalks along 12th Street, which connect to sidewalks in the downtown business district. There is a sidewalk gap along 12th Street, so this connection is incomplete and is not a pleasant place for pedestrians.

Twelfth Street functions as a Connector Street or a Neighborhood Connector Street, since there is through traffic and residential traffic. Twelfth Street is the primary connection between the downtown business district and the neighborhoods around Sherwood Street. There are higher traffic volumes and traffic speeds.

Providing a convenient connection between the two trails will increase the connectivity of the trail network and encourage trail use. Providing a more scenic route between the two trails around Buss Field will also encourage trail use and enhance recreational opportunities at Buss Field.

There are currently three soccer fields at Buss Field. On weekends this is a very busy area, with multiple soccer games going on at once. Providing a convenient trail connection to Buss Field may encourage families to walk and bike to the weekend soccer games.

Buss Field is primarily used by minority populations. Creating a connection between the two trails and the soccer fields will directly benefit minority populations in Worthington. There are a larger percentage of the minority populations that rely on walking and biking as a primary mode of transportation, so this connection will provide a safer place for pedestrians and make it more convenient to attend soccer games and other activities at Buss Field.

The route of a connection trail has not been discussed extensively. There is an existing railroad bridge the trail could utilize to pass underneath the railroad tracks. The trail could loop around all three of the soccer fields or around one side of the fields or the other.

“There is a beautiful new path on east side of Hwy 60. How does zone F get there? Suppose a walkway attached to the railroad bridge is too far out of reality?” Community Survey.

Table #59 A Highway 60 Trail & Lake Trail Gap

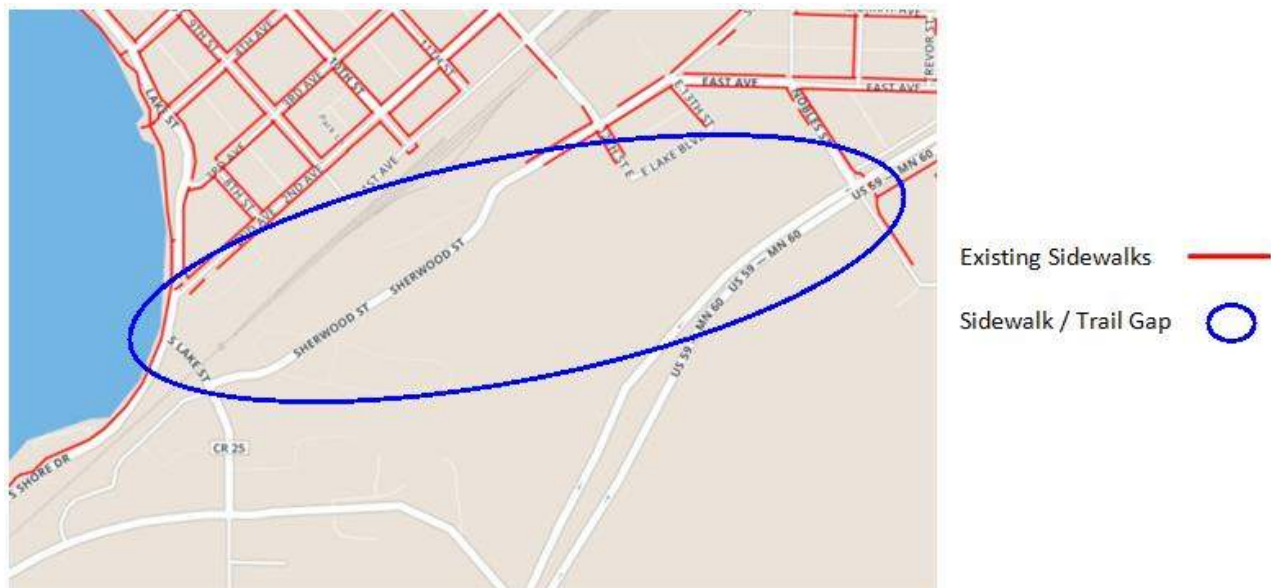
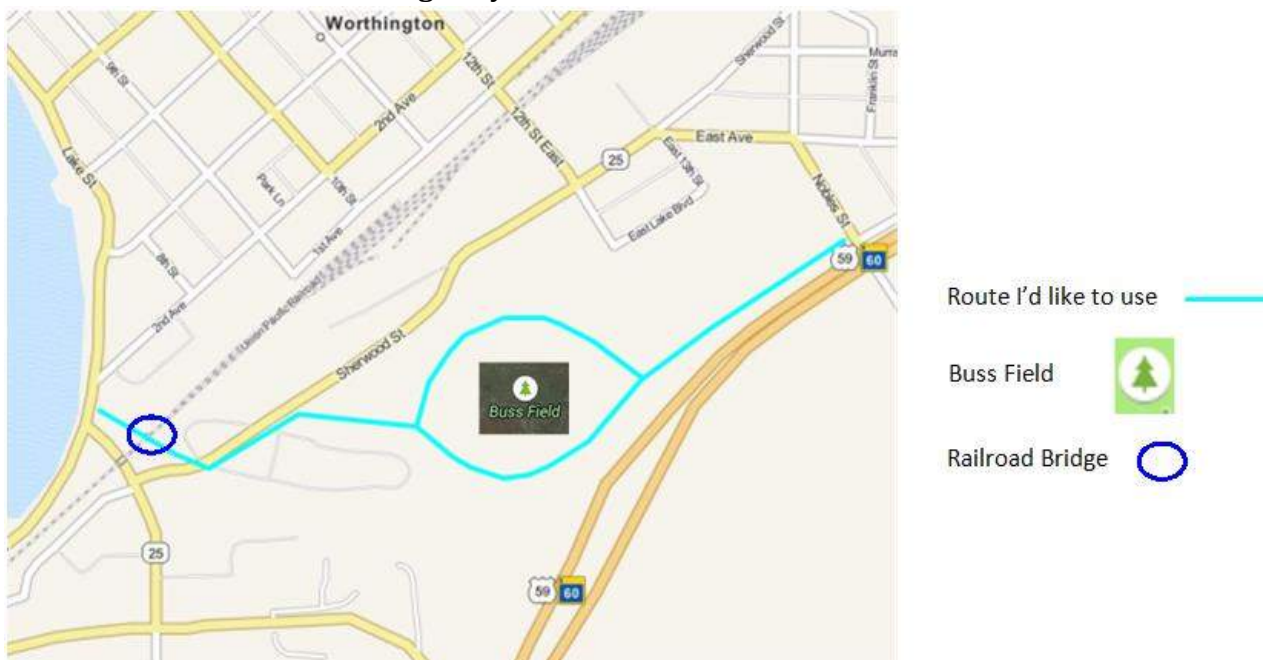


Table #59 B Highway 60 Trail & Lake Trail - Possible Connection



Worthington Non-infrastructure Goals & Strategies

Bike Parking

Goal: Increase biking within the City of Worthington.

Strategy:

Make biking more convenient within the City of Worthington by installing bike racks at key locations.

5 E(s): Encouragement and Engineering

Existing Conditions:

Bike racks make it convenient and safe to ride, store, and lock your bike. Not having bike racks in convenient locations discourages biking and leads potential bikers to not ride their bike. During the Worthington Active Living Plan Planning process locations were identified that need bike parking. These locations are major destinations in the community.

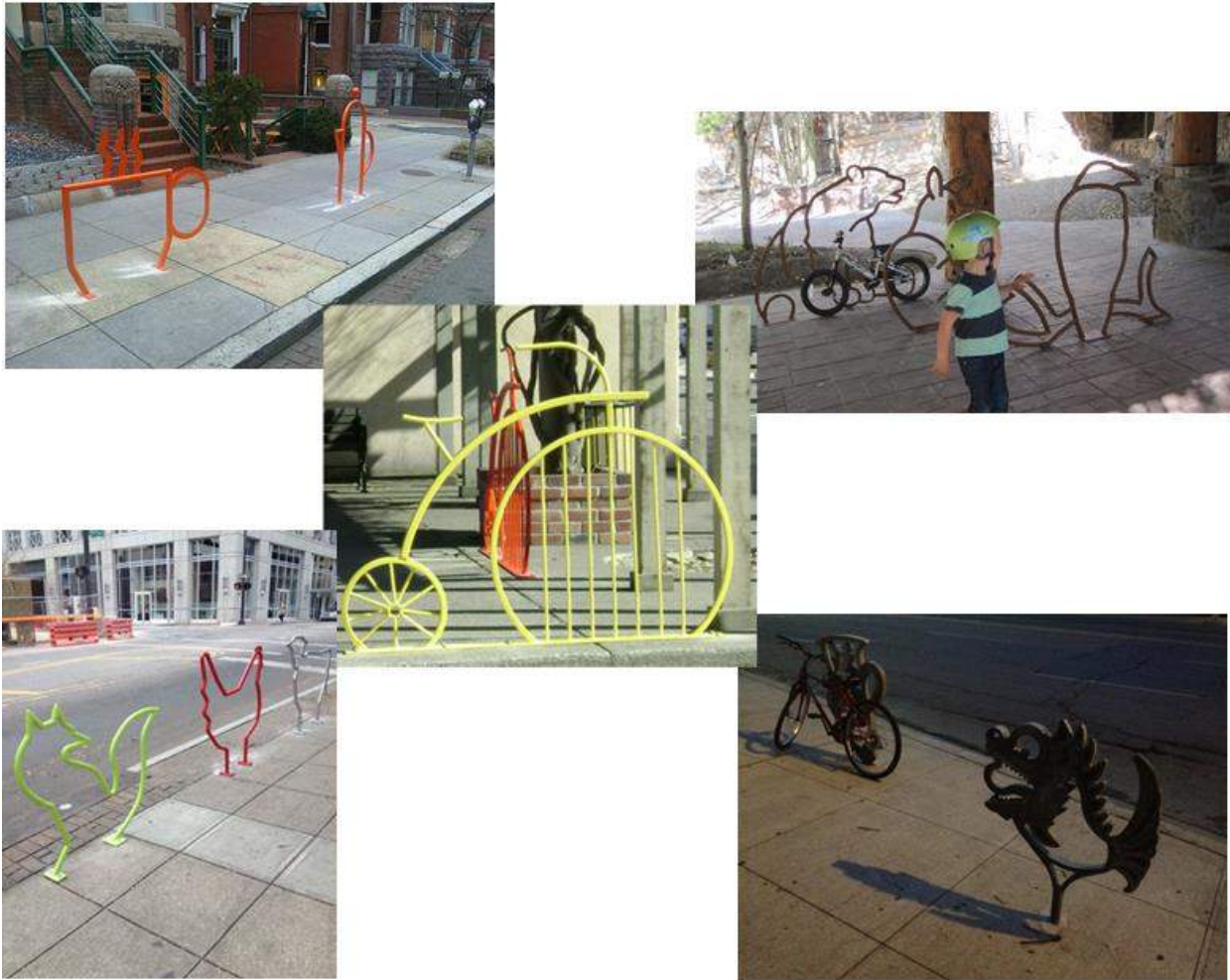
1. Downtown business district
2. Library
3. Prairie Elementary School
4. Parks around the lake
5. Center for Active Living
6. YMCA
7. Ball fields and tennis courts by the Worthington Middle School
8. Movie Theatre



Larger bike racks are great for schools, libraries, and other places that see higher volumes of bike traffic. It is also important to think about locations that need bike parking but a larger bike rack is not needed. Main Street may be one of these locations. The picture below shows animal outlines that work well for one or two bikes. You can have a few of these smaller bike racks along Main Street or in other shopping areas.

Table #60 A

Bike Rack - Examples



The bike racks could be unique and double as art in the community at the other identified locations. Having unique bike racks will increase the community feel and promote biking. Bike rack use may increase, since children may be more likely to use a bike rack shaped like a fish than a plain metal bike rack. The Planning Team did not choose a specific bike rack. Below are a few examples of larger bike racks.

Table #60 B

Bike Rack - Examples: Nautical



Pedestrian Benches

Goal: Increase the walkability within the City of Worthington.

Strategy:

Make walking more convenient by adding benches along higher pedestrian traffic routes and in key locations.

5 E(s): Education and Encouragement

Existing Conditions:

Not having pedestrian benches along higher pedestrian traffic routes and in key locations around Worthington discourages walking. Not having benches also discourages older adults from visiting the parks and using the trail. Older adults may need to rest along a route or in a park before continuing their walk.

Pedestrian amenities help to encourage use. Installing a bench along higher pedestrian traffic routes and in key locations will make it more convenient to visit the park. Below are a few locations that were identified as needing a pedestrian bench.

- Olsen Park by the trail under a tree
- Along the trail by the golf course on West Oxford Street

- Along the trail by the golf course on Crailsheim Road

Stop for Pedestrian Signs

Goal: Increase awareness of driver's responsibility to stop for pedestrians.

Strategy:

Purchase 7 'Stop for PED' signs that the city can rotate around the city at high pedestrian traffic intersections.

5 E(s): Encouragement and Engineering

Existing Conditions:

Drivers in Minnesota are supposed to stop for pedestrians in a crosswalk, but the majority of drivers do not stop for pedestrians. 'Stop for Pedestrian' signs will help to educate drivers on their responsibility to stop for pedestrians. Seeing more of these signs will help to make all crosswalks safer for pedestrians.

The Active Living Planning Team identified a need for 'Stop for PED' signs. These signs help to make drivers aware of their responsibility to stop for pedestrians. The City of Worthington can rotate these signs between high pedestrian traffic intersections. These signs can also be used during community events.

Identified intersections include:

- On 10th Street at the intersection with 5th Avenue
- On 10th Street at the intersection with 5th Avenue
- On 10th Street at the intersection with 2nd Avenue (farmers market)
- On Lake Street at the intersection with 7th Avenue and 8th Avenue
- On 5th Street at the intersection of 11th Street
- On Diagonal Road at the intersection of 10th Avenue

Input from the Active Living Community Survey

- "Drivers often do not yield to pedestrians."
- "Never know if cars are going to let me cross."
- "Very impatient drivers who do not yield to pedestrians or who do not even stop at stop sign."

The Minnesota Crosswalk Law

- Drivers MUST stop for crossing pedestrians at marked crosswalks and at all intersections without crosswalks or stop lights.
- Pedestrians MUST obey traffic signs and signals at all intersections that have them.



- Vehicles stopped for pedestrians can proceed once the pedestrian has completely crossed the lane in front of the stopped vehicle.
- Pedestrians MUST NOT enter a crosswalk if a vehicle is approaching and it is impossible for the driver to stop. There is no defined distance that a pedestrian must abide by before entering the crosswalk; use common sense.
- When a vehicle is stopped at an intersection to allow pedestrians to cross the roadway, drivers of other vehicles approaching from the rear MUST NOT pass the stopped vehicle. Failure to obey the law is a misdemeanor. A second violation within one year is a gross misdemeanor.²⁶

Center Line on Trails

Goal: Increase safety for walkers and bikers on the trails within the City of Worthington.

Strategy:

Stripe a center line on the existing and future trails in the City of Worthington.

5 E(s): Encouragement and Education

Existing Conditions:

The trails in the City of Worthington are shared use trails. Along with a shared use trail comes conflicts between users. Safety is impacted by the number of meetings (opposite direction encounters) and passes (same direction encounters) along the trail. Trail safety is also impacted by trail etiquette. The number of meetings and passes impact how wide a trail should be. The graph below outlines recommended trail widths based on the number of trail users.²⁷

²⁶ Minnesota Safety Council. Accessed: 4/17/15. Available:

<http://www.minnesotasafetycouncil.org/facts/factsheet.cfm?qs=BD3BE1A6DFA3335E>

²⁷ Federal Highway Administration. Evaluation of Safety, Design, and Operation of Shared-Use Paths. Accessed: 5/1/15. Available: <http://www.fhwa.dot.gov/publications/research/safety/05139/>

Table #61 Trail Width Chart

Trail users per hour in each direction	Trail width (feet)			
	8	12	16	20
25	B	B	A	A
50	D	B	A	A
75	D	B	B	A
100	D	B	B	A
150	E	C	B	B
200	F	D	C	B
250	F	D	C	C
300	F	E	D	C
400	F	F	E	E
500	F	F	F	F

Trail etiquette is a major component of trail safety. Striping the trail can also impact the safety of the trail. Users will be more aware of their responsibility to share the trail, if the trail is striped. Below is a quote from a Worthington trail user emphasizing the importance of striping trails.

- “I am mostly a biker and worked for a while in Worthington. Went around that lake hundreds of times. Having been on a number of trails, the biggest problem of course is sharing the trails with walkers and pets. Of course this is necessary but one thing that I think helps is to have a "center line". I think this simple thing helps to send the message to keep to the right and always be aware of oncoming traffic. It also helps bikers who, going faster, need to pass on those ahead.” Community Survey.

Table #62

Trail Center Line - Examples

*Driver Education Campaign*

Goal: Increase safety for walkers and bikers in the City of Worthington

Strategy:

Implement an annual Worthington Towards Zero Death (TZD) Coalition safety campaign targeting drivers and sharing the road with pedestrians.

5 E(s): Encouragement and Education

Existing Conditions:

Drivers in Minnesota are supposed to stop for pedestrians in a crosswalk, but the majority of drivers do not stop for pedestrians. Walkers are not sure if vehicles are going to stop. Bikers feel that they are invisible on the roadway and that the majority of drivers do not see bikers until it is too late.

The culture around sharing the roadway needs to be improved in southwest Minnesota. As more people are biking, drivers may become more aware of bikers, but etiquette towards sharing the road is still an issue. Annual education around sharing the road will help to raise awareness and safety for pedestrians.

Table #63

Share the Road / Towards Zero Deaths Graphics

*Trail Etiquette Campaign*

Goal: Increase safety along the trails in the City of Worthington.

Strategy:

Create a share the trail campaign that targets youth and adults.

5 E(s): Education and Encouragement

Existing Conditions:

All community members may not be aware of proper trail etiquette. On a trail the walker is the smallest, slowest object, so it is the responsibility of joggers and bikers to yield to walkers. It is also the responsibility of bikers to yield to joggers. Bikers are fast and can stop quickly, but bikers have to be in control and be able to stop and yield to walkers and joggers.

Walkers do have a responsibility to stay to the right and provide space for joggers and bikers to pass. Walkers should not be 3 or 4 wide and take up the entire trail. Below are other trail etiquette best practices to follow to keep the trail safe for all users.

Trail Etiquette

- Yield to pedestrians.
 - Pedestrians always have the right-of-way.
- Keep right and pass on the left.
 - The trail is like a roadway.
- Announce yourself; for example, "On your left!"
 - Warn trail users as you approach from behind.
- If you stop, get off the trail.

- Always allow other trail users to pass on the left.
- Report maintenance problems to city.
 - Please assist with minor maintenance – clearing sticks and other debris.
- Obey all signs and rules.
 - Stop at intersections.
 - Travel at safe speeds.
 - Keep right.
 - Keep the trail clean.
 - Don't litter.
 - Recycle trash on the trail.
- Keep animals under control.
 - Keep pets on a short leash.
 - Walk pets on the right-hand shoulder.
 - Clean animal waste from the trail.

Crosswalk Painting

Goal: Increase the visibility of pedestrian crossings in the City of Worthington

Strategy:

Maintain crosswalks along Collector Streets and Neighborhood Connector Streets.

5 E(s): Encouragement and Engineering

Existing Conditions:

A number of crosswalks are painted and well maintained within the City of Worthington. The Complete Streets Goal in this plan discusses one way of implementing Worthington's Complete Streets Policy. The proposal is to classify streets in Worthington into three different categories. The top two categories require some pedestrian infrastructure. The purpose of this classification system is to research the function of each street and determine the necessary pedestrian infrastructure to make it safe.

Generally, it is not safe to walk and bike on Connector Streets and Neighborhood Connectors. Additional pedestrian infrastructure is needed to create a safe space for pedestrians and bicyclists. It is

generally not safe since there are higher traffic volumes and speeds. There is also through traffic and residential traffic. Below is a description of Connector Streets and Neighborhood Connectors.

► Connector Streets

- Connects primary destinations
- Highest traffic volumes streets
- Require the highest level of pedestrian amenities - Sidewalks on both sides of the street or a trail conveniently located along the corridor that connects key locations.

► Neighborhood Connector Streets

- Connects Residential Streets to Connector Streets
- Medium level traffic volume streets
- Require some pedestrian amenities – A sidewalk on one side of the street or the other is required or a trail conveniently located that connects the neighborhood to key locations. The sidewalk needs to have continuity throughout, so the route is not jumping back and forth from one side of the street to the other.

Intersections along Connector Streets and Neighborhood Connectors need to be researched in regards to maintaining and painting crosswalks. Intersections along Connector Streets and Neighborhood Connectors that have higher pedestrian volumes need to have well maintained crosswalks. Crosswalks around schools, the downtown business district, and other higher pedestrian volume areas also need to be researched and maintained. The Worthington Safe Routes to School Plan outlines a number of crosswalks that should be painted and maintained.

- “Crosswalk needed at corner of Lake Avenue and Tower Street,” Community Survey.
- “There should be a crosswalk for people to take Nursing Home residents across South Shore Drive to Freedom Park,” Community Survey.
- “There should be a crosswalk at the intersection of Pinewood Drive and Miles Drive in Homewood Hills,” Community Survey.

Trail Adoption Program

Goal: Ensure trail maintenance during warm weather months.

Strategy:

Implement a trail adoption program.

5 E(s): Encouragement and Engineering

Existing Conditions:

To help ensure a clean and attractive looking trail network in and around the City of Worthington, you can adopt a section of the trail. Adopting a section of the trail provides an opportunity for community members to be actively involved in helping to maintain and enhance existing trails for all to enjoy.

When you adopt a trail, you are making sure trail users will always have a well-maintained trail to walk or bike. Managing the trail entails: keeping the trail surface clear of sticks, rocks, and other debris; picking up litter; reporting vandalism, large debris, and safety issues to the City. You are asked to visit the adopted trail section at least twice a month. You can visit the section of trail at your convenience, choosing the days and times you would like to visit the trail.

Who can adopt a trail?

1. Schools
2. Youth Groups
3. Church, Community and Service Organizations
4. Businesses
5. Families, Individuals or Groups of Individuals

Table #64

Adopt a Trail Signage



Adopt a Curb Extension or Street Corner

Goal: Enhance the aesthetic character of the City of Worthington.

Strategy:

Implement a curb extension, street corner, or island adoption program.

5 E(s): Encouragement

Existing Conditions:

To help ensure a clean and attractive looking public right-of-way around the City of Worthington, a curb extension or street corner adoption program should be established. Adding landscaping to the right-of-way will help to increase community feel within the City. Adopting a curb extension, street corner, or island will provide an opportunity for community members to be actively involved in helping to maintain and enhance existing trails for all to enjoy.

When you adopt a curb extension, street corner, or island, you are helping to create a visually more appealing community. The curb extension, street corner, or island adoption program will specify the height plants can be, so they do not decrease visibility. A sign can be displayed to highlight the individual, business, or service organization that adopted the curb extension, street corner, or island.

There are multiple examples landscaping adoption programs nationwide. The City of Randolph is one example that worked with the local Garden Club and the Department of Public Works to establish a landscaping adoption program. Below are pictures showing the size of recommended plants and signage highlighting who is responsible for the landscaping.²⁸

Table #65 A**Landscaping – Examples**

²⁸ City of Randolph. Accessed: 6/24/15. Available:
http://www.townofrandolph.com/Public_Documents/RandolphMA_Pics/adopt-an-island

Table #65 B

Landscaping Signage – Example

*Bike Education*

Goal: Improve bike safety within the City of Worthington.

Strategy:

Maintain the partnership between the Worthington Police Department and Worthington Public Schools.

Support the Walk! Bike! Fun! Pedestrian and Bike Safety Curriculum.

5 E(s): Enforcement

Existing Conditions:

Bike safety education is a critical component to ensure children know traffic laws and how to be safe while biking. Prairie Elementary hosted a Walk! Bike! Fun! Curriculum training in the spring of 2015. Worthington Public School is planning on integrating the curriculum into their physical education department.

The Worthington Police Department plans on supporting Worthington Public Schools in implementation of the Walk! Bike! Fun! Curriculum. Current Worthington Police Department staff is assisting with implementing the curriculum. This partnership is critical to improving bike safety in the City of Worthington.

Walk! Bike! Fun! is a pedestrian and bike safety curriculum that was developed specifically for Minnesota. “WALK! BIKE! FUN! is a comprehensive curriculum that teaches safe traffic behavior through classroom activities and on-the-bike skills practice. The goals of the extensive lesson plans teach

skills to children to walk and bicycle safely — building confidence and helping them stay safe, active, and healthy.”²⁹

Six benefits to walking or biking to school:

- To increase academic achievement — research shows that students who exercise before school concentrate better in class.
- To increase happiness — children that engage in physical activity are more likely to be happy.
- To lower your carbon footprint — a whole school committed to walking and biking can make an enormous impact on reducing carbon dioxide emissions and harmful pollutants.
- To help reduce traffic accidents — the benefit of schools that teach walking and bicycling skills result in up to a forty-nine percent decrease in childhood pedestrian and bicycle collision rates.
- To foster independence — children who walk or bike to school are more likely to walk to other destinations in the neighborhood.
- To increase physical activity — the CDC recommends that children get sixty minutes of physical activity every day.



Sidewalk Education & Enforcement Campaign

Goal: Maintain sidewalk access for pedestrians in the City of Worthington.

Strategy:

Implement an education and enforcement campaign to target illegal blocking of sidewalks.

5 E(s): Encouragement and Education

Existing Conditions:

A sidewalk being blocked by a parked car, truck, trailer, or anything else can be a safety issue. A pedestrian may have to backtrack along the sidewalk and walk on the street, which may not be safe. It is illegal in the City of Worthington to block a sidewalk.

²⁹ Walk! Bike! Fun! Curriculum. Accessed: 4/29/15. Available: <http://www.dot.state.mn.us/saferoutes/pdf/toolkit/walk-bike-fun-curriculum.pdf>

Sidewalk accessibility can also be an issue in the wintertime. Snow is a major barrier to walking in the wintertime. The City of Worthington does have a sidewalk maintenance policy that includes the clearing of snow, but it is not always enforced.

Not having consistent sidewalk connections decreases the walkability of the community and can be a safety issue if the pedestrian is forced to use the street. The Active Living Planning Team recommends an education and enforcement campaign to be implemented and maintained by the Worthington Police Department.

Educate the public that it is illegal to block a sidewalk and on their responsibility to clear snow and debris from sidewalks on their property. Outreach can be done by issuing press releases and by issuing written warnings. Having well maintained sidewalks will increase safety and connectivity within the City of Worthington. There are already policies in place to address these issues. An educational campaign followed by enforcement and continued enforcement can be effective.

On Street Bike Route

Goal: Increase biking within the City of Worthington

Strategy:

Create a signed bike route with painted bike lanes within the City of Worthington.

Market the Worthington bike routes via maps.

5 E(s): Engineering and Encouragement

Existing Conditions:

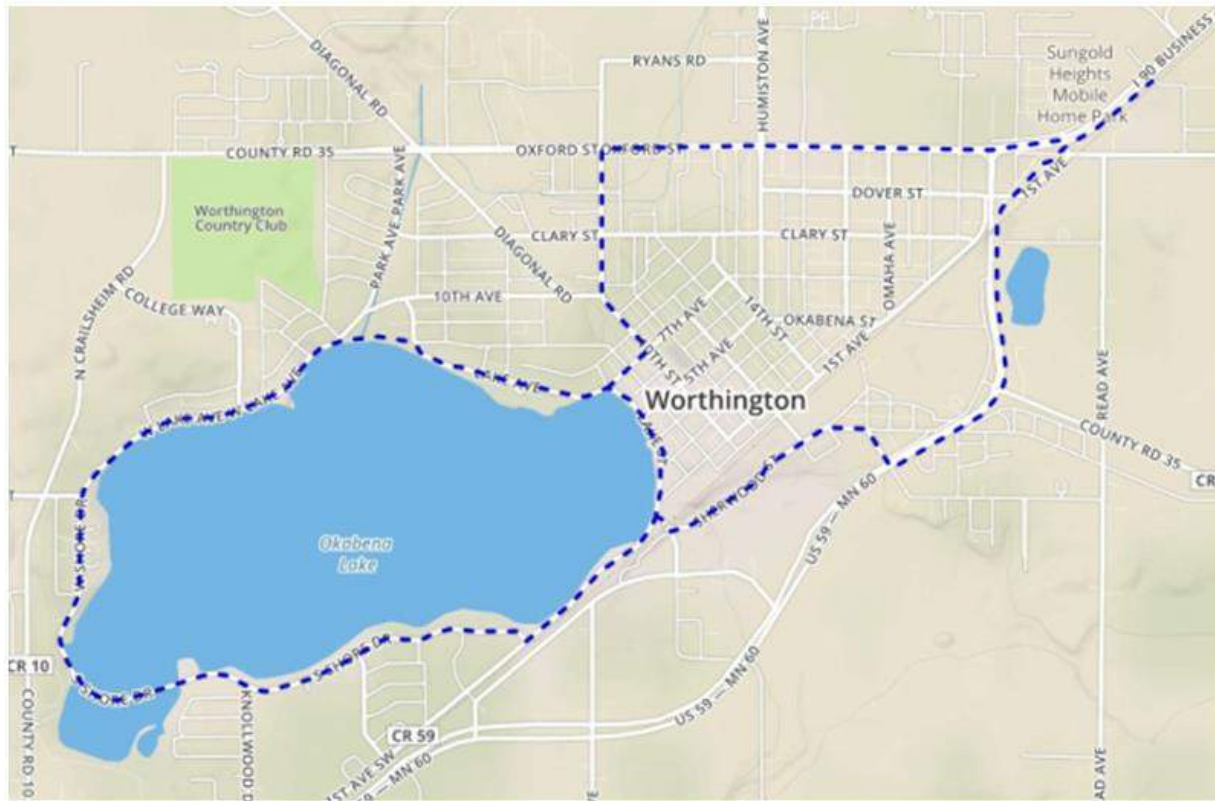
Mapping a bike route encourages greater use, so bikers can be directed to a specific route and safety improvements can be made along the route. A designated route helps to direct users and inform drivers of increased bike traffic in this area. Outlining the route will also show users and potential users how long the route is, so they know what to expect. As usage increases, visibility will also increase, making the loop safer.

The Planning Team identified a need of connecting the neighborhoods to JBS and other large employers. A bike path along Highway 59 / Oxford Street has been proposed by the Minnesota Department of Transportation (MnDOT). The City of Worthington is also willing to extend the bike lanes west of Oxford towards Diagonal Road, but the road dimensions change when you get closer to Diagonal Road. Additional engineering will have to take place to extend the bike lanes on Oxford Street from the trail along highway 60, which connects to JBS, and Diagonal Road.

During the bike route discussions at the two community meetings, a general bike route map was created. This map needs to be finalized by the City of Worthington Engineering Department. The map would have to be approved by the Traffic and Safety Committee and then the City Council before the bike route would be established.

Table #66 A

Proposed On Street Bike Route



Proposed On Street Bike Route — — —

Table #66 B

Proposed On Street Bike Route - Oxford Street



Marketing – Active Living

Goal: Increase walking and biking within the City of Worthington

Strategy:

Create a separate Active Living tab on City of Worthington's website under Parks & Campground.

5 E(s): Encouragement and Education

Existing Conditions:

Marketing is a critical role in getting people active. Visibility plays a big role in promoting walking and biking and increasing safety along primary routes. As more people walk a route, the visibility and culture changes. Drivers become more aware of walkers and bikers and safety improves. This process takes time, but it starts with promoting existing routes.

The City of Worthington does have a trail and a well-developed network of sidewalks. There are some sidewalk gaps, but hopefully over time the critical sidewalk gaps will be filled. Outlining routes and ways to be active will help to increase walking and biking and the health of the community.

Being active does not only affect physical health, but it also affects emotional health, community safety, and community health. Community members that walk and bike interact with other community members. This helps to increase cooperation and safety in the community. Community members are eyes and ears for law enforcement, so routes can become safer for all users.

Having an Active Living tab under Parks and Campground can be used to highlight:

- Trails
- Bike Routes (distance of the routes)
- Walking Routes (distance of the route)
- Community Events (ways to be active)
- Trail Etiquette
- Worthington Active Living Plan
- Worthington Safe Routes to School Plan

Sidewalk Maintenance

Goal: Keep sidewalk clear of debris and snow

Strategy:

Work with land owners to keep sidewalks clear of debris and obstructions.

Enforce the snow removal policy.

5 E(s): Encouragement and Engineering

Existing Conditions:

A vehicle or branch blocking a sidewalk can be a major obstacle for an elderly person or someone with a disability. It is important to keep sidewalks cleared of debris and snow. A sidewalk is public right-of-way and so is the space along a street where a sidewalk could be.

It is illegal to block a sidewalk with a vehicle parked in a driveway. The Worthington Police Department should work with land owners to not block sidewalks with parked cars. A written warning should be issued when a sidewalk is blocked and a ticket should be written for repeat offenders.

It is the responsibility of the land owner to remove branches and other debris that can block a sidewalk. A written warning could be issued when a sidewalk is blocked and a ticket should be written for repeat offenders. It is important to work with property owners, since the majority of residents will comply if asked.

The City of Worthington has a snow removal policy for sidewalks. This policy needs to be enforced. A written warning could be issued when a sidewalk is not cleared of snow and a ticket should be written for repeat offenders.

Keeping sidewalks cleared helps to encourage walking in Worthington. If sidewalks are not maintained, pedestrians will have to walk on the street. Some streets in Worthington may not be safe to walk on due to higher traffic volumes and traffic speeds. Not maintaining sidewalks creates a potential safety issue.

During the Active Living community meetings, a specific section of sidewalk by the library was identified. The sidewalk by the library is partially blocked by bushes. These bushes need to be trimmed back to provide adequate clearance. It may be difficult for a person in a wheelchair to navigate this section of sidewalk.

- Cars often park blocking the sidewalk.

Pedestrian Lighting Issues

Goal: Improve pedestrian lighting in the City of Worthington.

Strategy:

Install pedestrian scale lighting along the trail leading to JBS.

Install pedestrian scale lighting on trails within the City of Worthington.

Improve lighting on Highway 59 just south of Interstate 90.

Improve lighting on North McMillan Street between Oxford Street and Stower Drive.

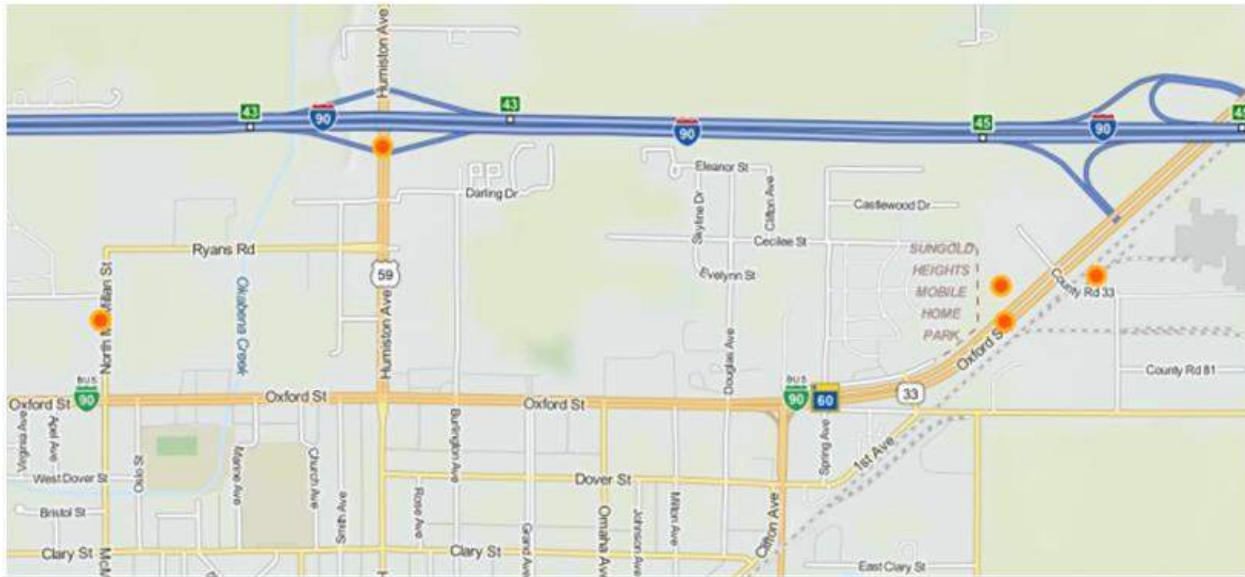
5 E(s): Encouragement, and Engineering


Existing Conditions:

Pedestrian scale lighting helps to encourage walking and biking. With work and school some people may only have time to walk or bike in the morning or in the evening. Evening shifts also result in some people having to walk or bike to and from work when it is dark outside. It is important to have good visibility, so pedestrians can see where they are going and vehicles can see pedestrians. Well lit trails and streets also make pedestrians feel safer from harassment and abduction.

“Need trail lighting for JBS employees who walk or bike to work. They come or go in the dark due to shift work hours. Need safe lighting in order for JBS to encourage employees to walk and bike.” Wikimapping.

Table #67 **Locations with Poor Lighting – Wikimapping**



Pedestrian Lighting Issue 

The locations above were identified via Wikimapping or at the Active Living community meetings. These locations have poor pedestrian lighting. Below are examples of pedestrian scale light and street lighting.

Table #68 A **Pedestrian Lighting Examples**



Table #68 B

Pedestrian Lighting Examples – Sizing Scale

*Trail Safety*

Goal: Improve bike safety along the trails in the City of Worthington.

Strategy:

Add pedestrian scale stop signs along the trails, so pedestrians have to stop and look for crossing vehicle traffic.

5 E(s): Encouragement, and Engineering

Existing Conditions:

Pedestrian scale stop signs along trails help to alert users of a busy intersection and the need to stop and check for crossing vehicle traffic. Stop signs are critical at crossings where cross traffic does not stop. These crossings and other busy trail crossings need to be identified and pedestrian scale stop signs need to be posted.

Additional signage along with the pedestrian scale stop sign could include:

- Cross traffic does not stop
- Cyclist Dismount (helps to ensure cyclists look carefully for cross traffic before crossing).

Table #69 Pedestrian Scale Stop Sign – Cross Traffic Does Not Stop



Table #69 B Pedestrian Scale Stop Sign – Cyclist Dismount



V Plan Maintenance

The Worthington Active Living Plan is a working document. The City of Worthington will maintain the plan. The City of Worthington will continue to make updates to the plan with assistance and recommendations from local organizations and groups.

Monitoring, Evaluation, & Updating the Plan

As community planning occurs, additional goals and strategies will be added in Chapter VI, Additional Goals and Strategies. It is critical to allow for public input regarding additional goals and strategies. Community residents and the Worthington City Council should be asked to provide input regarding infrastructure projects.

Continued Public Involvement

Future trails and pedestrian projects will be discussed at city council meetings and open community meetings. It is recommended that a trails committee be formed in the City of Worthington or in Nobles County. A trails committee will help to ensure continued planning occurs. Continued planning and public involvement will ensure proposed projects have been vetted.

VI Additional Goals & Strategies

The Worthington Active Living Plan is a working document. The City of Worthington will continue to make updates to the plan. As planning continues, additional recommendations should be made to the Additional Goals and Strategies Chapter.

Conclusion

When making a land use decision and an investment in the future, it is critical to consider all the costs, not just the construction costs. There are costs associated with sprawl, inactivity and negative health outcomes, and the loss of community. Decision makers need to consider the function of every street, road, and plot of land. Is the function of this place to create wealth or is the function of this particular roadway to move cars quickly. If it is to move cars quickly, do not try and generate developments that will decrease walkability and connectivity in the community.

When you are rebuilding a street, ask yourself if this street can be narrow. The cost savings of narrowing the street could be used to install sidewalks and create a more livable community. On existing streets consider other higher return investments, like painting crosswalks or putting in striping on the streets to narrow the lanes and slow down traffic.

*"When you narrow up the street lanes, cars drive slower, people feel more comfortable there. They walk across the streets, to a store across the street. And in a real subtle and cheap way you get a lot more pedestrian traffic, a lot more retail, a lot more people. And people spend money, and that's what makes a place wealthier. When people are there, people invest more."*³⁰

³⁰ Econ Talk. Charles Marohn on Strong Towns, Urban Development, and the Future of American Cities. Accessed: 5/13/15. Available: http://www.econtalk.org/archives/2014/05/charles_marohn.html

Table #70

Decision Makers Checklist: Built Environment**Ask Yourself and the Decision Making Group**

- How will my decision impact health?
- How will my decision impact connectivity?
 - Compact efficient development vs. sprawl (which decreases connectivity)
- Will my decision make the community more inviting (more walkable and bikeable)?
- Were all users considered when making the decision?

A RESOLUTION ESTABLISHING A COMPLETE STREETS POLICY.

WHEREAS, it is the purpose of complete streets to create transportation corridors that are safe, functional and aesthetically attractive for all users;

AND WHEREAS, the mobility of freight and passengers and the safety, convenience, and comfort of motorists, cyclists, pedestrians - including people requiring mobility aids, transit riders, and neighborhood residents of all ages and abilities should all be considered when planning and designing Worthington's streets;

AND WHEREAS, integrating sidewalks, bike facilities, and safe crossings into the initial design of street projects avoids the expense of retrofits later;

AND WHEREAS, streets are a critical component of public space and play a major role in establishing the image and identity of a city, providing a key framework for current and future development;

AND WHEREAS, active living integrates physical activity into daily routines and active living communities encourage individuals of all ages and abilities to be more physically active;

AND WHEREAS, communities that support active living strive to create amenities that will enhance the quality of life of its residents, improve the physical and social environment in ways that attract businesses and workers, and contribute to economic development;

AND WHEREAS, the goal of complete streets is to improve the access and mobility for all users of streets in the community by improving safety through reducing conflict and encouraging non-motorized transportation;

AND WHEREAS, it is recognized that there are some streets or corridors in the City which would not fully satisfy a complete streets environment;

NOW THEREFORE, Be It Resolved that the City Council of the City of Worthington, Minnesota establish a Complete Streets Policy that provides as follows:

1. The City of Worthington will, whenever it is economically feasible, seek to enhance the safety, access, convenience and comfort of all users of all ages and abilities, including bicyclists, pedestrians (including people requiring mobility aids), motorists and freight drivers, through the design, operation and maintenance of the transportation network so as to create a connected network of facilities accommodating each mode of travel that is consistent with and supportive of the local community, recognizing that all streets are different and that the needs of various users will need to be balanced in a flexible manner.

2. Unless one or more of the conditions set forth in Section 4 exist, transportation improvements will include appropriate facilities and amenities that are recognized as contributing to complete streets, which may include street and sidewalk lighting; sidewalks and pedestrian safety improvements such as median refuges or crosswalk improvements; improvements that provide ADA (Americans with Disabilities Act) compliant accessibility; bicycle accommodations including bicycle parking, bicycle routes, shared-use lanes, wide travel lanes or bike lanes as appropriate; and street trees, boulevard landscaping, street furniture and adequate drainage facilities.
3. Early consideration of all modes for all users will be important to the success of this Policy. Those planning and designing street projects will give due consideration to bicycle, pedestrian, from the very start of planning and design work. This will apply to all roadway projects, including those involving new construction, full reconstruction, or changes in the allocation of pavement space on an existing roadway such as the reduction in the number of travel lanes or removal of on-street parking.
4. Bicycle, pedestrian and transit facilities will be considered when developing street construction, reconstruction, re-paving, and re-habilitation projects, except under one or more of the following conditions:
 - A. Reconstruction or re-paving of a street, excluding collector and arterial streets, which does not involve substantial curb removal.
 - B. A project involves only ordinary maintenance activities designed to keep assets in serviceable condition, such as mowing, cleaning, sweeping, spot repair, concrete joint repair, or pothole filling, or when interim measures are implemented on temporary detour or haul routes.
 - C. The City Engineer, with Council consultation, determines there are relatively high safety risks.
 - D. The City Council exempts a project due to excessive and disproportionate cost of establishing a bikeway or walkway as part of a project.
 - E. It is determined that the construction is not practically feasible or cost effective for reasons including, but not limited to: significant or adverse environmental impacts to streams, floor plains, remnants of native vegetation, wetlands, steep slopes or other critical areas; or due to impacts on neighboring land uses, including impact from right-of-way acquisition.

5. It will be important to the success of the Complete Streets policy to ensure that the project development process includes early consideration of the land use and transportation context of the project, the identification of gaps or deficiencies in the network for various user groups that could be addressed by the project, and an assessment of the tradeoffs to balance the needs of all users. The context factors that should be given high priority include the following:
 - A. Whether the corridor provides a primary access to a significant destination such as a community or regional park or recreational area, a school, a shopping/commercial area, or an employment center;
 - B. Whether the corridor provides access to across a natural or man-made barrier such as a river or freeway;
 - C. Whether the corridor is in an area where a relatively high number of users of non-motorized transportation modes can be anticipated;
 - D. Whether a road corridor provides important continuity or connectivity links for an existing trail or path network; or
 - E. Whether nearby routes that provide a similar level of convenience and connectivity already exists
6. The design of new or reconstructed facilities should anticipate likely future demand for bicycling, walking and transit facilities and should not preclude the provision of future improvements. (For example, under most circumstances, bridges, which last for 75 years or more, should be built with sufficient width for safe bicycle and pedestrian use in anticipation of a future need for such facilities.)
7. The City will maintain a comprehensive inventory of the pedestrian and bicycling facility infrastructure integrated with City street and utility maps and will carry out projects to reduce gaps in the sidewalk and trail networks.
8. Complete streets may be achieved through single projects or incrementally through a series of smaller improvements or maintenance activities over time.
9. The City will generally follow accepted or adopted design standards when implementing improvements intended to fulfill this Complete Streets policy but will consider innovative or non-traditional design options where a comparable level of safety for users is present.

10. The City will develop implementation strategies that may include evaluating and revising manuals and practices, developing and adopting network plans, identifying goals and targets, and developing methods to evaluate success.
11. This policy will provide notification to Parks and Recreation for review of impacts to street trees and incorporate recommendations into design consideration.
12. The City Engineer shall be responsible for developing and implementing the Complete Streets Policy through the recommendations of project to the City Council.

BE IT FURTHER RESOLVED, that the feasibility report prepared for a street project shall address this policy.

Adopted by the City Council of the City of Worthington, Minnesota, this the 25th day of March, 2013.

Mayor

City Clerk



Minnesota Department of Natural Resources
Division of Parks and Trails
500 Lafayette Road
St. Paul, MN 55155-4039

June 24, 2024

Mindy Eggers, City Clerk
City of Worthington
303 Ninth Street
Worthington, MN 56187

Dear Mindy Eggers,

RE: City of Worthington, Project Number: C014-24-4B
East Okabena Lake Trail

I am pleased to inform you that your application to the 2024 Local Trail Connections Program has been selected for funding. A reimbursement grant in the amount of \$250,000 will be established contingent upon final processing of your application.

Our program staff will contact you soon with more information on how to proceed with your project. If you have any questions, please contact Daniel Golner, Trail Grants Coordinator at 651-259-5599 or Daniel.golner@state.mn.us.

Thank you for helping to connect people and the outdoors by providing outstanding parks and trails in your community.

Sincerely,

A handwritten signature in black ink, appearing to read 'Ann M. Pierce'.

Ann M. Pierce, Director
Division of Parks and Trails

CC: File #C014-24-4B



LETTERS OF SUPPORT

NOTE: While these letters were collected for a different solicitation, community and agency support remains consistent.



Worthington District Office
1117 Marine Avenue • Worthington, MN 56187
Main Phone: 507.372.2172
Fax: 507.372.2174
www.isd518.net

March 25, 2024

ATTN: Dan Golner
Parks and Trails Division
500 Lafayette Road, Box 39
Saint Paul, MN 55155

Re: Support for the City of Worthington's East Okabena Lake Trail Project

Dear Mr. Golner,

On behalf of the students and families of Independent School District 518, I am writing to express my support for the City of Worthington's pursuit of MnDNR Local Trail Connections funding to construct the East Okabena Lake Trail Project. This project would be very beneficial for students walking and biking to school.

The proposed project will fill a trail gap within the City of Worthington and create more opportunities for students to access Prairie Elementary by walking or biking. The proposed trail would also connect to the local trail and sidewalk network, which would improve bicycle and pedestrian accessibility throughout the City of Worthington. Furthermore, this project would provide a safe bicycle and pedestrian connection from residential neighborhoods along East Lake Boulevard. This would allow students to walk and bike to school without having to share the road with vehicles.

There is support among students' families for this project because it improves safety for students coming to school while also improving connectivity throughout the entire city.

I greatly appreciate your time and consideration in reviewing the application.

Sincerely,

John Landgaard
Superintendent
Independent School District 518



March 21, 2024

ATTN: Dan Golner
Parks and Trails Division
500 Lafayette Road, Box 39
Saint Paul, MN 55155

Re: Support for the City of Worthington's East Okabena Lake Trail Project

Dear Mr. Golner,

On behalf of the citizens of Worthington, I am writing to express my support for the City of Worthington's pursuit of MnDNR Local Trail Connections funding to construct the East Okabena Lake Trail Project. The City of Worthington will provide safe operation and maintenance of the facility upon completion.

The proposed project will construct a trail beginning at S Lake Street and S Shore Drive, which will then run along Sherwood Street and through currently undeveloped land before aligning with East Lake Boulevard and connecting to Highway 60 at Nobles Street. The proposed trail would also connect to the local trail and sidewalk network, which would improve bicycle and pedestrian accessibility throughout the City of Worthington. Furthermore, this project would provide a safe bicycle and pedestrian connection to Prairie Elementary School. This would allow students to walk and bike to school without having to share the road with vehicles.

There is support among residents for the improvement project because it improves connectivity throughout the entire city for both residents and visitors.

I greatly appreciate your time and consideration in reviewing the application.

Sincerely,

A handwritten signature in blue ink that reads "Rick von Holdt".

Rick VonHoldt
Mayor, City of Worthington



March 21, 2024

ATTN: Dan Golner
Parks and Trails Division
500 Lafayette Road, Box 39
Saint Paul, MN 55155

Re: Support for the City of Worthington's East Okabena Lake Trail Project

Dear Mr. Golner,

This is written confirmation that the City of Worthington Public Works Department is committed to maintaining the proposed trail improvement project beginning at S Lake Street and S Shore Drive, which will then run along Sherwood Street and through currently undeveloped land before aligning with East Lake Boulevard and connecting to Highway 60 at Nobles Street. The City of Worthington will provide safe operation and maintenance of the facility upon completion.

The proposed trail will fill a trail gap for trail users, providing bicycle and pedestrian amenities where none currently exist. The proposed trail would also connect to the local trail and sidewalk network, which would improve bicycle and pedestrian accessibility throughout the City of Worthington.

Public Works supports the improvement project because it improves safety and connectivity throughout the entire city for both residents and visitors.

The Worthington Public Works Department greatly appreciates your time and consideration in reviewing the application.

Sincerely,

A handwritten signature in blue ink, appearing to read "Todd Wietzema".

Todd Wietzema
Public Works Director, City of Worthington



March 21, 2024

ATTN: Dan Golner
Parks and Trails Division
500 Lafayette Road, Box 39
Saint Paul, MN 55155

Re: Support for the City of Worthington's East Okabena Lake Trail Project

Dear Mr. Golner,

On behalf of the citizens of Nobles County District 4 and the City of Worthington, I am writing to express my support for the City of Worthington's pursuit of MnDNR Local Trail Connections funding to construct the East Okabena Lake Trail Project. The City of Worthington will provide safe operation and maintenance of the facility upon completion.

The proposed trail will fill a trail gap for trail users, providing bicycle and pedestrian amenities where none currently exist. The proposed trail would also connect to the local trail and sidewalk network, which would improve bicycle and pedestrian accessibility throughout the City of Worthington.

There is support among residents for the improvement project because it improves connectivity throughout the entire city for both residents and visitors.

I greatly appreciate your time and consideration in reviewing the application.

Sincerely,

A handwritten signature in blue ink, which appears to read "Robert S. DeMuth, Jr.", is written over the word "Sincerely,". The signature is fluid and cursive.

Robert S. DeMuth, Jr.
Nobles County Commissioner, District 4

RESOLUTION NO. 2025-05-_____

**Active Transportation Program Grant Agreement
Grant Terms and Conditions
SAP 177-090-006**

WHEREAS, City of Worthington has applied to the Commissioner of Transportation for a grant from the Active Transportation Fund; and

WHEREAS, the Commissioner of Transportation has given notice that funding for this project is available; and

WHEREAS, the amount of the grant has been determined to be \$1,000,000.00 by reason of the lowest responsible bid;

NOW THEREFORE, be it resolved that City of Worthington does hereby agree to the terms and conditions of the grant consistent with Minnesota Statutes, section 174.52, and will pay any additional amount by which the cost exceeds the estimate, and will return to the Active Transportation Fund any amount appropriated for the project but not required. The proper city officers are authorized to execute a grant agreement and any amendments thereto with the Commissioner of Transportation concerning the above-referenced grant.

Adopted by the City Council of the City of Worthington, Minnesota, this ____ day of _____, 2025.

(SEAL)

Rick Von Holdt, Mayor

Attest: _____
Mindy L. Eggers, City Clerk

CITY OF WORTHINGTON, MINNESOTA

CONSTRUCTION PLANS FOR

EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS

SANITARY SEWER, WATERMAIN, STORM SEWER, REGIONAL STORMWATER BASIN,
STREET CONSTRUCTION AND TURF RESTORATION

MAY, 2025

RESOURCE LIST

CITY OF WORTHINGTON

City Hall
303 9th Street
Worthington, MN 56187
(507) 372-8600

City Administrator:
Steve Robinson

Mayor: Rick von Holdt

City Council Members:
Larry Janssen
Amy Ernst
Christopher Kielblock
Mike Kuhle
Dennis Weber

City Engineer:
Hyunmyeong Goo

Consultant Engineer:
Travis L. Winter P.E.
Bolton & Menk, Inc.
1501 S State Street, Suite 100
Fairmont, MN 56031
(507) 238-4738

Public Works Director:
Todd Wietzema

WATER/WASTEWATER/ELECTRIC
Worthington Public Utilities
Scott Hain
318 9th Street
Worthington, MN 56187
(507) 372-8688

Utility Coordinator:
Eric Roos
(507) 360-9017

Water Superintendent:
Quinn Koplin
(507) 372-8680 (general)

Wastewater Superintendent:
Jeremy Braaksma
(507) 360-9690

Electric Superintendent:
Joe Byrne
(507) 372-8680 (general)

UTILITIES

GAS
MINNESOTA ENERGY RESOURCES
Alex Deleon
650 Sherwood Street
Worthington, MN 56187
(507) 529-5113 (office)
(507) 848-1067 (cell)

Ryan Schaefer
(507) 358-6558 (cell)

COMMUNICATIONS
FRONTIER COMMUNICATIONS
Chris Pollack (WI, MN, NE, IA)
(715) 297-4773 (cell)

MEDIACOM / XTREAM
Mike Klingenberg
948 22nd Ave South
Brookings, SD 57006
(605) 691-3978

BLUEPEAK BROADBAND
David Stahl
5100 South Broadband Lane
Sioux Falls, SD 57108
(605) 681-4082

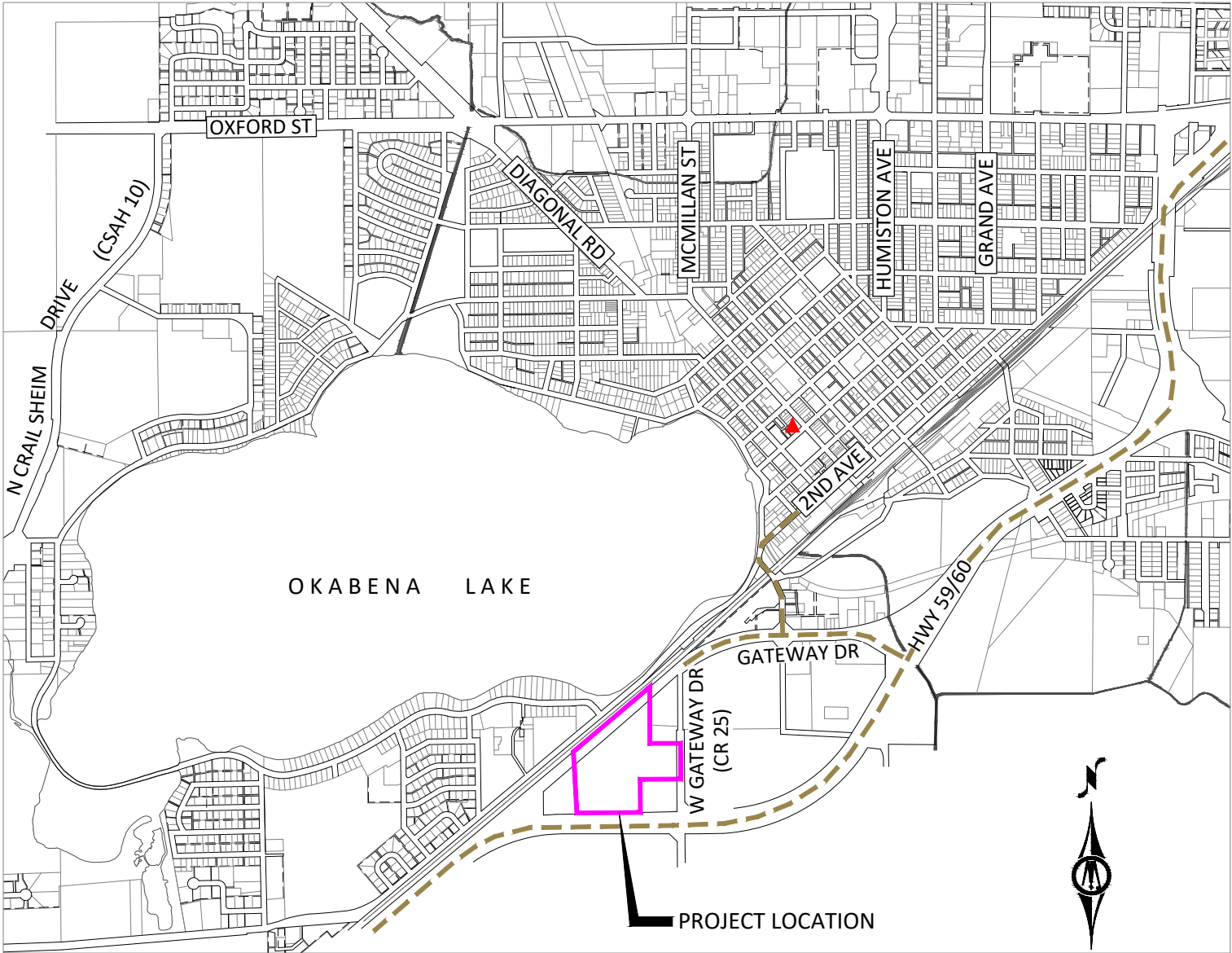
SDN COMMUNICATIONS
Tyler Larson
2900 West 10th Street
Sioux Falls, SD 57104
(605) 978-7341
(605) 310-8301 (cell)

MIDCO
Derrick Abraham
1599 South State Highway 15
Fairmont, MN 56031
(507) 848-5809 (office)

COGENT COMMUNICATIONS
Kevin Persons
Engineer OSP
email: kpersons@cogentco.com

SOUTHWEST BROADBAND
Travis M. Thies
110 S. Hwy 86
Lakefield, MN 56150
(507) 662-4001 (office)
(507) 822-6909 (cell)

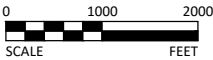
Ben _____
(507) 822-6907 (cell)



MAP LEGEND

- PROJECT LIMITS
- BID LOCATION
- HAUL ROUTE

MAP OF THE
CITY OF WORTHINGTON
NOBLES COUNTY, MN



NOTE: EXISTING UTILITY INFORMATION SHOWN ON THIS PLAN HAS BEEN PROVIDED BY THE UTILITY OWNER. THE CONTRACTOR SHALL FIELD VERIFY EXACT LOCATIONS PRIOR TO COMMENCING CONSTRUCTION AS REQUIRED BY STATE LAW. NOTIFY GOPHER STATE ONE CALL, 1-800-252-1166 OR 651-454-0002.

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D UNLESS OTHERWISE NOTED. THIS UTILITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF C/ASCE 38-22, ENTITLED "STANDARD GUIDELINE FOR INVESTIGATING AND DOCUMENTING EXISTING UTILITIES".

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



1501 SOUTH STATE ST, SUITE 100
FAIRMONT, MN 56031
Phone: (507) 238-4738
Email: Fairmont@bolton-menk.com
www.bolton-menk.com



DESIGNED	JTO	NO.	ISSUED FOR	DATE
DRAWN	JVZ	95% REV	05/22/2025	
CHECKED	TLW			
CLIENT PROJ. NO.				

<div>⊕ BM=1586.71</div> <div>TOP NUT HYD 19+50, 20' LT</div> <div>INTERSECTION OF FLOWER LN & GATEWAY DR</div>	PROJECT DATUM: NOBLES COUNTY	RECORD DRAWING INFORMATION	
		OBSERVER:	
		CONTRACTOR:	
		DATE:	
CITY OF WORTHINGTON, MINNESOTA		SHEET	
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS		G0.01	
TITLE SHEET			

EXISTING TOPOGRAPHIC SYMBOLS

	ACCESS GRATE		REGULATION STATION GAS
	AIR CONDITION UNIT		SATELLITE DISH
	ANTENNA		SIGN TRAFFIC
	AUTO SPRINKLER CONNECTION		SIGNAL CONTROL CABINET
	BARRICADE PERMANENT		SOIL BORING
	BASKETBALL POST		SIREN
	BENCH		TELEPHONE BOOTH
	BIRD FEEDER		TILE INLET
	BOLLARD		TILE OUTLET
	BUSH		TILE RISER
	CATCH BASIN RECTANGULAR CASTING		TRANSFORMER-ELECTRIC
	CATCH BASIN CIRCULAR CASTING		TREE-CONIFEROUS
	CURB STOP		TREE-DEAD
	CLEAN OUT		TREE-DECIDUOUS
	CULVERT END		TREE STUMP
	DRINKING FOUNTAIN		TRAFFIC ARM BARRIER
	DOWN SPOUT		TRAFFIC SIGNAL
	ELECTRIC CAR CHARGE STATION		TRASH CAN
	FILL PIPE		UTILITY MARKER
	FIRE HYDRANT		VALVE
	FLAG POLE		VALVE POST INDICATOR
	FLARED END / APRON		VALVE VAULT
	FUEL PUMP		VAULT
	GRILL		VENT PIPE
	GUY WIRE ANCHOR		WATER SPIGOT
	HANDHOLE		WELL
	HANDICAP SPACE		WETLAND DELINEATED MARKER
	IRRIGATION SPRINKLER HEAD		WETLAND
	IRRIGATION VALVE BOX		WET WELL
	LIFT STATION CONTROL PANEL		YARD HYDRANT
	LIFT STATION		
	LIGHT POLE		
	MAILBOX		

PROPOSED TOPOGRAPHIC SYMBOLS

	CLEANOUT
	MANHOLE
	LIFT STATION
	STORM SEWER CIRCULAR CASTING
	STORM SEWER RECTANGULAR CASTING
	STORM SEWER FLARED END / APRON
	STORM SEWER OUTLET STRUCTURE
	STORM SEWER OVERFLOW STRUCTURE
	CURB BOX
	FIRE HYDRANT
	WATER VALVE
	WATER REDUCER
	WATER BEND
	WATER TEE
	WATER CROSS
	WATER SLEEVE
	WATER CAP / PLUG
	RIP RAP
	DRAINAGE FLOW
	TRAFFIC SIGNS

SURVEY SYMBOLS

	BENCHMARK LOCATION		CAST IRON MONUMENT
	CONTROL POINT		STONE MONUMENT
	MONUMENT FOUND		

EXISTING TOPOGRAPHIC LINES

	RETAINING WALL
	FENCE
	FENCE-DECORATIVE
	GUARD RAIL
	TREE LINE
	BUSH LINE

SURVEY LINES

	CONTROLLED ACCESS
	BOUNDARY
	CENTERLINE
	EXISTING EASEMENT LINE
	PROPOSED EASEMENT LINE
	EXISTING LOT LINE
	PROPOSED LOT LINE
	EXISTING RIGHT-OF-WAY
	PROPOSED RIGHT-OF-WAY
	SETBACK LINE
	SECTION LINE
	QUARTER LINE
	SIXTEENTH LINE
	TEMPORARY EASEMENT

EXISTING UTILITY LINES

	FORCEMAIN
	SANITARY SEWER
	SANITARY SERVICE
	STORM SEWER
	STORM SEWER DRAIN TILE
	WATERMAIN
	WATER SERVICE
	RECLAIMED WATER

PROPOSED UTILITY LINES

	FORCEMAIN
	SANITARY SEWER
	SANITARY SERVICE
	STORM SEWER
	STORM SEWER DRAIN TILE
	WATERMAIN
	WATER SERVICE
	PIPE CASING
	TRENCHLESS PIPE (PLAN VIEW)
	TRENCHLESS PIPE (PROFILE VIEW)

GRADING INFORMATION

	EXISTING CONTOUR MINOR
	EXISTING CONTOUR MAJOR
	PROPOSED CONTOUR MINOR
	PROPOSED CONTOUR MAJOR
	PROPOSED GRADING LIMITS / SLOPE LIMITS
	PROJECT LIMITS
	PROPOSED SPOT ELEVATION
	RISE:RUN (SLOPE)

HATCH PATTERNS

	BITUMINOUS		GRAVEL
	CONCRETE		

EXISTING PRIVATE UTILITY LINES

NOTE:
EXISTING UTILITY INFORMATION SHOWN ON THIS PLAN HAS BEEN PROVIDED BY THE UTILITY OWNER. THE CONTRACTOR SHALL FIELD VERIFY EXACT LOCATIONS PRIOR TO COMMENCING CONSTRUCTION AS REQUIRED BY STATE LAW. NOTIFY GOPHER STATE ONE CALL, 1-800-252-1166 OR 651-454-0002.

THE SUBSURFACE UTILITY INFORMATION IN THIS PLAN IS UTILITY QUALITY LEVEL D UNLESS OTHERWISE NOTED. THIS UTILITY LEVEL WAS DETERMINED ACCORDING TO THE GUIDELINES OF CI/ASCE 38-22, ENTITLED "STANDARD GUIDELINE FOR INVESTIGATING AND DOCUMENTING EXISTING UTILITIES".

	UNDERGROUND FIBER OPTIC
	UNDERGROUND ELECTRIC
	UNDERGROUND GAS
	UNDERGROUND COMMUNICATION
	OVERHEAD ELECTRIC
	OVERHEAD COMMUNICATION
	OVERHEAD UTILITY

UTILITIES IDENTIFIED WITH A QUALITY LEVEL :

LINE TYPES FOLLOW THE FORMAT: UTILITY TYPE - QUALITY LEVEL
EXAMPLE: UNDERGROUND GAS, QUALITY LEVEL A
UTILITY QUALITY LEVEL (A,B,C,D) DEFINITIONS CAN BE FOUND IN CI/ASCE 38-22.

UTILITY QUALITY LEVELS:

QUALITY LEVEL D: PROVIDES THE MOST BASIC LEVEL OF INFORMATION. IT INVOLVES COLLECTING DATA FROM EXISTING UTILITY RECORDS. RECORDS MAY INCLUDE AS-BUILT DRAWINGS, DISTRIBUTION AND SERVICES MAPS, EXISTING GEOGRAPHIC INFORMATION SYSTEM DATABASES, CONSTRUCTION PLANS, ETC.

QUALITY LEVEL C: INVOLVES SURVEYING VISIBLE SUBSURFACE UTILITY STRUCTURES SUCH AS MANHOLES, HAND-HOLES, UTILITY VALVES AND METERS, FIRE HYDRANTS, PEDESTALS AND UTILITY MARKERS, AND THEN CORRELATING THE INFORMATION WITH EXISTING UTILITY RECORDS TO CREATE COMPOSITE DRAWINGS. INCLUDES QUALITY LEVEL D ACTIVITIES.

QUALITY LEVEL B: INVOLVES DESIGNATING THE HORIZONTAL POSITION OF SUBSURFACE UTILITIES THROUGH SURFACE DETECTION METHODS AND COLLECTING THE INFORMATION THROUGH A SURVEY METHOD. INCLUDES QUALITY LEVEL C AND D TASKS.

QUALITY LEVEL A: PROVIDES THE HIGHEST LEVEL OF ACCURACY. IT INVOLVES LOCATING OR POTHOLING UTILITIES AS WELL AS ACTIVITIES IN QUALITY LEVELS B, C, AND D. THE LOCATED FACILITY INFORMATION IS SURVEYED AND MAPPED AND THE DATA PROVIDES PRECISE PLAN AND PROFILE INFORMATION.

ABBREVIATIONS

A	ALGEBRAIC DIFFERENCE	GRAV	GRAVEL	RSC	RIGID STEEL CONDUIT
ADJ	ADJUST	GU	GUTTER	RT	RIGHT
ALT	ALTERNATE	GV	GATE VALVE	SAN	SANITARY SEWER
B-B	BACK TO BACK	HDPE	HIGH DENSITY POLYETHYLENE	SCH	SCHEDULE
BIT	BITUMINOUS	HH	HANDHOLE	SERV	SERVICE
BLDG	BUILDING	HP	HIGH POINT	SHLD	SHOULDER
BMP	BEST MANAGEMENT PRACTICE	HWL	HIGH WATER LEVEL	STA	STATION
BR	BEGIN RADIUS	HYD	HYDRANT	STD	STANDARD
BV	BUTTERFLY VALVE	I	INVERT	STM	STORM SEWER
CB	CATCH BASIN	K	CURVE COEFFICIENT	TC	TOP OF CURB
C&G	CURB AND GUTTER	L	LENGTH	TE	TEMPORARY EASEMENT
CIP	CAST IRON PIPE	LO	LOWEST OPENING	TEMP	TEMPORARY
CIPP	CURED-IN-PLACE PIPE	LP	LOW POINT	TNH	TOP NUT HYDRANT
CL	CENTER LINE	LT	LEFT	TP	TOP OF PIPE
CL	CLASS	MAX	MAXIMUM	TYP	TYPICAL
CLVT	CULVERT	MH	MANHOLE	VCP	VITRIFIED CLAY PIPE
CMP	CORRUGATED METAL PIPE	MIN	MINIMUM	VERT	VERTICAL
C.O.	CHANGE ORDER	MR	MID RADIUS	VPC	VERTICAL POINT OF CURVE
COMM	COMMUNICATION	NIC	NOT IN CONTRACT	VPI	VERTICAL POINT OF INTERSECTION
CON	CONCRETE	NMC	NON-METALLIC CONDUIT	VPT	VERTICAL POINT OF TANGENT
CSP	CORRUGATED STEEL PIPE	NTS	NOT TO SCALE	WM	WATERMAIN
DIA	DIAMETER	NWL	NORMAL WATER LEVEL		
DIP	DUCTILE IRON PIPE	OHW	ORDINARY HIGH WATER LEVEL		
DWY	DRIVEWAY	PC	POINT OF CURVE	AC	ACRES
E	EXTERNAL CURVE DISTANCE	PCC	POINT OF COMPOUND CURVE	CF	CUBIC FEET
ELEC	ELECTRIC	PE	PERMANENT EASEMENT	CV	COMPACTED VOLUME
ELEV	ELEVATION	PED	PEDESTRIAN, PEDESTAL	CY	CUBIC YARD
EOF	EMERGENCY OVERFLOW	PERF	PERFORATED PIPE	EA	EACH
ER	END RADIUS	PERM	PERMANENT	EV	EXCAVATED VOLUME
ESMT	EASEMENT	PI	POINT OF INTERSECTION	LB	POUND
EX	EXISTING	PL	PROPERTY LINE	LF	LINEAR FEET
FES	FLARED END SECTION	PRC	POINT OF REVERSE CURVE	LS	LUMP SUM
F-F	FACE TO FACE	PT	POINT OF TANGENT	LV	LOOSE VOLUME
FF	FINISHED FLOOR	PVC	POLYVINYL CHLORIDE PIPE	SF	SQUARE FEET
F&I	FURNISH AND INSTALL	PVMT	PAVEMENT	SV	STOCKPILE VOLUME
FM	FORCEMAIN	R	RADIUS	SY	SQUARE YARD
FO	FIBER OPTIC	R/W	RIGHT-OF-WAY		
F.O.	FIELD ORDER	RCP	REINFORCED CONCRETE PIPE		
GRAN	GRANULAR	RET	RETAINING		

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



BOLTON & MENK

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DESIGNED	JTO	NO.	ISSUED FOR	DATE
DRAWN	JVZ	95% REV	05/22/2025	
CHECKED	TLW			
CLIENT PROJ. NO.				

CITY OF WORTHINGTON, MINNESOTA

EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS

LEGEND

SHEET

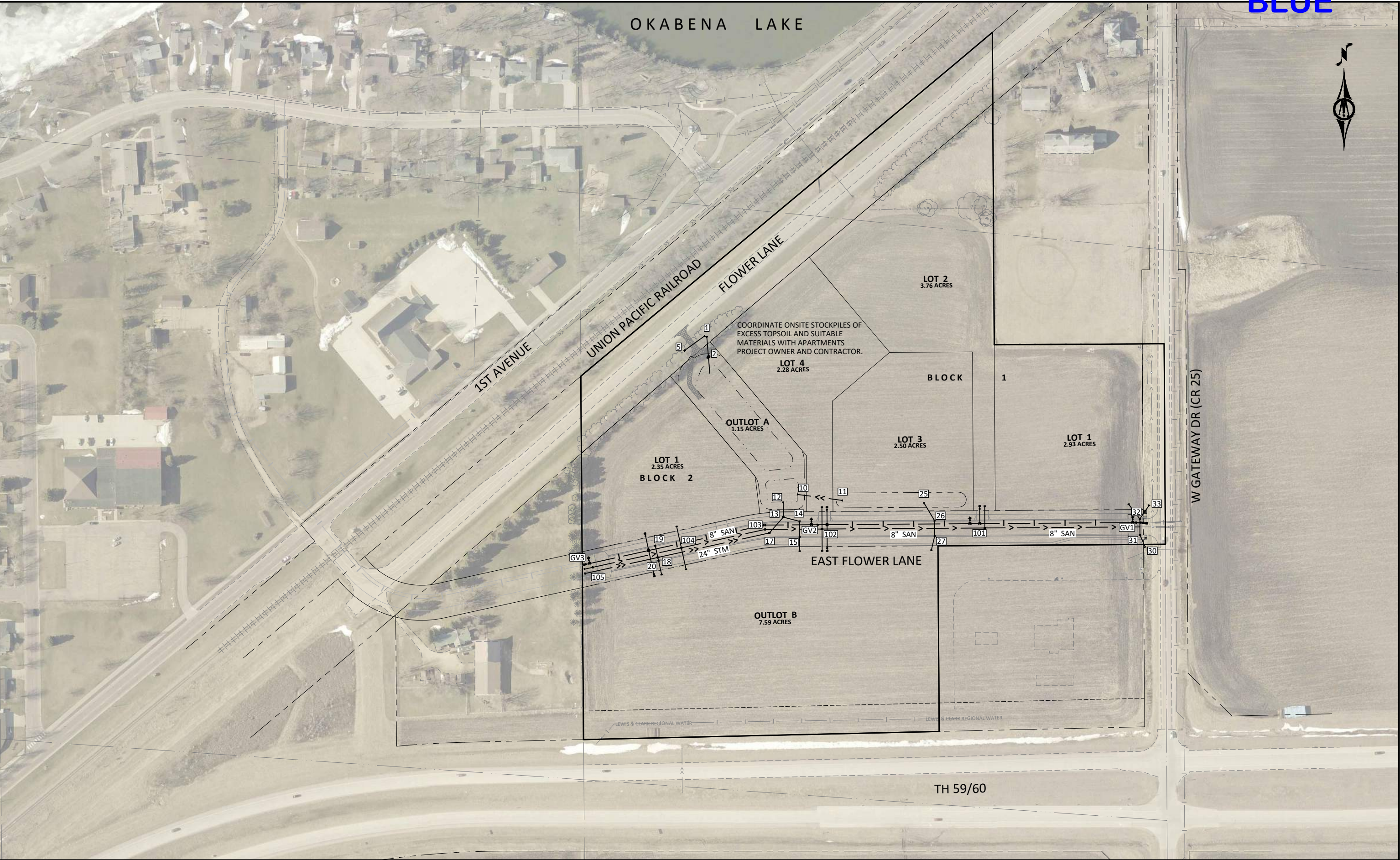
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STATEMENT OF ESTIMATED QUANTITIES								
ITEM NO.	ITEM	NOTES	UNIT	ROADWAY	STORM	SANITARY	WATERMAIN	TOTAL
SCHEDULE A								
1	MOBILIZATION		LUMP SUM	0.58	0.20	0.09	0.13	1.00
2	CLEARING AND GRUBBING		ACRE		0.20			0.20
3	REMOVE GATE VALVE & BOX		EACH				2.00	2.00
4	REMOVE HYDRANT		EACH				1.00	1.00
5	REMOVE WATERMAIN		LIN FT				34.00	34.00
6	REMOVE WATER SERVICE		LIN FT				48.00	48.00
7	REMOVE CURB STOP & BOX		EACH				1.00	1.00
8	EXCAVATION - COMMON (P)	(1)	CU YD	6,144.00	10,471.00			16,615.00
9	EXCAVATION - MUCK	(2)	CU YD	400.00				400.00
10	COMMON EMBANKMENT (CV) (P)	(3)	CU YD	7,746.00	1,248.00			8,994.00
11	STABILIZING AGGREGATE (CV)	(2)	CU YD	400.00				400.00
12	GEOTEXTILE FABRIC TYPE 4 (MODIFIED)		SQ YD	6,080.00				6,080.00
13	AGGREGATE SURFACING CLASS 5		TON		120.00			120.00
14	EXPLORATORY EXCAVATION	(4)	HOURL		10.00	5.00	5.00	20.00
15	STREET SWEEPER (WITH PICKUP BROOM)		HOURL	10.00	10.00	10.00	10.00	40.00
16	GRANULAR BACKFILL (LV)	(5)	CU YD		250.00	215.00	250.00	715.00
17	GRANULAR FOUNDATION (CV)	(5)	CU YD		155.00	115.00	130.00	400.00
18	15" RC APRON		EACH		2.00			2.00
19	18" RC APRON		EACH		4.00			4.00
20	30" RC APRON		EACH		1.00			1.00
21	36" RC APRON		ACRE		1.00			1.00
22	4" PERF PE PIPE DRAIN		LIN FT	2,470.00				2,470.00
23	DRAIN TILE REPAIR	(6)	LIN FT		20.00	20.00	20.00	60.00
24	12" RC PIPE SEWER DES 3006 CL V		LIN FT		60.00			60.00
25	15" RC PIPE SEWER DES 3006 CL V		LIN FT		213.00			213.00
26	18" RC PIPE SEWER DES 3006 CL IV		LIN FT		288.00			288.00
27	24" RC PIPE SEWER DES 3006 CL III		LIN FT		517.00			517.00
28	30" RC PIPE SEWER DES 3006 CL III		LIN FT		73.00			73.00
29	36" RC PIPE SEWER DES 3006 CL III		LIN FT		47.00			47.00
30	CONNECT TO EXISTING SANITARY SEWER		EACH			1.00		1.00
31	8" PVC SANITARY SEWER		LIN FT			1,527.00		1,527.00
32	CONNECT TO EXISTING WATERMAIN		EACH				1.00	1.00
33	INSTALL HYDRANT UNIT		EACH				4.00	4.00
34	INSTALL 6" GATE VALVE & BOX		EACH				1.00	1.00
35	INSTALL 8" GATE VALVE & BOX		EACH				4.00	4.00
36	INSTALL 10" GATE VALVE AND CONSTRUCT MANHOLE		EACH				3.00	3.00
37	2" CORP STOP & SADDLE		EACH				1.00	1.00
38	2" CURB STOP & BOX		EACH				1.00	1.00
39	2" TYPE PE WATER SERVICES		LIN FT				50.00	50.00
40	6" PVC WATERMAIN		LIN FT				35.00	35.00
41	8" PVC WATERMAIN		LIN FT				205.00	205.00
42	10" PVC WATERMAIN		LIN FT				1,295.00	1,295.00
43	WATERMAIN FITTINGS		POUNDS				2,920.00	2,920.00
44	ADJUST SANITARY MANHOLE	(7)	LIN FT			2.60		2.60
45	CONST STANDARD 48" MANHOLE, SANITARY		EACH			5.00		5.00
46	CONST EXTRA DEPTH MANHOLE, SANITARY		LIN FT			10.20		10.20
47	CONST DRAINAGE STRUCTURE R-1 (2X3)		LIN FT		23.30			23.30
48	CONST DRAINAGE STRUCTURE 48-4020		LIN FT		10.00			10.00
49	CONST DRAINAGE STRUCTURE 60-4020		LIN FT		11.10			11.10
50	CONST DRAINAGE STRUCTURE 48-4022		LIN FT		11.50			11.50
51	CONST DRAINAGE STRUCTURE 60-4022		LIN FT		5.70			5.70
52	CONST POND CONTROL STRUCTURE		EACH		1.00			1.00
53	CASTING ASSEMBLY, R-2577		EACH		1.00			1.00
54	CASTING ASSEMBLY, R-3067-L		EACH		8.00			8.00
55	CASTING ASSEMBLY, R-1733		EACH		1.00	1.00	1.00	3.00
56	RANDOM RIPRAP, CLASS III		TON		90.00			90.00
57	4" CONCRETE WALK		SQ FT	6,366.00				6,366.00
58	6" CONCRETE WALK		SQ FT	9,844.00				9,844.00
59	CONCRETE CURB & GUTTER DESIGN B624		LIN FT	2,470.00				2,470.00
60	8" CONCRETE DRIVEWAY PAVEMENT		SQ YD	197.00				197.00
61	TRUNCATED DOMES		SQ FT	24.00				24.00
62	TRAFFIC CONTROL		LUMP SUM	0.58	0.20	0.09	0.13	1.00
63	SIGN PANELS TYPE C		SQ FT	6.25				6.25
64	STABILIZED CONSTRUCTION EXIT		LUMP SUM	0.50	0.50			1.00
65	STORM DRAIN INLET PROTECTION		EACH	11.00	2.00			13.00
66	SILT FENCE, TYPE MS		LIN FT	550.00				550.00
67	SEDIMENT CONTROL LOG TYPE STRAW		LIN FT	120.00	60.00			180.00
68	SEDIMENT CONTROL LOG TYPE ROCK		LIN FT	50.00				50.00
69	ROLLED EROSION PREVENTION CATEGORY 30		SQ YD	8,250.00	5,910.00			14,160.00
70	RAPID STABILIZATION METHOD 3	(8)	M GAL	1.00	2.00			3.00
71	SEEDING, WET DITCH MIX	(9)	SQ YD		3,000.00			3,000.00
72	SEEDING, SOUTHERN BOULEVARD MIX	(9)	SQ YD	8,250.00				8,250.00
73	SEEDING, SOUTHERN TALL GRASS MIX	(9)	SQ YD		2,910.00			2,910.00
74	SEEDING, TEMPORARY	(10)	ACRE	3.40	2.50			5.90

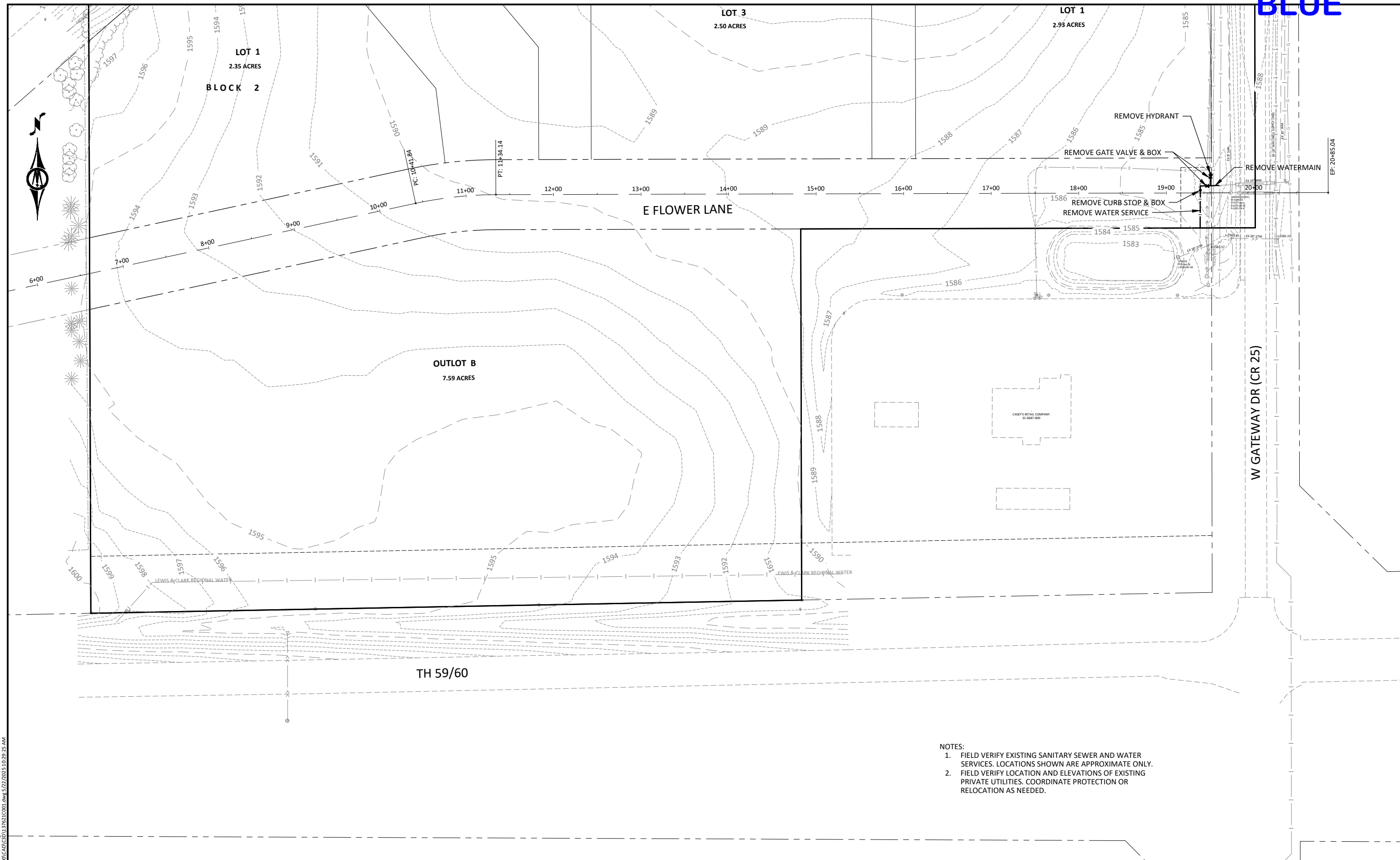
STATEMENT OF ESTIMATED QUANTITIES								
ITEM NO.	ITEM	NOTES	UNIT	ROADWAY	STORM	SANITARY	WATERMAIN	TOTAL
SCHEDULE B - BITUMINOUS PAVEMENT								
75	AGGREGATE BASE CLASS 5 (CV)		CU YD	1,520.00				1,520.00
76	DRAINABLE AGGREGATE BASE, TYPE DSB (CV)		CU YD	1,015.00				1,015.00
77	TYPE SP 9.5 WEARING COURSE MIXTURE (2,C)		TON	435.00				435.00
78	TYPE SP 12.5 NONWEAR COURSE MIXTURE (2,C)		TON	1,245.00				1,245.00
79	CASTING ASSEMBLY R-1733		EACH		2.00	5.00	2.00	9.00
SCHEDULE C - CONCRETE PAVEMENT								
80	AGGREGATE BASE CLASS 5 (CV)		CU YD	1,050.00				1,050.00
81	DRAINABLE AGGREGATE BASE, TYPE DSB (CV)		CU YD	1,015.00				1,015.00
82	TYPE SP 9.5 WEARING COURSE MIXTURE (2,C)		TON	40.00				40.00
83	TYPE SP 12.5 NONWEAR COURSE MIXTURE (2,C)		TON	105.00				105.00
84	7" CONCRETE PAVEMENT		SQ YD	4,390.00				4,390.00
85	CASTING ASSEMBLY R-1955-1		EACH		2.00	5.00	2.00	9.00

- NOTES ON ESTIMATED QUANTITIES:
- (P) DENOTES PLAN QUANTITY
- (1) INCLUDES TOPSOIL STRIPPING UNDER STREET IMPROVEMENTS.
- (2) NOMINAL QUANTITY PROVIDED FOR POOR SOIL CONDITIONS, WILL VARY WITH FIELD CONDITIONS.
- (3) INCLUDES TOPSOIL PLACEMENT.
- (4) NOMINAL QUANTITY PROVIDED, WILL VARY WITH FIELD CONDITIONS.
- (5) NOMINAL QUANTITY PROVIDED FOR POOR TRENCH CONDITIONS, WILL VARY WITH FIELD CONDITIONS.
- (6) NOMINAL QUANTITY PROVIDED FOR REPAIRS OF UNKNOWN TILE, WILL VARY WITH FIELD CONDITIONS.
- (7) INCLUDES ALL WORK TO RAISE EXISTING MANHOLE TO NEW RIM ELEVATION. CASTING PAID SEPARATELY.
- (8) NOMINAL QUANTITY FOR EMERGENCY TEMPORARY STABILIZATION, WILL VARY WITH FIELD CONDITIONS.
- (9) INCLUDES SUBSOILING, SOIL BED PREP, SEEDING, SEED MIXTURE, FERTILIZER AND MAINTENANCE REQUIRED FOR TURF ESTABLISHMENT.
- (10) INCLUDES SUBSOILING, SOIL BED PREP, SEEDING, SEED MIXTURE, FERTILIZER, STABILIZING COVER AND MAINTENANCE REQUIRED FOR TURF ESTABLISHMENT.

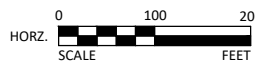
THE FOLLOWING STANDARD PLATES, APPROVED BY THE FEDERAL HIGHWAY ADMINISTRATION, SHALL APPLY ON THIS PROJECT	
STANDARD PLATES	
PLATE NO.	DESCRIPTION
1070N	SUPPLEMENTAL PAVEMENT REINFORCEMENT
1103M	TYPICAL DOWEL BAR ASSEMBLY - CONTRACTION JOINT
3000M	REINFORCED CONCRETE PIPE (6 SHEETS)
3006H	GASKET JOINT FOR R.C. PIPE (2 SHEETS)
3100G	CONCRETE APRON FOR REINFORCED CONCRETE PIPE
3133D	RIPRAP AT RCP OUTLETS
3145G	CONCRETE PIPE OR PRECAST BOX CULVERT TIES
4007C	PRECAST MECHANICAL JOINT SEWER MANHOLE
4010I	CONCRETE ADJUSTING RINGS
4011E	PRECAST CONCRETE BASE
4020J	MANHOLE OR CATCH BASIN (FOR USE WITH OR WITHOUT TRAFFIC LOADS) (2 SHEETS)
4022A	MANHOLE OR CATCH BASIN COVER (3 FT. × 2 FT. OPENING)
4026B	CONCRETE ENCASED CONCRETE ADJUSTING RINGS
4180J	MANHOLE OR CATCH BASIN STEP
7000E	INTEGRANT CURBS (DESIGN B, DESIGN V AND DESIGN D)
7038A	DETECTABLE WARNING SURFACE TRUNCATED DOMES
7100H	CONCRETE CURB AND GUTTER (DESIGN B AND DESIGN V)
7112C	INSTALLATION & REINFORCEMENT OF CATCH BASIN & MANHOLE CASTINGS (CONCRETE INTEGRANT CURBS)
8000K	TEMPORARY CHANNELIZERS (3 SHEETS)



BLUE



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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TRAVIS L. WINTER

LIC. NO. 46649 DATE MAY 27, 2025



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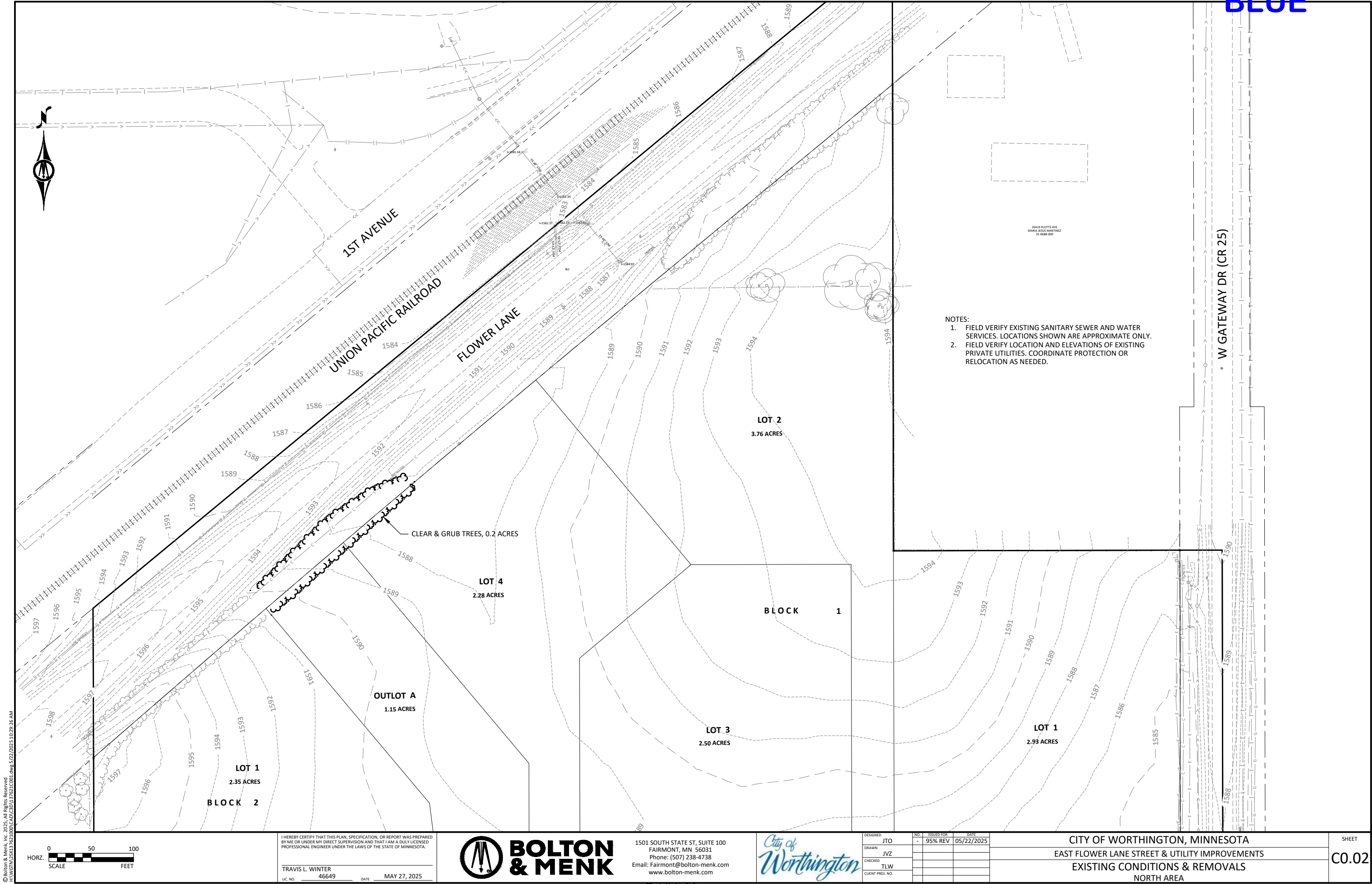


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CLIENT PROJ. NO.			

CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
EXISTING CONDITIONS & REMOVALS
SOUTH AREA

SHEET

C0.01



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SANITARY SEWER TABLE													
STRUCTURE NO.	STATION	OFFSET	MANHOLE					CONNECT TO EXIST SAN SEWER (EACH)	PIPE 8" PVC SDR 35 (LIN FT)	DRAINS TO			NOTES
			CASTING ASSEMBLY (1)	RIM ELEV.	OUTLET ELEV.	0' - 10' STANDARD 48" MANHOLE (EACH)	EXTRA DEPTH 48" MANHOLE (LIN FT)			STRUCTURE NO.	PIPE GRADE (%)	INVERT OR CONNECTION ELEV.	
STUB W	100+66.07	CL			1581.46				19	105	0.40%	1581.38	(3)
105	100+85.40	CL	R-1955-1	1594.22	1581.28	1	2.9		207	104	0.40%	1580.45	
STUB N	102+92.44	50' LT			1580.95				50	104	1.00%	1580.45	(3)
STUB S	102+92.44	50' RT			5080.95				50	104	1.00%	1580.45	(3)
104	102+92.44	CL	R-1955-1	1592.45	1580.35	1	2.1		196	103	0.40%	1579.57	
103	104+88.16	CL	R-1955-1	1590.45	1579.47	1	1.0		130	102	0.40%	1578.95	
STUB N	106+18.31	50' LT			1579.45				50	102	1.00%	1578.95	(3)
STUB S	106+18.31	50' RT			1579.45				50	102	1.00%	1578.95	(3)
102	106+18.31	CL	R-1955-1	1590.22	1578.85	1	1.4		372	101	0.40%	1577.36	
STUB N	109+90.10	50' LT			1577.86				50	101	1.00%	1577.36	(3)
101	109+90.10	CL	R-1955-1	1590.03	1577.26	1	2.8	1	353	EX PIPE	0.40%	1575.85	
			TOTALS			5	10.2	1	1,527				

- (1) REFERS TO NEENAH FOUNDARY CO. DESIGNATION. ASSEMBLY REFERS TO CASTING FOR USE WITH SCHEDULE C CONCRETE PAVEMENT. SUBSTITUE R-1733 FRAME FOR SCHEDULE B BITUMINOUS PAVEMENT
- (2) PROVIDE SHEAR RESISTANT STRONG BACK FERNCO FOR CONNECTING TO EXISTING SANITARY SEWER MAIN (INCIDENTAL).
- (3) PROVIDE WATER-TIGHT CAP WITH 2" X 2" STAKE MARKING END OF STUB, INCIDENTAL.

STORM SEWER TABLE																											
STRUCT. NO.	STATION	LOCATION (OFFSET)	CASTING ASSEMBLY (1)	RIM ELEV.	OUTLET ELEV.	SUMP ELEV	STRUCTURE						APRON				PIPE						DRAINS TO			NOTES	
							DES R-1 2' X 3' (LIN FT) (2)	DES 48" 4020 (LIN FT)	DES 60" 4020 (LIN FT)	DES 48" 4022 (LIN FT) (2)	DES 60" 4022 (LIN FT) (2)	POND CONTROL STRUCTURE (EACH)	15" RC (EACH)	18" RC (EACH)	30" RC (EACH)	36" RC (EACH)	12" RCP CL V (LIN FT)	15" RCP CL V (LIN FT)	18" RCP CL IV (LIN FT)	24" RCP CL III (LIN FT)	30" RCP CL III (LIN FT)	36" RCP CL III (LIN FT)	STRUCT. NO.	PIPE GRADE (%)	INVERT OR CONNECTION ELEV.		
5	1+24.28	52.95' LT	R-2577	1588.90	1584.90	1583.90		5.0					1					57					4	0.70%	1584.50		
3	0+67.46	0.0'			1580.00																	38	2	0.00%	1580.00		
2	1+05.00	0.0'	SKIMMER GRATE	1588.00	1584.50	1579.00					1				1							47	1	0.10%	1584.45	APRON INCLUDES TRASH GUARD	
22	612+29.11	0.0'			1588.32													170				18	0.50%	1587.47	PROVIDE WATER-TIGHT PLUG (INCIDENTAL)		
21	610+58.94	40.0' LT			1589.10											32						20	2.00%	1588.45	PROVIDE WATER-TIGHT PLUG (INCIDENTAL)		
20	610+58.94	7.67' LT	R-3067-L	1592.60	1588.33	1587.33	5.3										8					18	1.00%	1588.25			
19	610+58.94	27.67' RT	R-3067-L	1592.60	1588.73	1587.73	4.9									28						18	1.00%	1588.45			
18	610+58.94	0.0'	R-1955-1	1592.82	1587.37				5.5										261			17	0.80%	1585.28			
17	607+98.32	0.0'	R-1955-1	1590.28	1585.23			5.0											48			13	0.80%	1584.84			
16	11+66.67	50' RT			1586.00														32			15	1.20%	1585.61	PROVIDE WATER-TIGHT PLUG (INCIDENTAL)		
15	11+66.67	17.67' RT	R-3067-L	1589.60	1585.60	1584.60	5.0											36			14	1.40%	1585.10				
14	11+66.67	17.92' LT	R-3067-L	1589.60	1585.00	1584.00				5.6									38			13	40.00%	1584.85			
13	607+50.00	0.0'	R-1733	1590.38	1584.74				5.6						1						35	12	70.00%	1584.73	APRON INCLUDES TRASH GUARD		
11	12+65.53	65.03' LT			1585.00									2				105				10	0.70%	1584.50	APRON INCLUDES TRASH GUARD		
28	14+67.35	49.78' RT			1586.65													33				27	0.60%	1586.45	PROVIDE WATER-TIGHT PLUG (INCIDENTAL)		
27	14+75.00	17.67' RT	R-3067-L	1589.39	1586.45	1585.45	3.9											35				26	0.70%	1586.20			
26	14+75.00	17.67' LT	R-3067-L	1589.39	1586.19	1585.19	4.2							1					48			25	0.40%	1586.00			
34	19+18.46	52.15' LT			1582.13													42				32	0.30%	1582.00	PROVIDE WATER-TIGHT PLUG (INCIDENTAL)		
33	19+65.41	49.75' LT			1583.50								1					38				32	3.50%	1582.17			
32	19+43.75	18.42' LT	R-3067-L	1586.58	1581.92	1580.92					5.7							36				31	0.60%	1581.7			
31	19+43.75	17.92' RT	R-3067-L	1586.58	1581.69	1580.69				5.9				1				31				30	0.60%	1581.50			
						TOTALS	23.3	10.0	11.1	11.5	5.7	1	2	4	1	1	60	213	288	517	73	47					

- NOTES
- (1) REFERS TO NEENAH FOUNDARY CO. DESIGNATION. R-1955-1 ASSEMBLY REFERS TO CASTING FOR USE WITH SCHEDULE C CONCRETE PAVEMENT. SUBSTITUE R-1733 FRAME FOR SCHEDULE B BITUMINOUS PAVEMENT
- (2) PROVIDE HOLES IN STRUCTURE FOR EDGE DRAIN CONNECTIONS AS SHOWN ON PLANS.

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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025

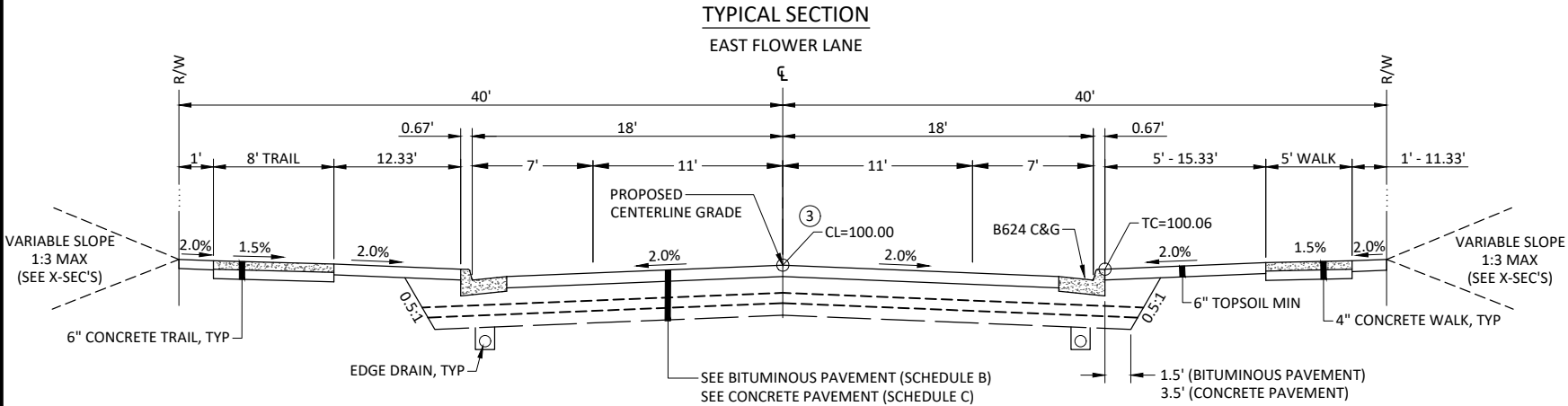


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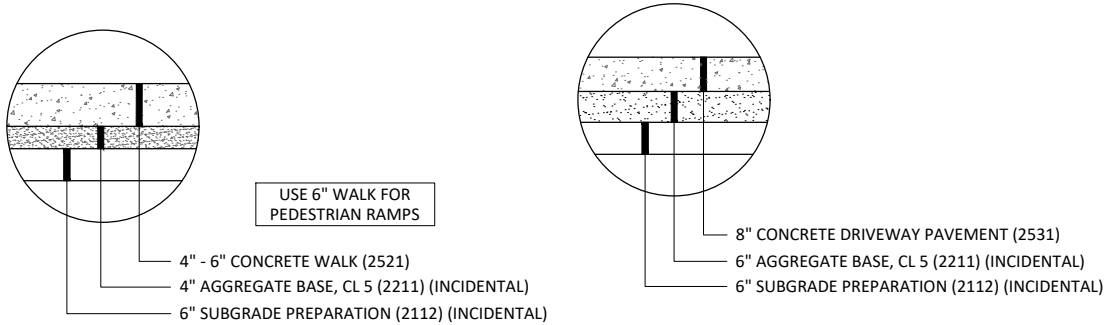


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DRAWN	JVZ						
CHECKED	TLW						
CLIENT PROJ. NO.							

CITY OF WORTHINGTON, MINNESOTA		SHEET C1.01
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS		
TABLES		

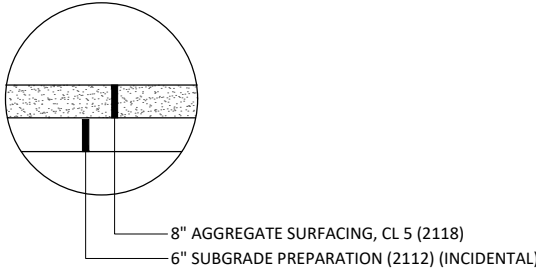


- NOTES:
1. ANY VARIATIONS FROM THE TYPICAL SECTION DIMENSIONS ARE SHOWN ON THE PLAN DRAWINGS.
 2. PAVEMENT SLOPES AT INTERSECTION LOCATIONS MAY VARY FROM THOSE SHOWN ON THE TYPICAL SECTION.
 3. ELEVATIONS SHOWN ARE RELATIVE TO C/L AND DO NOT RELATE TO ACTUAL CONSTRUCTION GRADES.
 4. SEE SHEET C8.03 FOR ADDITIONAL INFORMATION ON TOPSOIL STRIPPING AND SUITABLE FILL UNDER THE STREET AREA.

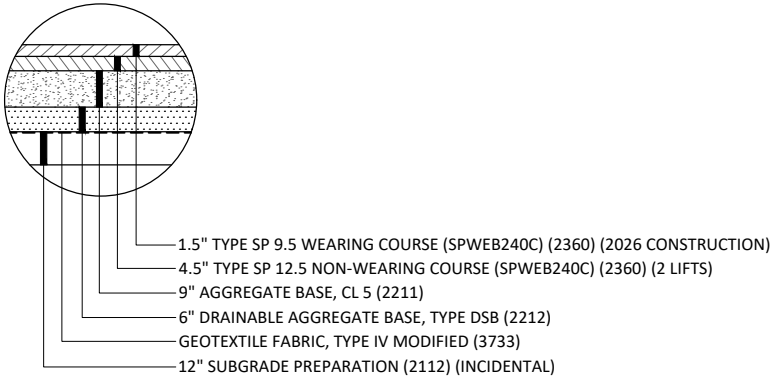


CONCRETE WALK
NOT TO SCALE

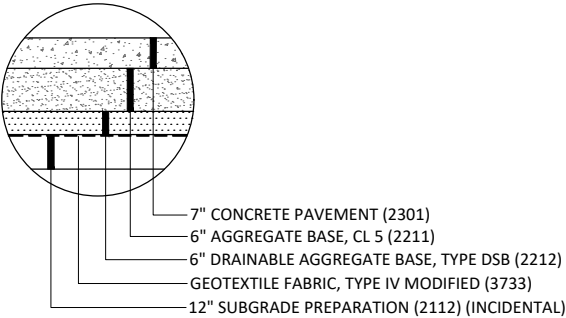
CONCRETE DRIVEWAY PAVEMENT
NOT TO SCALE



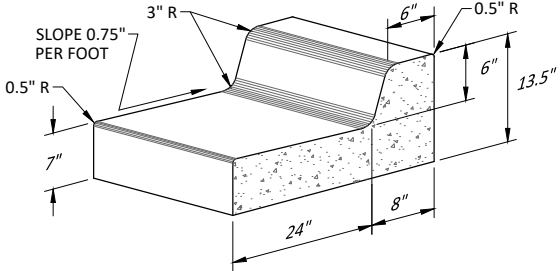
MAINTENANCE ACCESS - AGGREGATE SURFACING
NOT TO SCALE



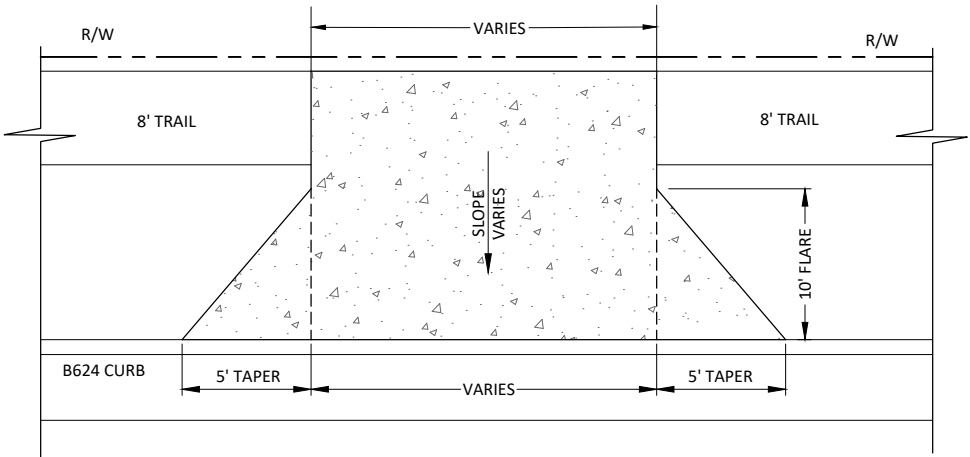
BITUMINOUS PAVEMENT (SCHEDULE B)
NOT TO SCALE



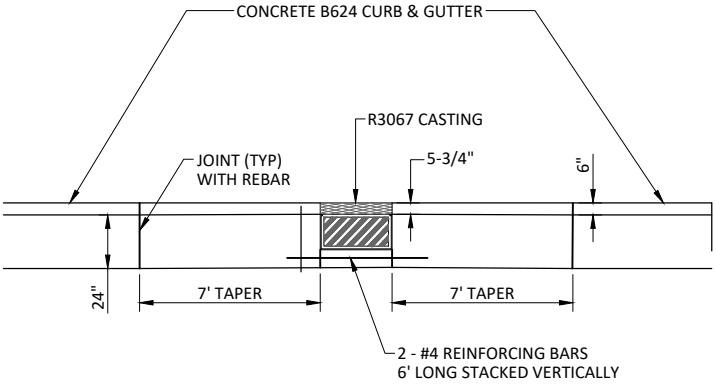
CONCRETE PAVEMENT (SCHEDULE C)
NOT TO SCALE



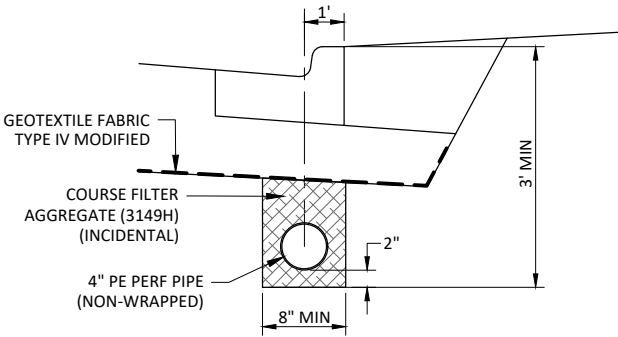
CONCRETE CURB & GUTTER
DESIGN B624
NOT TO SCALE



CONCRETE DRIVEWAY DETAIL
NOT TO SCALE



CATCHBASIN - CURB DETAIL FOR
R-3067 IN "B" STYLE CURB
NOT TO SCALE



SUBSURFACE EDGE DRAIN
NOT TO SCALE

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P:\WORTHINGTON\2513762100\CON\CD\11762100.dwg 5/22/2025 10:29:50 AM

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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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FAIRMONT, MN 56031
Phone: (507) 238-4738
Email: Fairmont@bolton-menk.com
www.bolton-menk.com

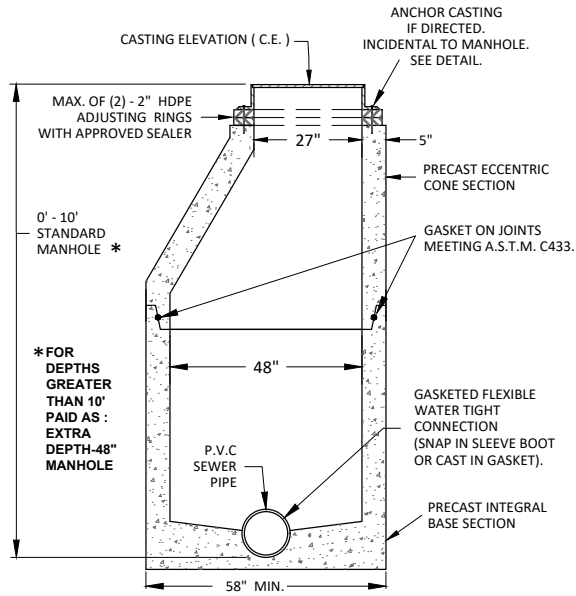


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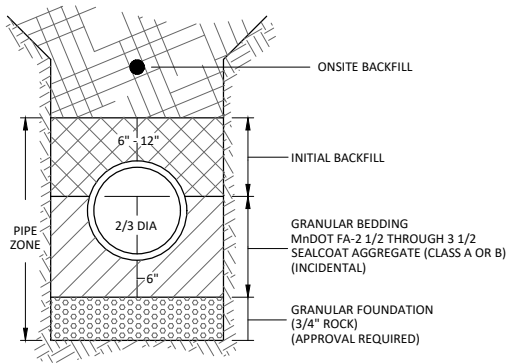
CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
TYPICAL SECTIONS

SHEET

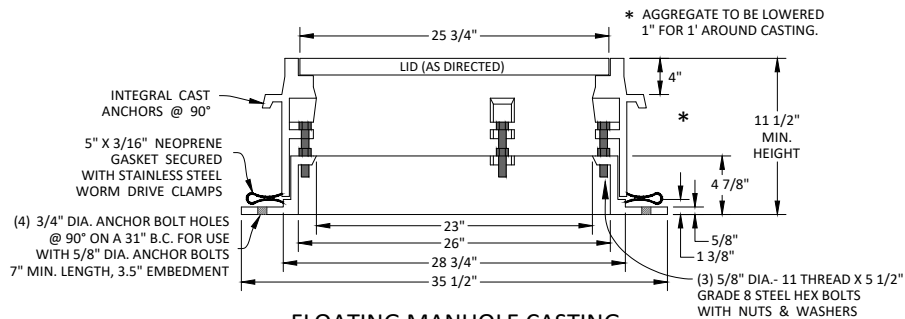
C1.02



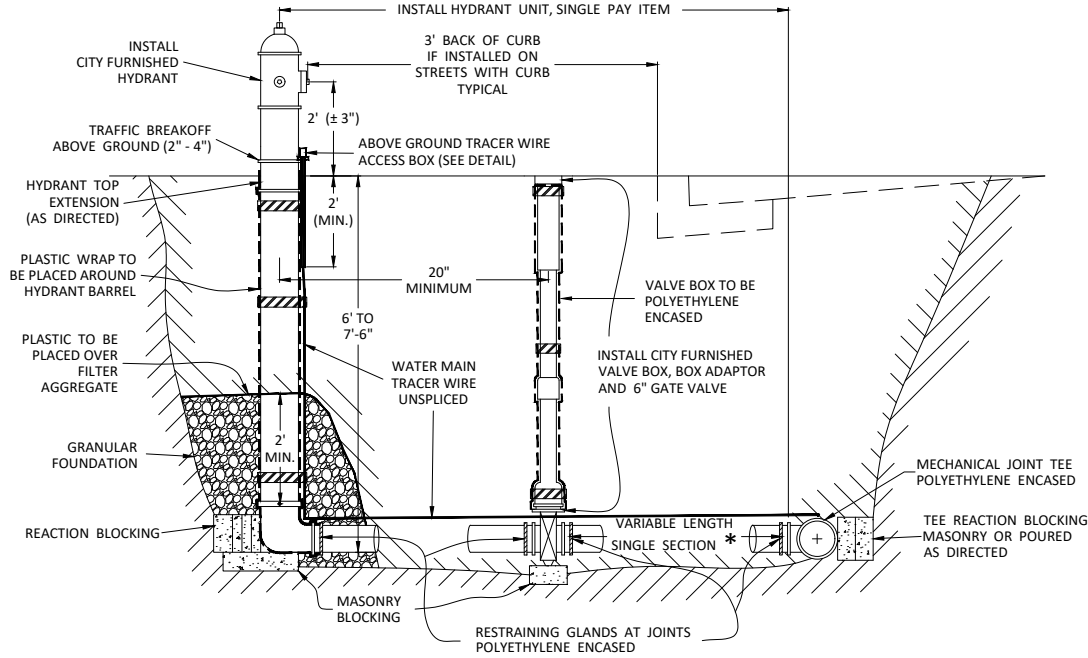
STANDARD 48" SANITARY MANHOLE
NOT TO SCALE



NON-RIGID SANITARY SEWER TRENCH
BEDDING DETAIL
NOT TO SCALE

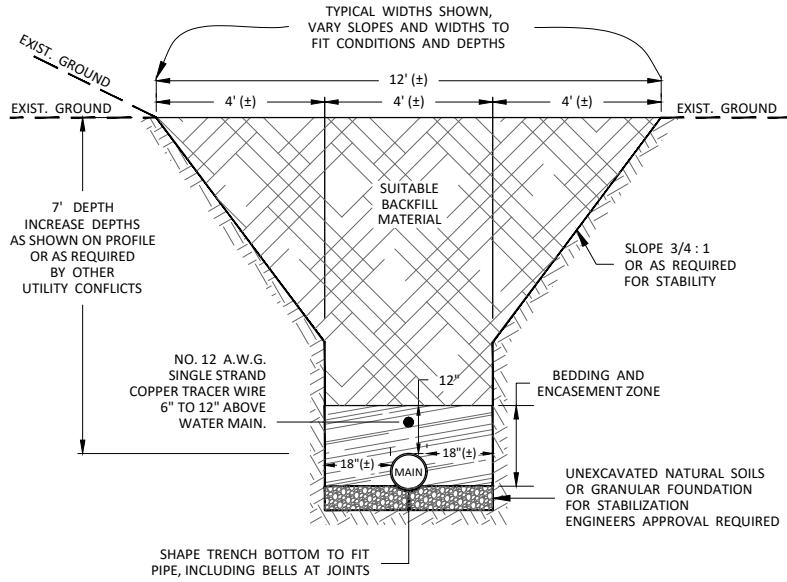


FLOATING MANHOLE CASTING
(NEENAH R-1955-1 OR EQUAL)

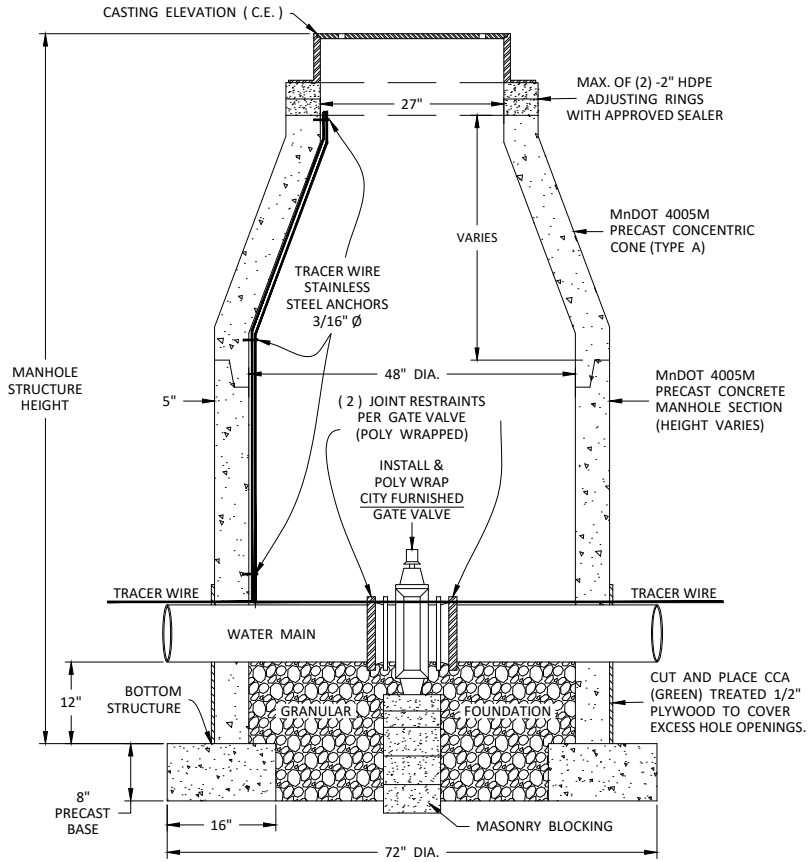


UNIT TO INCLUDE : HYDRANT, GATE VALVE, VALVE BOX, RESTRAINING GLANDS AND PIPE
* LENGTHS GREATER THAN 19' SHALL INCLUDE PIPE JOINT RESTRAINTS (INCIDENTAL)

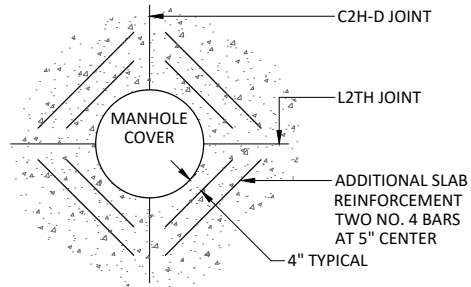
INSTALL HYDRANT UNIT
CITY TO SUPPLY HYDRANT, GATE VALVE, VALVE BOX AND ADAPTOR ONLY
CONTRACTOR TO SUPPLY ALL JOINT ACCESSORIES - SEE SPECIFICATIONS



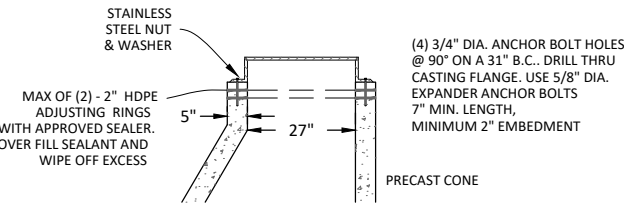
WATER MAIN BEDDING WITH TRACER WIRE DETAIL AND
TYPICAL TRENCH WIDTHS
NOT TO SCALE



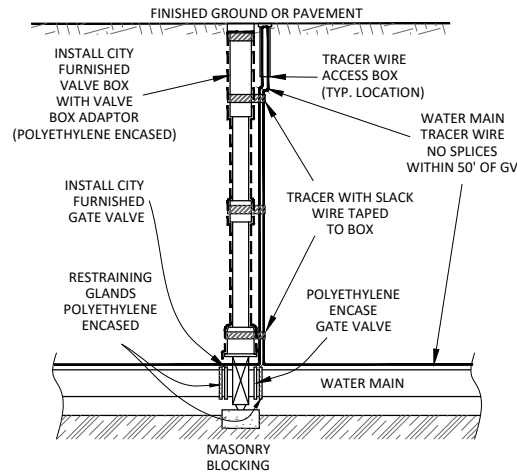
INSTALL GATE VALVE AND
CONSTRUCT MANHOLE FOR GATE VALVE
CITY TO SUPPLY GATE VALVE ONLY
CONTRACTOR TO SUPPLY ALL JOINT ACCESSORIES - SEE SPECIFICATIONS



CONCRETE REINFORCEMENT
AT MANHOLES
NOT TO SCALE

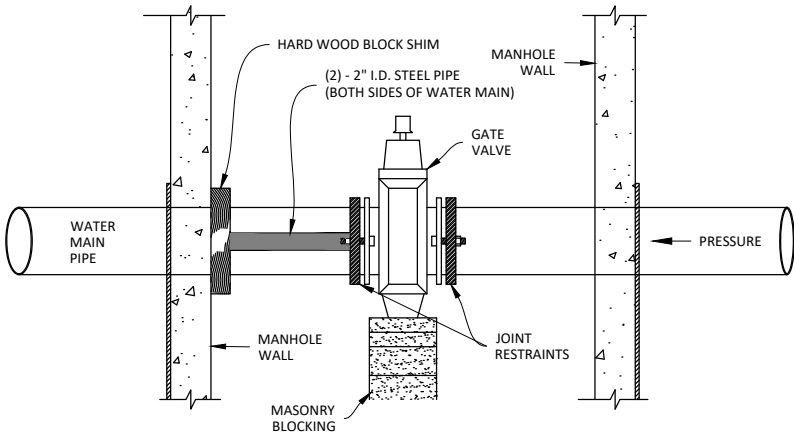


ANCHOR MANHOLE CASTING
WITH RINGS
NOT TO SCALE

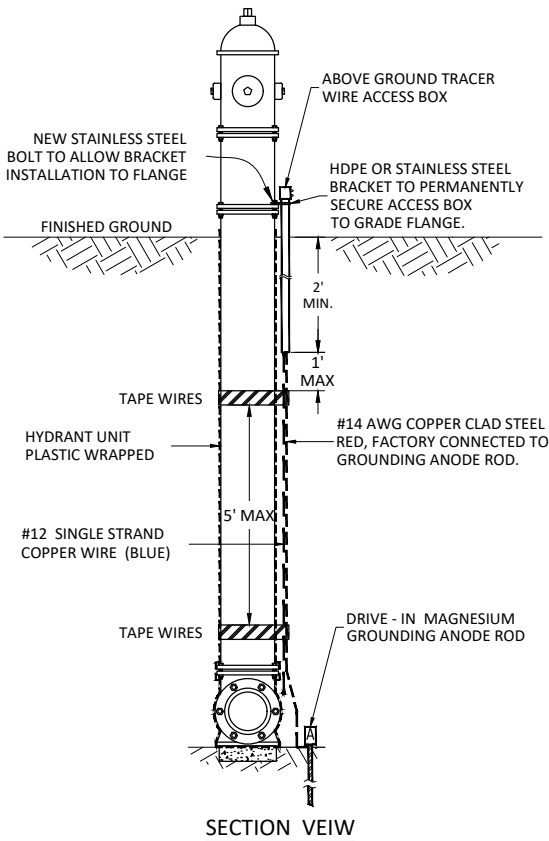
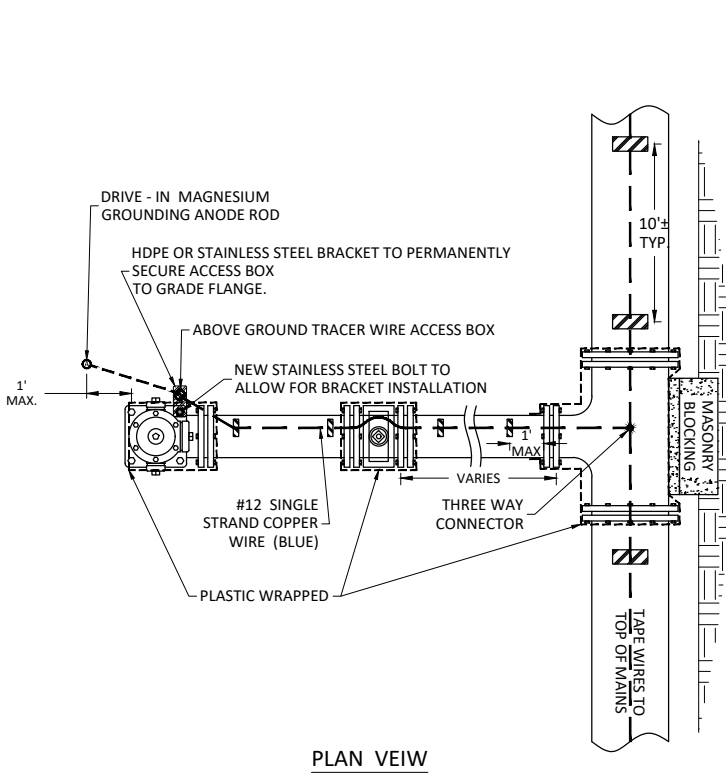
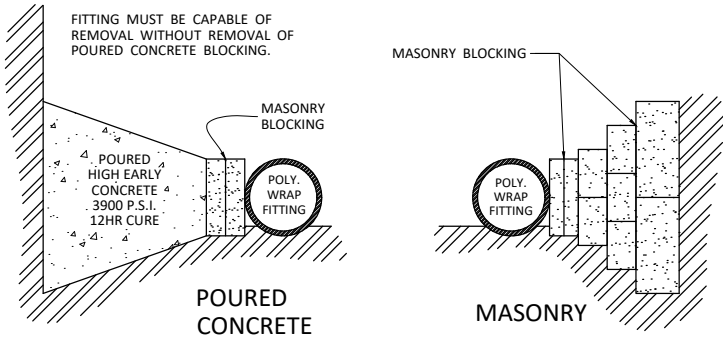
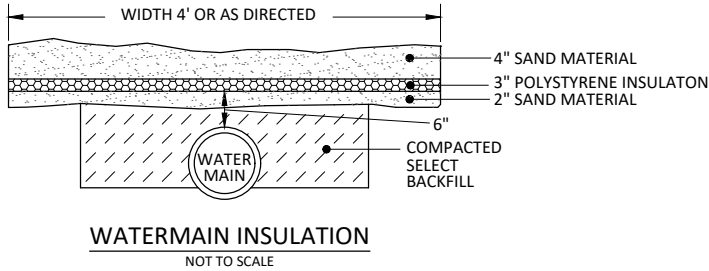


CITY TO FURNISH GATE VALVE, VALVE BOX & ADAPTOR ONLY

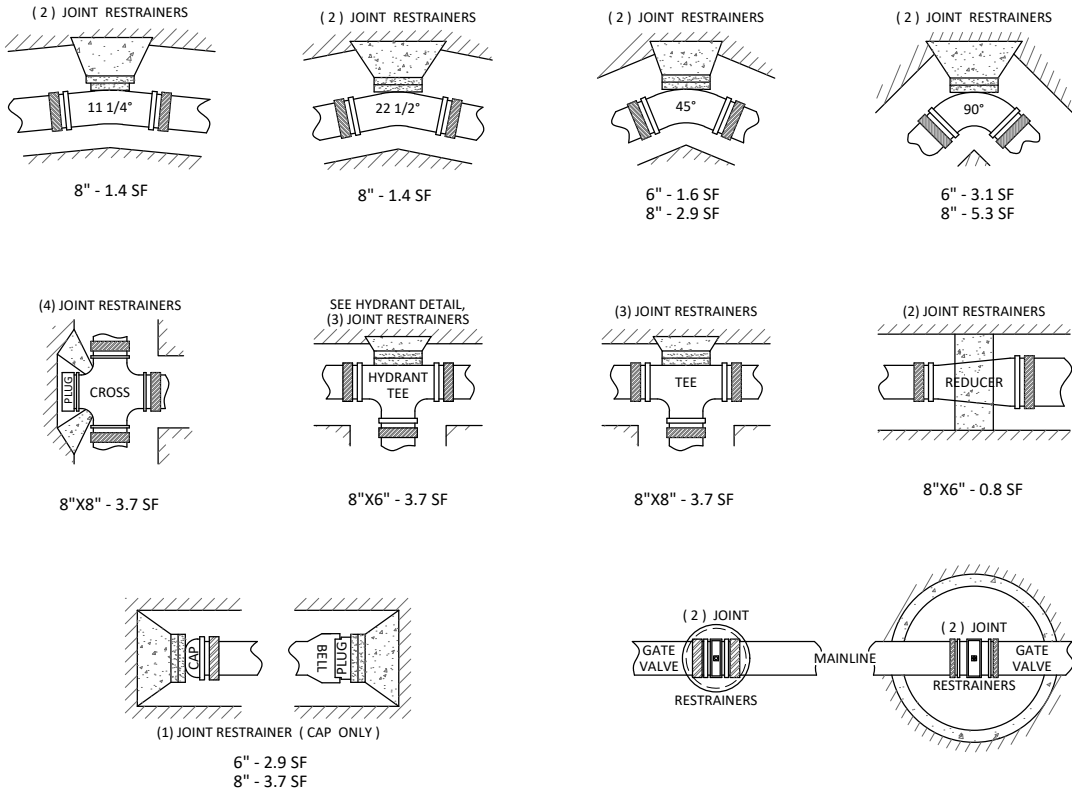
GATE VALVE AND BOX WITH TRACER WIRE
(NON HYDRANT LOCATIONS, DIRECT BURY IN LINE)
NOT TO SCALE

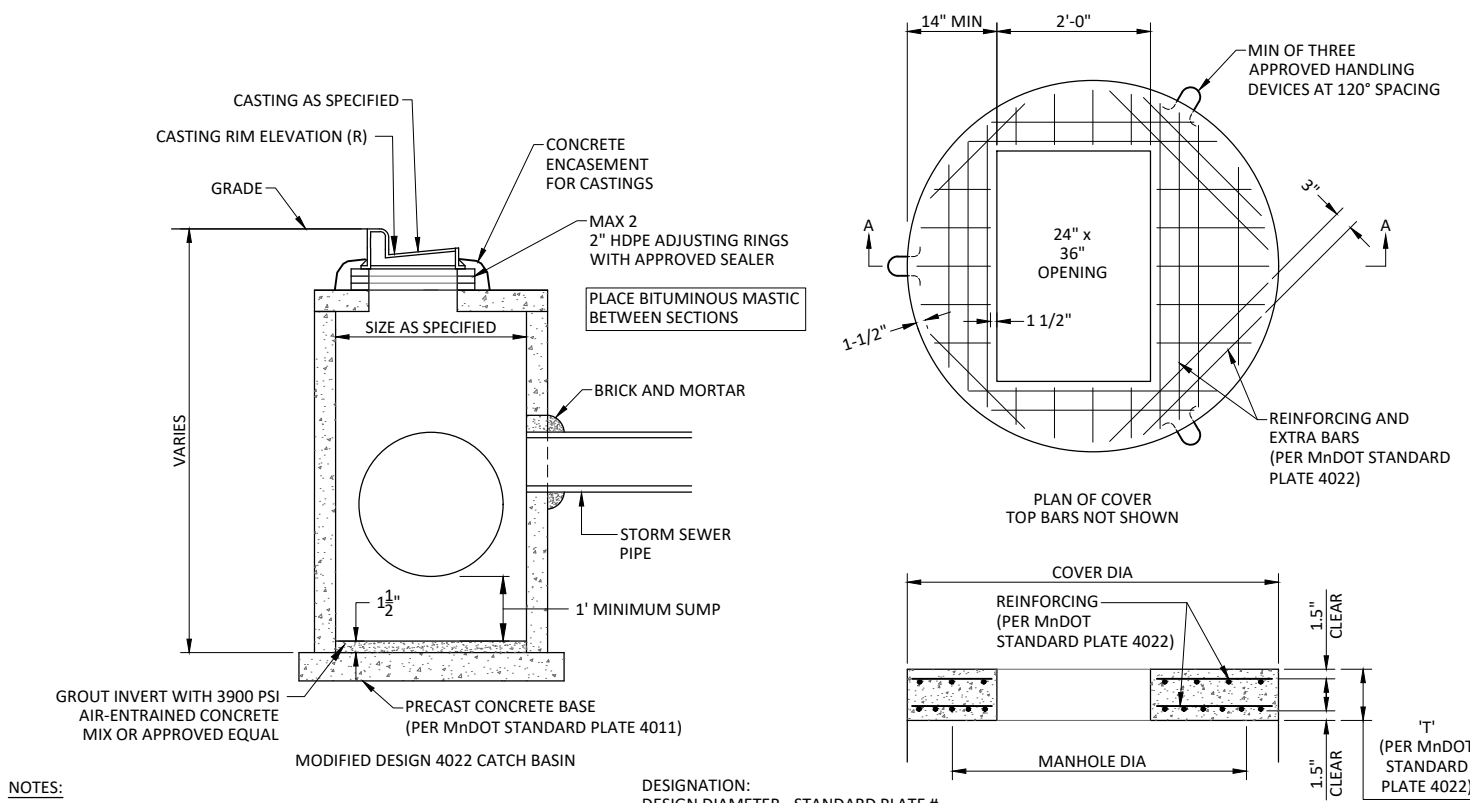


GATE VALVE TEMPORARY THRUST RESTRAINT



TRACER WIRE HYDRANT DETAIL
CITY TO SUPPLY GATE VALVE ONLY
CONTRACTOR TO SUPPLY ALL JOINT ACCESSORIES - SEE SPECIFICATIONS

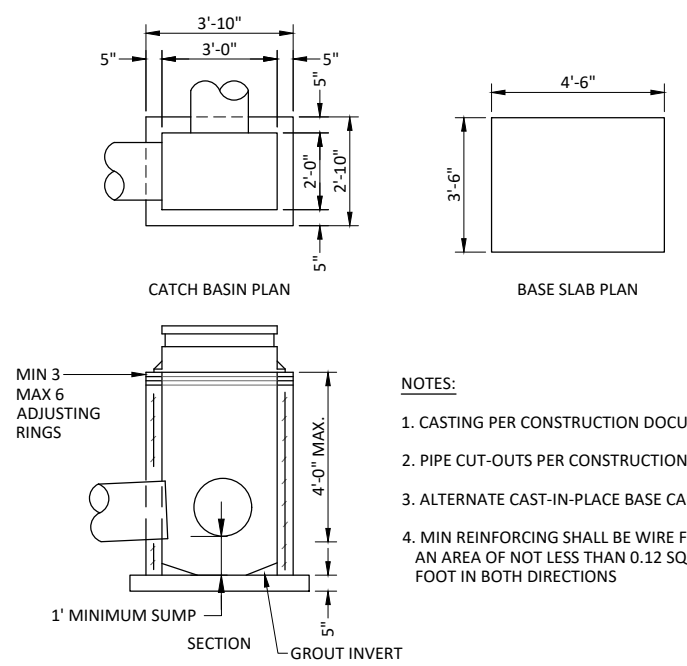




- NOTES:
1. AASHTO HS 25 LOADING MAX FILL HEIGHT 15'
 2. THE # 4022 SHALL BE PERMANENTLY MARKED ON THE TOP COVER
 3. EQUIVALENT STEEL AREAS IN WIRE MESH MAY BE USED
 4. REINFORCEMENT PER SPEC 3301, GRADE 60 A SINGLE HOOP OF 8ga STEEL WIRE

**STORM SEWER STRUCTURE
DESIGN 4022 W/ SUMP**

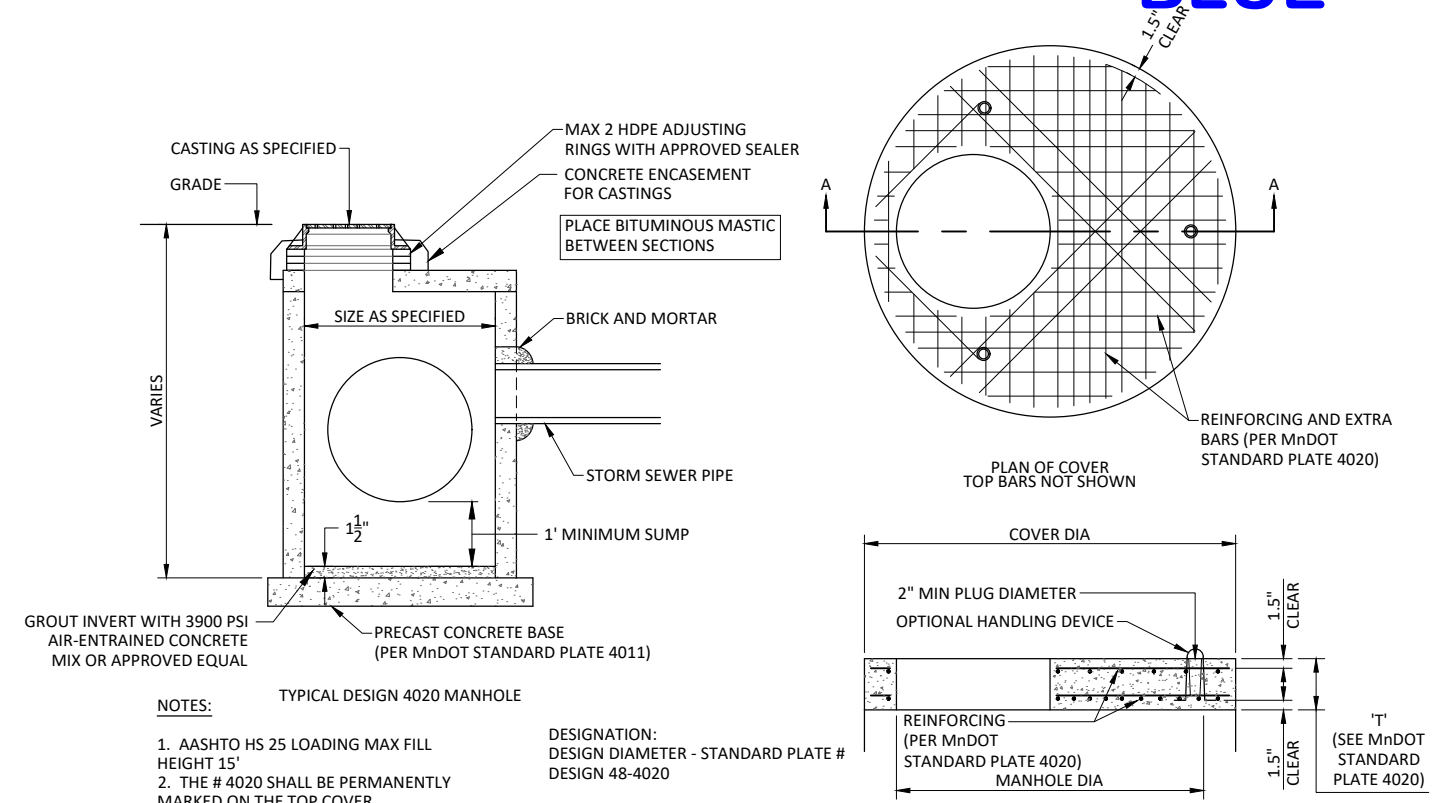
NOT TO SCALE



**CATCH BASIN STRUCTURE
DESIGN SPECIAL 1 - R-1 (2'x3')**

NOT TO SCALE

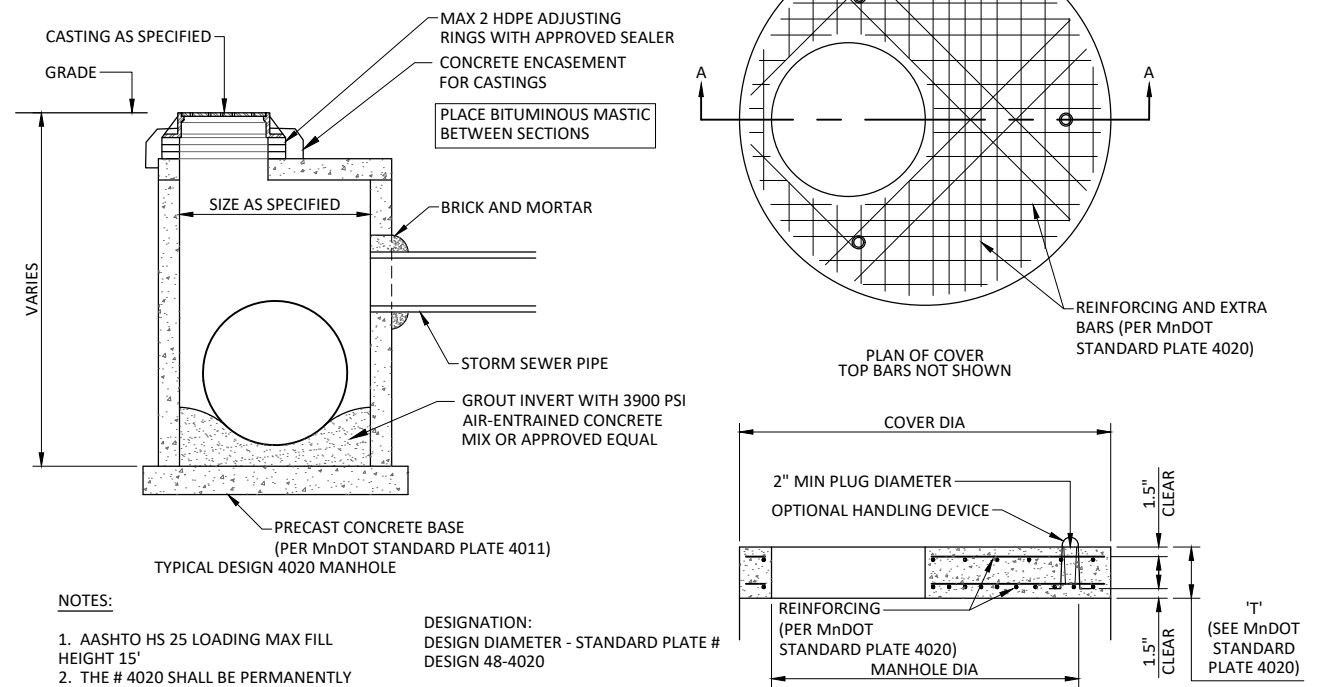
- NOTES:
1. CASTING PER CONSTRUCTION DOCUMENTS
 2. PIPE CUT-OUTS PER CONSTRUCTION DOCUMENTS
 3. ALTERNATE CAST-IN-PLACE BASE CAN BE USED
 4. MIN REINFORCING SHALL BE WIRE FABRIC HAVING AN AREA OF NOT LESS THAN 0.12 SQ IN PER FOOT IN BOTH DIRECTIONS



- NOTES:
1. AASHTO HS 25 LOADING MAX FILL HEIGHT 15'
 2. THE # 4020 SHALL BE PERMANENTLY MARKED ON THE TOP COVER
 3. EQUIVALENT STEEL AREAS IN WIRE MESH MAY BE USED
 4. REINFORCEMENT PER SPEC 3301, GRADE 60 A SINGLE HOOP OF 8ga STEEL WIRE

**STORM SEWER STRUCTURE
DESIGN 4020 W/ SUMP**

NOT TO SCALE



- NOTES:
1. AASHTO HS 25 LOADING MAX FILL HEIGHT 15'
 2. THE # 4020 SHALL BE PERMANENTLY MARKED ON THE TOP COVER
 3. EQUIVALENT STEEL AREAS IN WIRE MESH MAY BE USED
 4. REINFORCEMENT PER SPEC 3301, GRADE 60 A SINGLE HOOP OF 8ga STEEL WIRE

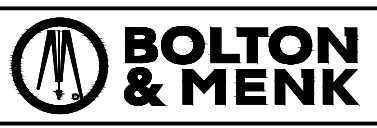
**STORM SEWER STRUCTURE
DESIGN 4020**

NOT TO SCALE

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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



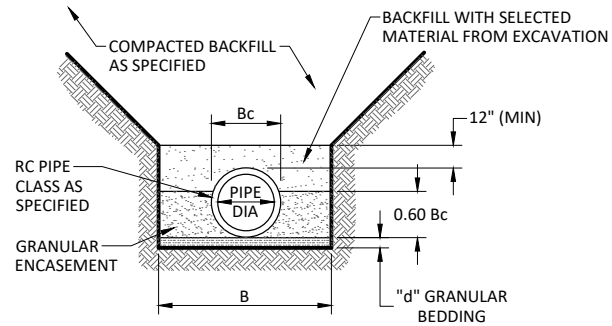
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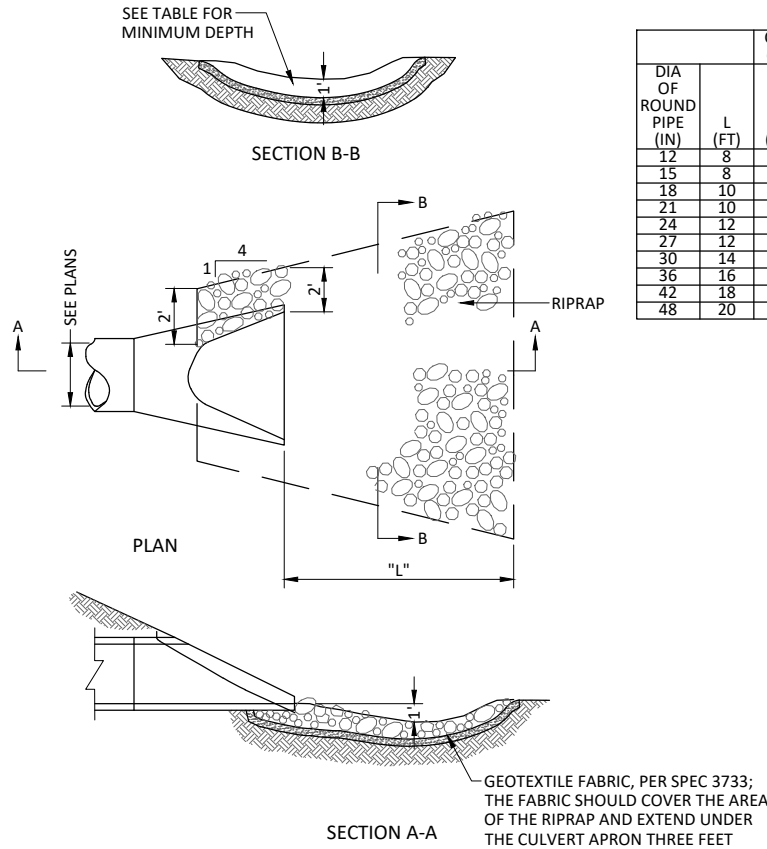
CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STORM SEWER DETAILS

SHEET
C1.05



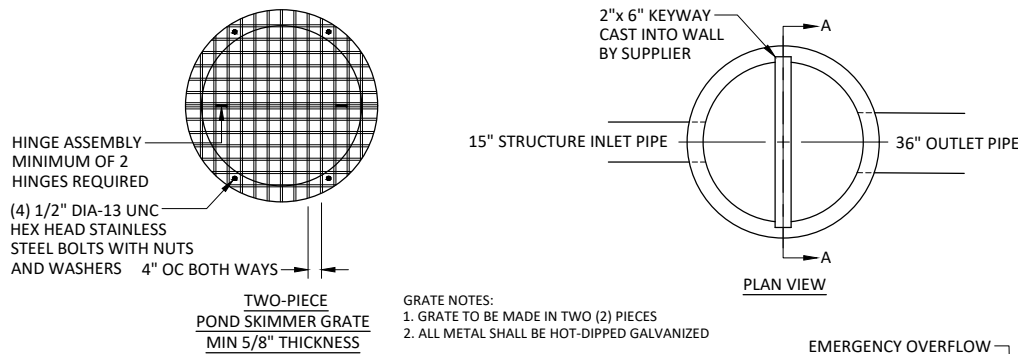
PIPE DIA	d	B
27" OR LESS	3"	Bc + 24"
30" TO 60"	4"	1.5 x Bc
66" OR OVER	6"	Bc + 36"

RC PIPE
CLASS "B" BEDDING-SQUARE TRENCH
NOT TO SCALE

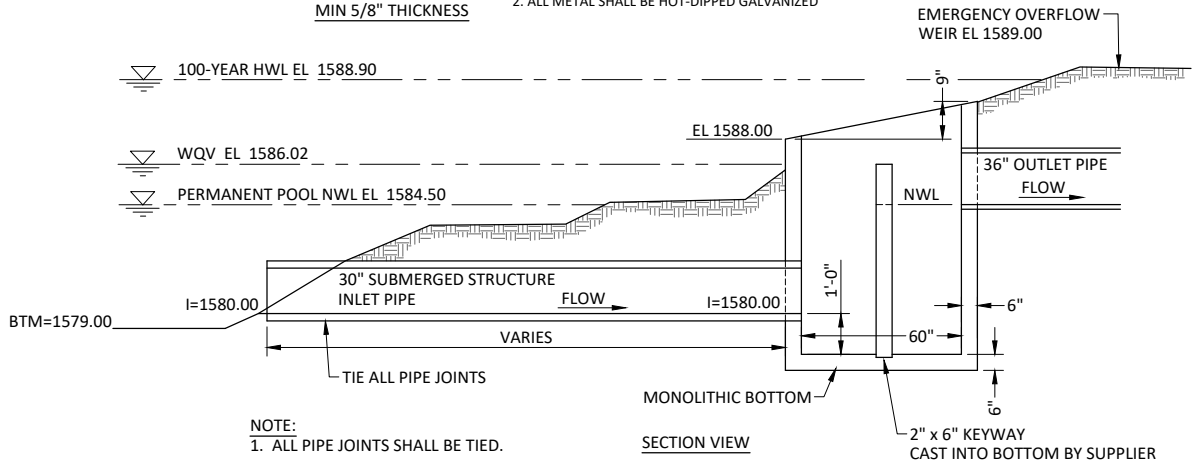


DIA OF ROUND PIPE (IN)	L (FT)	CLASS II d50=6"	CLASS III d50=9"	CLASS IV d50=12"
		12" DEPTH RIPRAP (CU YD)	18" DEPTH RIPRAP (CU YD)	24" DEPTH RIPRAP (CU YD)
12	8	5	8	10
15	8	5	8	10
18	10	6	10	15
21	10	8	15	15
24	12	10	15	20
27	12	10	15	20
30	14	15	20	25
36	16	18	25	30
42	18	20	30	40
48	20	20	40	50

RIPRAP AT RCP CULVERT END
NOT TO SCALE

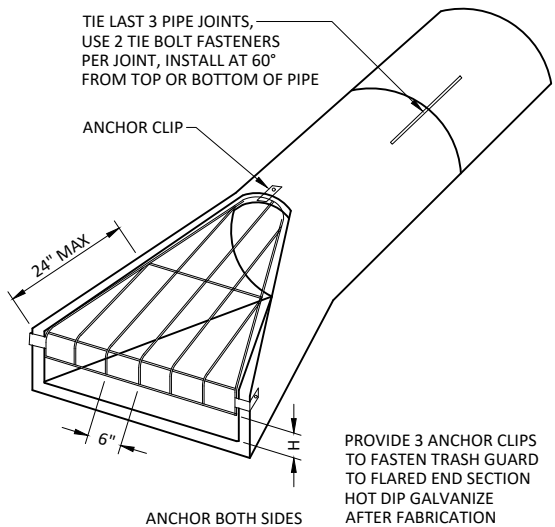
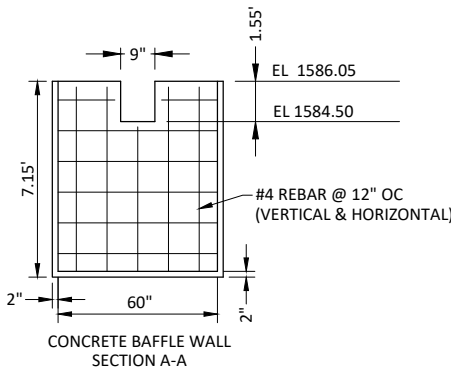


GRATE NOTES:
1. GRATE TO BE MADE IN TWO (2) PIECES
2. ALL METAL SHALL BE HOT-DIPPED GALVANIZED



NOTE:
1. ALL PIPE JOINTS SHALL BE TIED.

DRAINAGE STRUCTURE DESIGN SPECIAL
PRECAST CONCRETE POND SKIMMER STRUCTURE
NOT TO SCALE



TRASH GUARD SIZES			
PIPE SIZE	BAR	"H"	BOLTS
12"-18"	3/4"Ø	4"	5/8"
21"-42"	1"Ø	6"	3/4"
42"-72"	1 1/4"Ø	12"	1"

RC APRON TRASH GUARD
(STEEL BARS)
NOT TO SCALE

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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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FAIRMONT, MN 56031
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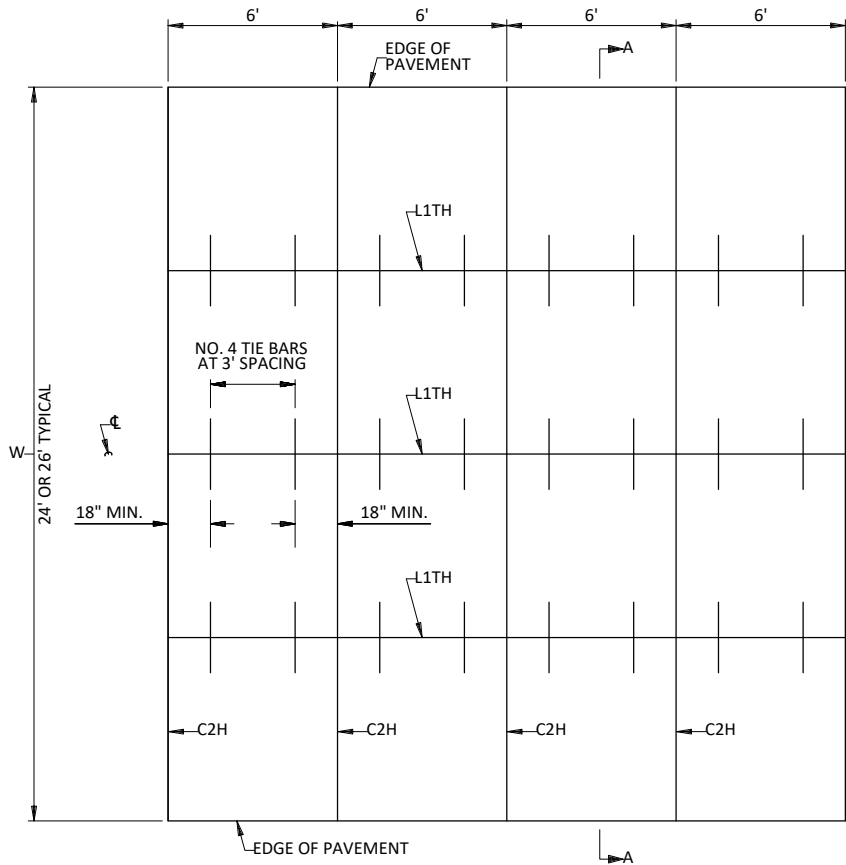


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CHECKED	TLW		
CLIENT PROJ. NO.			

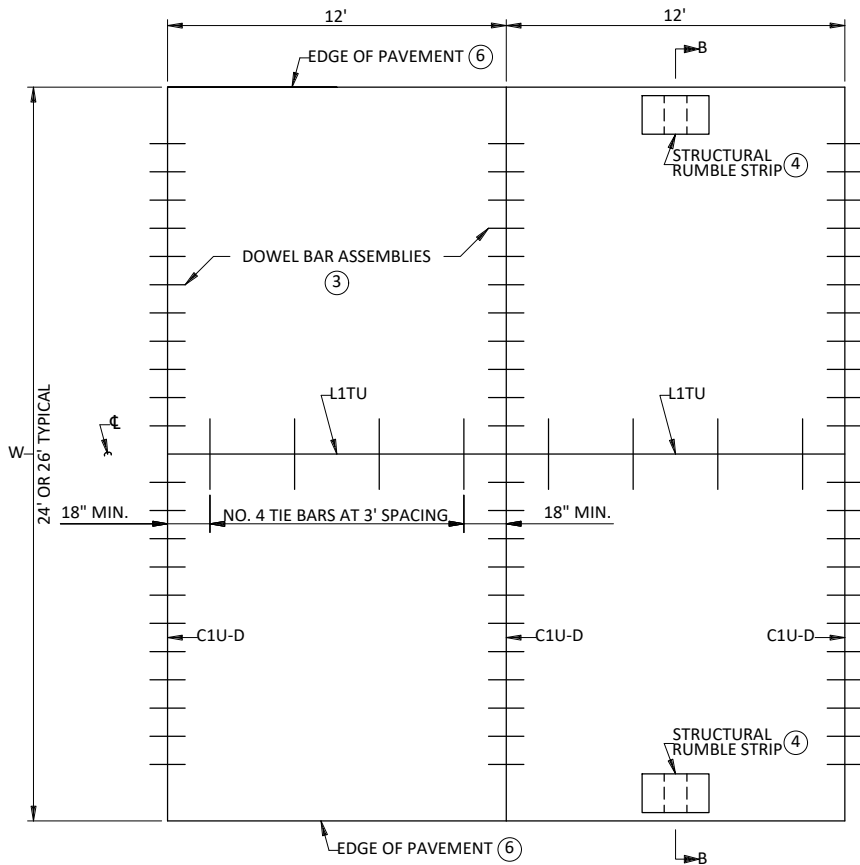
CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STORM SEWER DETAILS

SHEET

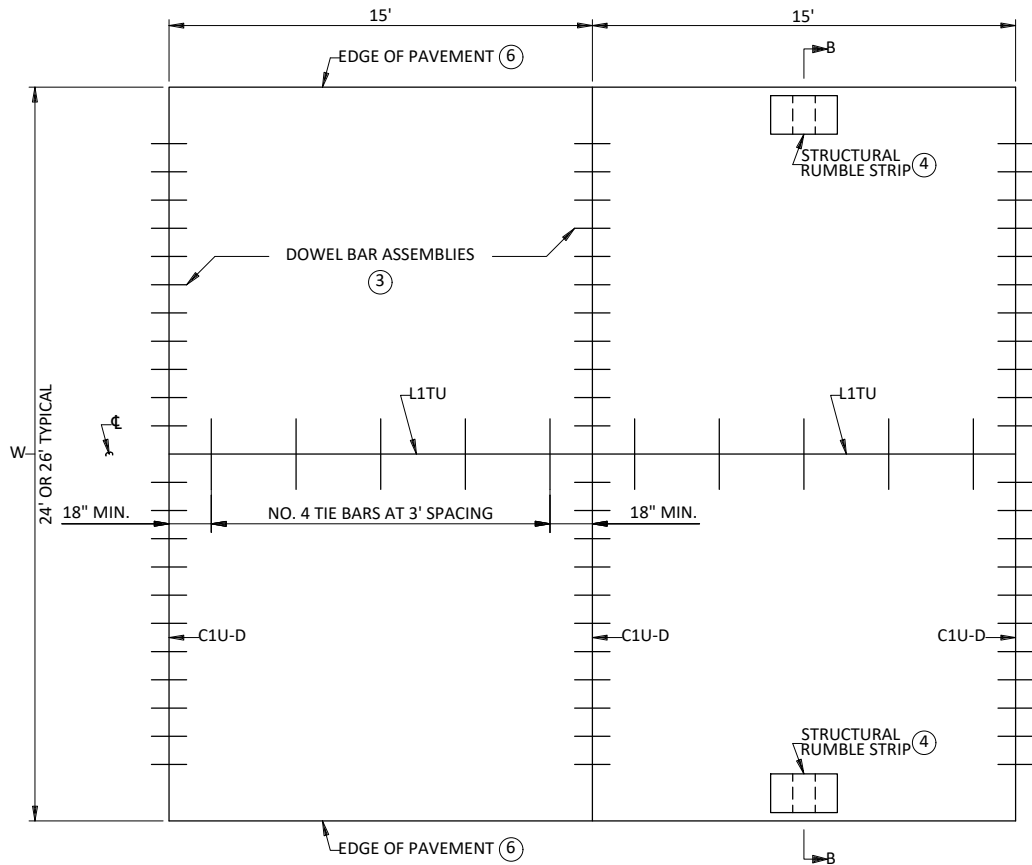
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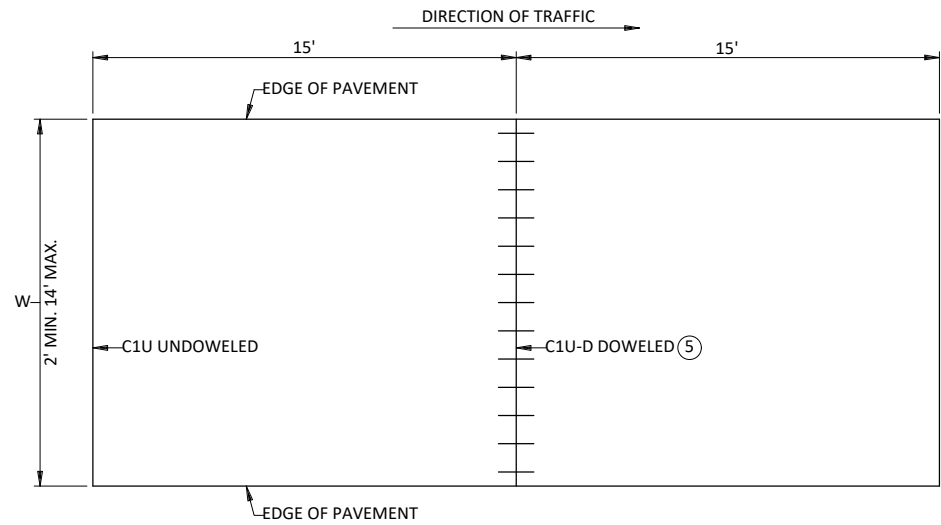
MAINLINE PAVEMENT
UNDOWELED
(PAVEMENT THICKNESS LESS THAN 7")



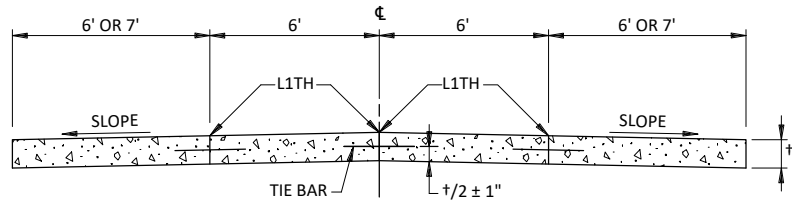
MAINLINE PAVEMENT
DOWELED
(PAVEMENT THICKNESS FROM 7" TO 7½")



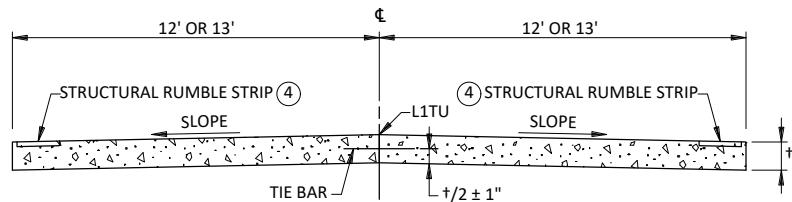
MAINLINE PAVEMENT
DOWELED
(PAVEMENT THICKNESS 8" AND GREATER)



PAVEMENT 2' THRU 14' WIDTH
UNDOWELED OR DOWELED
①②



SECTION A-A



SECTION B-B

NOTES:

- SEE TYPICAL SECTIONS AND PLAN SHEETS FOR CROSS SLOPES, LANE WIDTHS AND PAVEMENT THICKNESS.
- PROVIDE EPOXY COATED TIE BARS COMPLYING WITH SPEC. 3301.
- FOR SUPPLEMENTAL PAVEMENT REINFORCEMENT, SEE STANDARD PLATE 1070.
- FOR DOWEL BAR ASSEMBLIES, SEE STANDARD PLATE 1103.
- FOR SAFETY EDGE DETAILS, SEE STANDARD PLAN 5-297.220.
- FOR PAVEMENT JOINT DETAILS, SEE STANDARD PLAN 5-297.221.
- ① ADD A LONGITUDINAL JOINT DOWN THE MIDDLE OF THE LANE WHEN THE PAVING WIDTH IS GREATER THAN 14'.
- ② IN PAVEMENT TAPER AREAS 6' WIDE OR LESS, PLACE A NO. 4 REINFORCEMENT BAR 4" FROM AND ALONG EACH SIDE OF THE TAPERING PANEL.
- ③ PROVIDE 11 DOWEL BARS PER BASKET FOR NEW/RECONSTRUCTED PAVEMENTS. PROVIDE 8 WHEEL PATH DOWEL BARS PER BASKET FOR CONCRETE OVERLAYS.
- ④ SEE PLANS FOR RUMBLE STRIP DETAILS. CONSTRUCT STRUCTURAL RUMBLE STRIPS (INCIDENTAL) IN THE PLASTIC CONCRETE WHERE THE PAVEMENT WIDTH (W) IS GREATER THAN 24' AND THERE IS NO OUTSIDE CONCRETE SHOULDER ADDED.
- ⑤ PROVIDE DOWELS WHEN PAVEMENT WIDTH IS GREATER THAN OR EQUAL TO 4'.
- ⑥ PROVIDE AN L1TU OR L2TU JOINT WHEN ADDING OUTSIDE CONCRETE SHOULDER.

LEAD
EXPERT
OFFICE

CURT TURGEON
DIRECTOR
OFFICE OF MATERIALS
& ROAD RESEARCH

CONCRETE MAINLINE PAVEMENT
TWO-LANE ROADWAY

APPROVED:
01-30-2025

THOMAS STYRBICKI
STATE DESIGN ENGINEER

STANDARD
PLAN
5-297.217

1 OF 2

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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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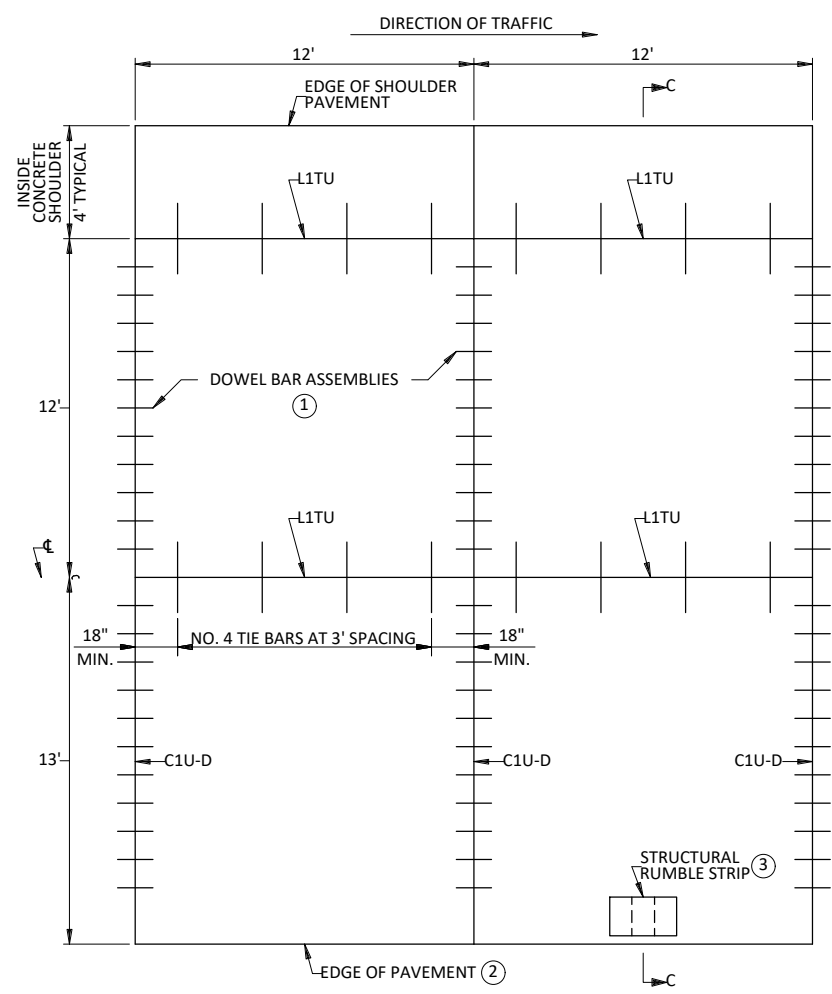


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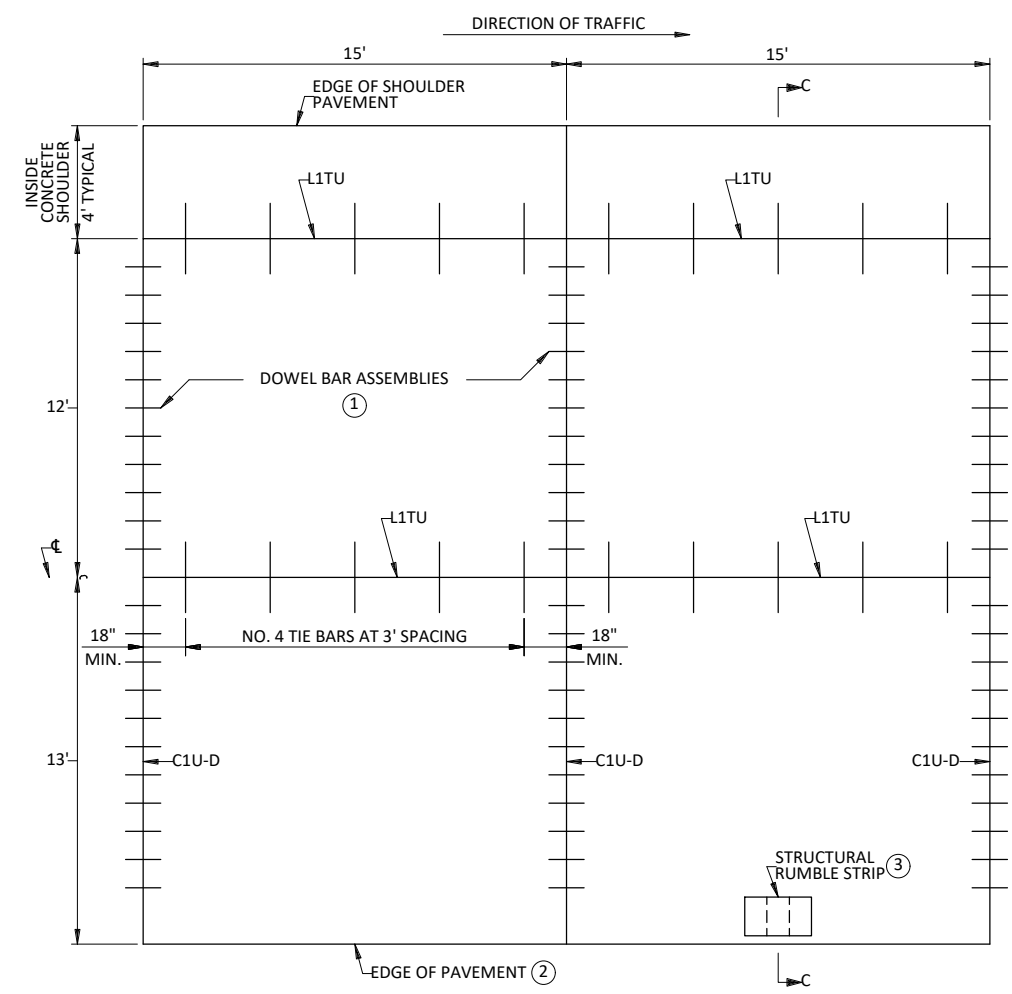
CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STANDARD PLANS
CONCRETE MAINLINE PAVEMENT

SHEET

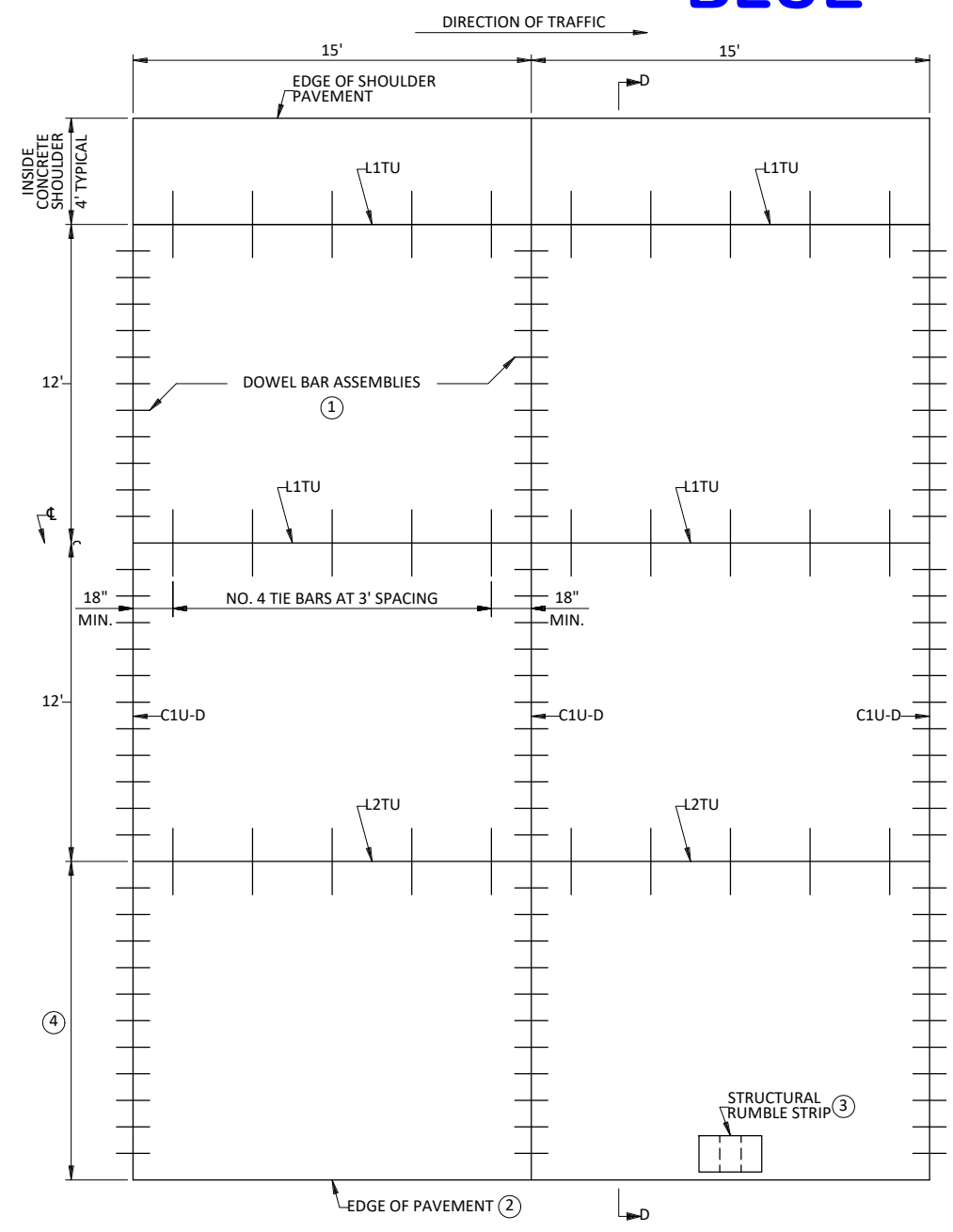
C1.07



MAINLINE PAVEMENT WITH INSIDE CONCRETE SHOULDERS
(4 LANE DIVIDED)
DOWELED
(PAVEMENT THICKNESS FROM 7" TO 7½")



MAINLINE PAVEMENT WITH INSIDE CONCRETE SHOULDERS
(4 LANE DIVIDED)
DOWELED
(PAVEMENT THICKNESS 8" AND GREATER)

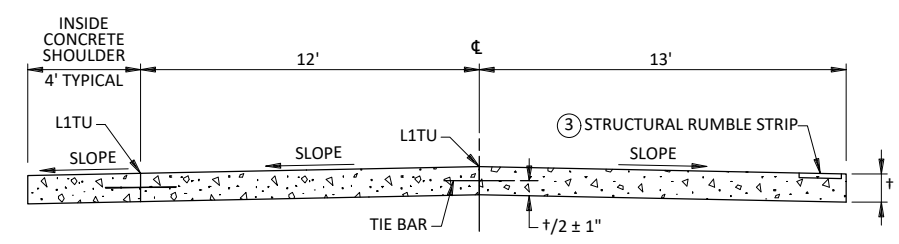


MAINLINE PAVEMENT
(MULTIPLE LANE DIVIDED)
DOWELED

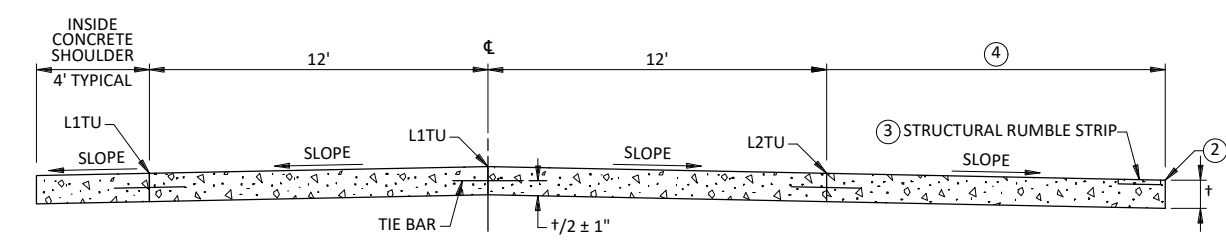
NOTES:

- SEE TYPICAL SECTIONS AND PLAN SHEETS FOR CROSS SLOPES, LANE WIDTHS AND PAVEMENT THICKNESS.
- PROVIDE EPOXY COATED TIE BARS COMPLYING WITH SPEC. 3301.
- FOR SUPPLEMENTAL PAVEMENT REINFORCEMENT, SEE STANDARD PLATE1070.
- FOR DOWEL BAR ASSEMBLIES, SEE STANDARD PLATE 1103.
- FOR SAFETY EDGE DETAILS, SEE STANDARD PLAN 5-297.220.
- FOR PAVEMENT JOINT DETAILS, SEE STANDARD PLAN 5-297.221.

- ① PROVIDE 11 DOWEL BARS PER BASKET ASSEMBLY FOR NEW/RECONSTRUCTED PAVEMENTS. PROVIDE 8 WHEEL PATH DOWEL BARS PER BASKET ASSEMBLY FOR CONCRETE OVERLAYS.
- ② PROVIDE AN L1TU OR L2TU JOINT WHEN ADDING AN OUTSIDE CONCRETE SHOULDER.
- ③ SEE PLANS FOR RUMBLE STRIP DETAILS. CONSTRUCT STRUCTURAL RUMBLE STRIPS (INCIDENTAL) IN THE PLASTIC CONCRETE WHEN THERE IS NO OUTSIDE CONCRETE SHOULDER OR CURB AND GUTTER ADDED.
- ④ 13'-WIDE LANE WHEN HMA SHOULDERS ARE USED, OTHERWISE 12'-WIDE LANE.



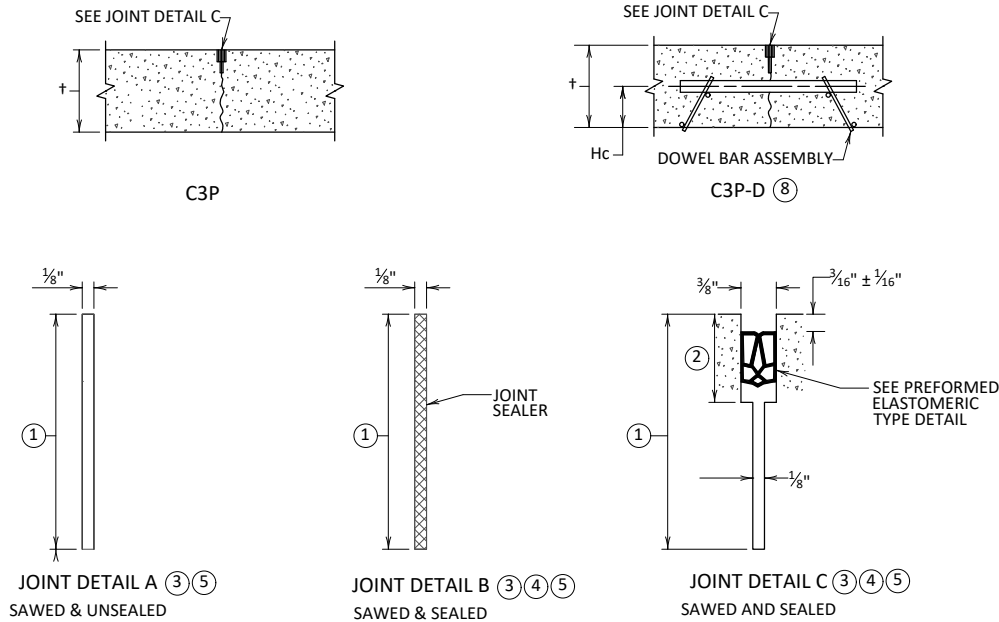
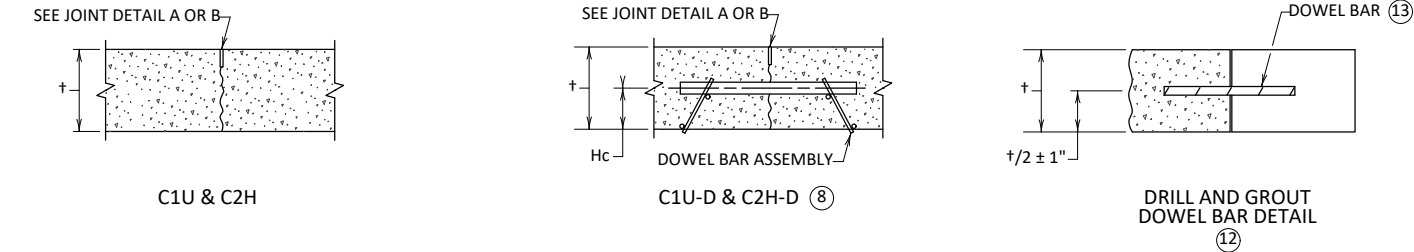
SECTION C-C



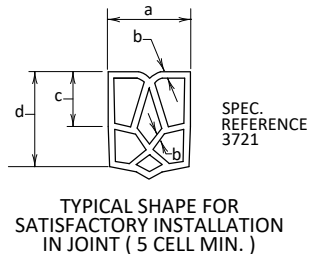
SECTION D-D

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LEAD EXPERT OFFICE		CURT TURGEON DIRECTOR OFFICE OF MATERIALS & ROAD RESEARCH	CONCRETE MAINLINE PAVEMENT DIVIDED ROADWAY		APPROVED: 01-30-2025	THOMAS STYRBICKI STATE DESIGN ENGINEER	STANDARD PLAN 5-297.217	2 OF 2
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.		1501 SOUTH STATE ST, SUITE 100 FAIRMONT, MN 56031 Phone: (507) 238-4738 Email: Fairmont@bolton-menk.com www.bolton-menk.com		CITY OF WORTHINGTON, MINNESOTA EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS STANDARD PLANS CONCRETE MAINLINE PAVEMENT		SHEET C1.08		
TRAVIS L. WINTER LIC. NO. 46649 DATE MAY 27, 2025		BOLTON & MENK		DESIGNED JTO DRAWN JVZ CHECKED TLW CLIENT PROJ. NO.		NO. 95% REV 05/22/2025		



REQUIRED DIMENSIONS ②	
JOINT TYPE	TRANSVERSE
NOMINAL SEALER SIZE	USE IN ALL $\frac{11}{16}$ " JOINTS
a	$0.69" + 0.13" - 0.05"$
b	$0.08" \pm 0.02"$
c	0.25" MIN.
d	0.63" MIN.



PREFORMED ELASTOMERIC TYPE DETAIL ②

CONTRACTION JOINT REFERENCE, DETAIL & SEALER SPEC. TABLE				
JOINT REFERENCE		JOINT DETAIL	JOINT SEALER SPEC.	JOINT WIDTH
WITHOUT DOWELS	WITH DOWELS			
C1U	C1U-D	A	UNSEALED	$\frac{3}{8}$ "
C2H	C2H-D	B	3725	$\frac{1}{8}$ "
C3P	C3P-D	C	3721	$\frac{3}{8}$ "
<div><div>LEGEND</div><div>EXAMPLE</div></div>				
C = CONTRACTION JOINT NO. = JOINT REFERENCE U = UNSEALED H = HOT POURED P = PREFORMED -D = DOWEL BARS				

NOTES:

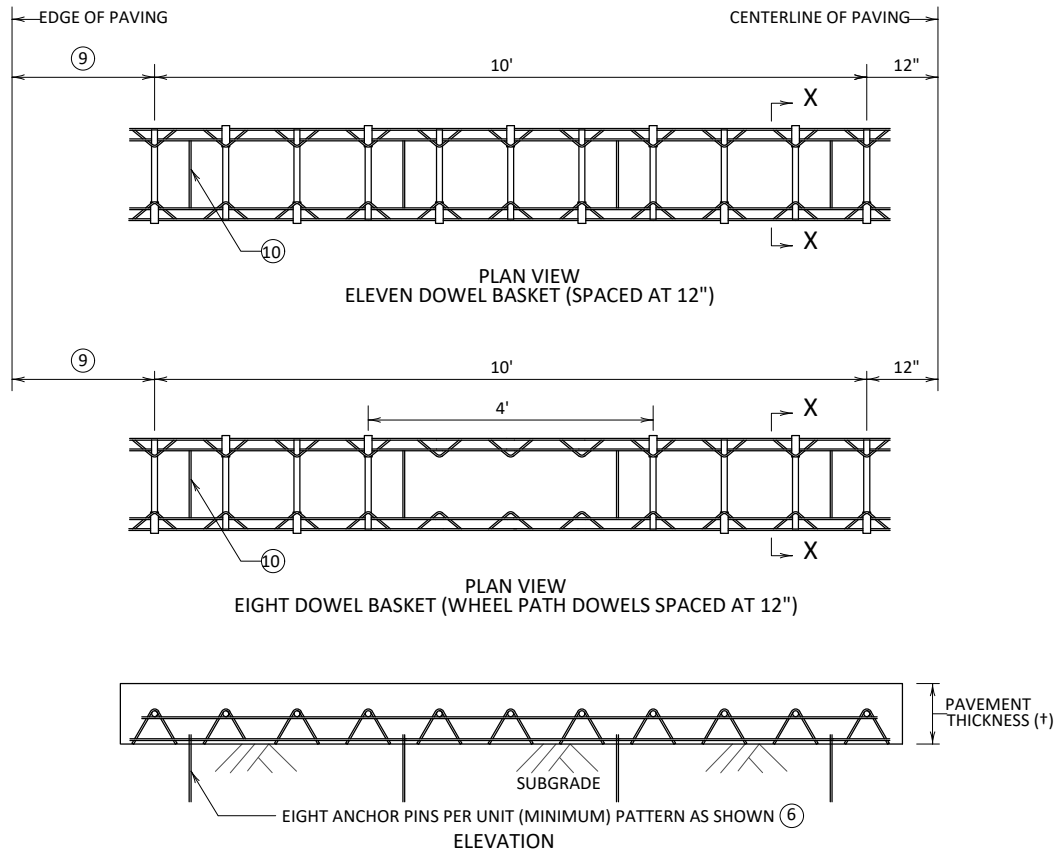
SEE STANDARD PLATE 1103 FOR DOWEL BAR ASSEMBLY.

FURNISH AND INSTALL ALL JOINT SEALER IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.

SEE STANDARD PLANS 5-297.217 AND 5-297.219 FOR CONCRETE MAINLINE/RAMP PAVEMENT.

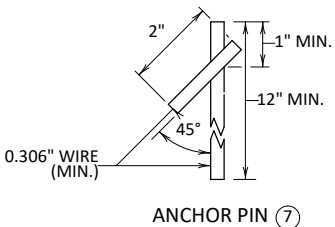
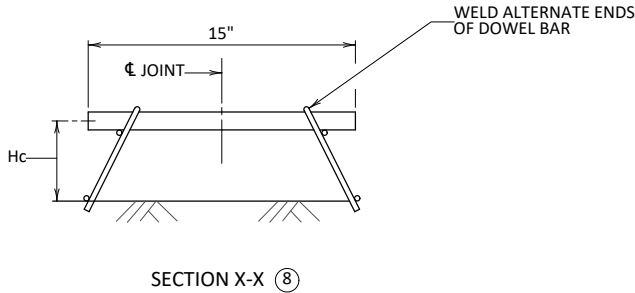
SEE PAVING LAYOUTS IN THE PLANS FOR JOINT CLASS DESIGNATION TO USE AND SPECIAL REINFORCEMENT REQUIREMENTS.

- ① JOINT DEPTH AND TOLERANCE: $\pm \frac{1}{3} \pm \frac{1}{4}$ ".
- ② JOINT TO FIT THE JOINT DESIGN WIDTH. "a" DIMENSION APPLIES AT ANY POINT THROUGHOUT "c" DEPTH. SHARP CORNERS NOT PERMITTED. PROVIDE CORNERS WITH SUITABLE FILLET.
- ③ WHEN WET-CUT SAWING, CLEAN JOINT FACES WITH WATER DURING THE SAW CUTTING OPERATION OR BY WATER BLASTING AFTER SAWING. WHEN EARLY-ENTRY SAWING, USE AIR BLASTING TO REMOVE SAWING RESIDUE.



CONTRACTION JOINT DOWEL BAR ASSEMBLIES FOR 12' LANES OR GREATER ⑪

DOWEL BAR TABLE		
† PAVEMENT THICKNESS (INCHES)	DOWEL BAR DIAMETER (INCHES)	Hc HEIGHT TO CENTER OF DOWEL BAR (INCHES)
7 - $7\frac{1}{2}$	1	3
8 - 10	$1\frac{1}{4}$	4
$> 10\frac{1}{2}$	$1\frac{1}{2}$	5



- ④ CLEAN AND DRY JOINT FACES BY SANDBLASTING AND AIR BLASTING. TOP OF JOINT SEALER 3725 FLUSH TO $\frac{1}{8}$ " BELOW TOP OF PAVEMENT SURFACE.
- ⑤ JOINT WIDTH TOLERANCE IS $+\frac{1}{16}$ " TO $-\frac{1}{32}$ ".
- ⑥ EVENLY SPACE A MINIMUM OF EIGHT ANCHOR PINS (FOUR PER SIDE) PER DOWEL ASSEMBLY. PROVIDE QUALITY CONTROL PLAN FOR ANCHORING THE DOWEL BAR ASSEMBLIES TO THE ENGINEER FOR ACCEPTANCE IN ACCORDANCE WITH SPEC. 2301.
- ⑦ ANCHOR PIN REQUIREMENTS FOR CONCRETE PAVEMENT ON GRADE CONSTRUCTION. FOR CONCRETE OVERLAYS, ANCHOR PIN REQUIREMENT AS APPROVED BY THE ENGINEER.
- ⑧ TOLERANCES:
- PLACE DOWEL BARS PARALLEL TO THE SUBSTRATE SURFACE, $\pm \frac{1}{8}$ " IN 15".
- PLACE DOWEL BARS PARALLEL TO THE CENTERLINE OF THE PAVEMENT, $\pm \frac{1}{4}$ " IN 15".
- SAW CONTRACTION JOINTS PERPENDICULAR TO THE CENTERLINE OF THE PAVEMENT AND CENTERED ON THE DOWEL BAR, ± 3 ".
- HEIGHT (Hc) TO CENTER OF DOWEL BAR: $\pm \frac{1}{2}$ ".
- ⑨ DISTANCE TO EDGE OF PAVEMENT FROM OUTSIDE DOWEL:
- 3' 0" FOR 14' 0" LANE.
- 2' 6" FOR 13' 6" LANE.
- 2' 0" FOR 13' 0" LANE.
- 1' 0" FOR 12' 0" LANE.
- ⑩ CONTRACTOR OPTION TO CUT AND BEND SPACER WIRES AFTER STAKING.
- ⑪ REMOVE ONE DOWEL BAR PER FOOT OF LANE WIDTH REDUCTION LESS THAN 12'.
- ⑫ DRILL THE HOLE $\frac{3}{8}$ " WIDER THAN THE NOMINAL OUTSIDE DIAMETER OF THE BAR TO A DEPTH OF 9". INJECT A MnDOT-APPROVED EPOXY OR NON-SHRINK GROUT IN THE BACK OF THE DRILL HOLE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
- ⑬ PROVIDE DOWEL BAR, 18" LONG, SPACED AT 12" ON CENTER.

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OFFICE

CURT TURGEON
DIRECTOR
OFFICE OF MATERIALS
& ROAD RESEARCH

PAVEMENT JOINTS
CONTRACTION (DESIGN C)

APPROVED:
01-30-2025

THOMAS STYRBICKI
STATE DESIGN ENGINEER

STANDARD
PLAN
5-297.221

1 OF 5

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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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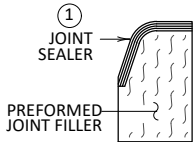
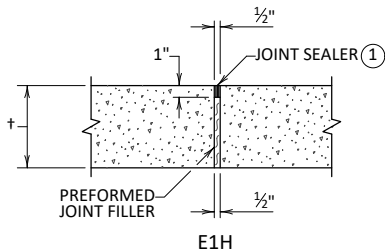
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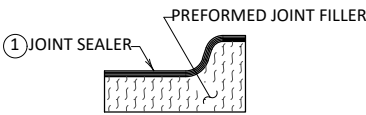
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SECTION THRU CURB



SECTION THRU CURB

EXPANSION JOINT REFERENCE, DETAIL & SEALER SPEC. TABLE			
JOINT REFERENCE	PREFORMED JOINT FILLER SPEC.	JOINT SEALER SPEC.	JOINT WIDTH
WITHOUT DOWELS			
E1H	3702	3725	1/2"
<div>LEGEND<div>E = EXPANSION JOINT</div><div>NO. = JOINT REFERENCE</div><div>H = HOT POURED</div><div>-D = DOWEL BARS</div></div> <div>EXAMPLE<div>E1H-D</div></div>			

- NOTES:
- PROVIDE PREFORMED JOINT FILLER MATERIAL IN ACCORDANCE WITH SPEC. 3702.
- FURNISH AND INSTALL ALL JOINT SEALER IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
- ① JOINT SEALER SPEC. 3725. CLEAN AND DRY JOINT FACES BY SANDBLASTING AND AIR BLASTING. TOP OF SEALER FLUSH TO 1/8" BELOW TOP OF PAVEMENT SURFACE.

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PAVEMENT JOINTS
EXPANSION (DESIGN E)

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01-30-2025


THOMAS STYRBICKI
STATE DESIGN ENGINEER

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PLAN
5-297.221

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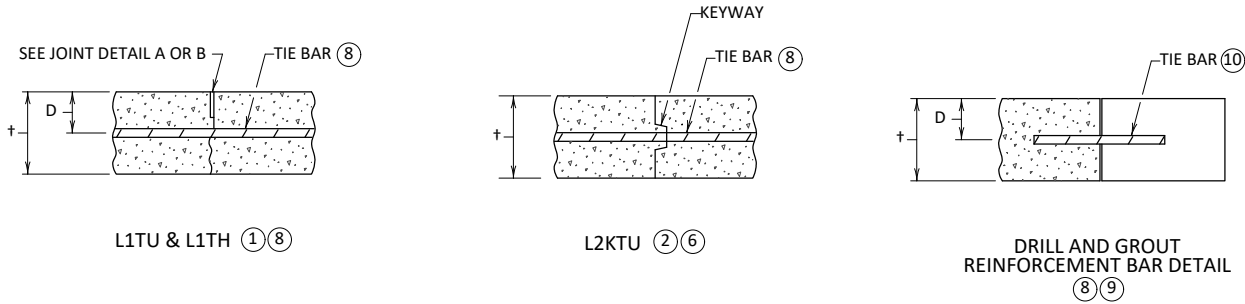
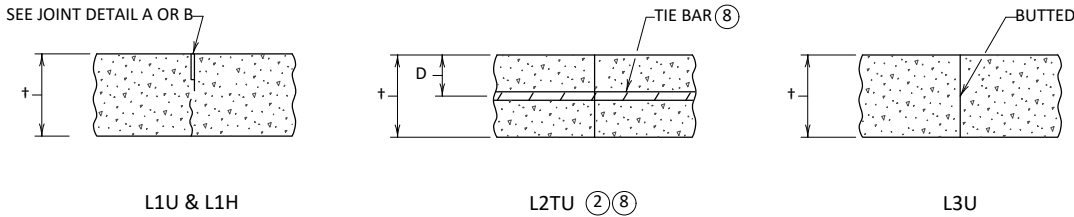


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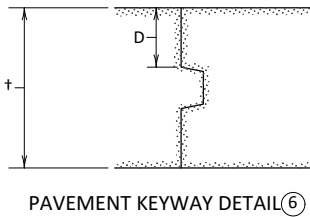
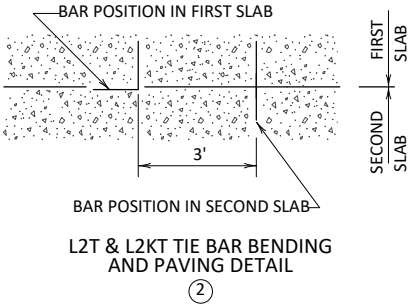
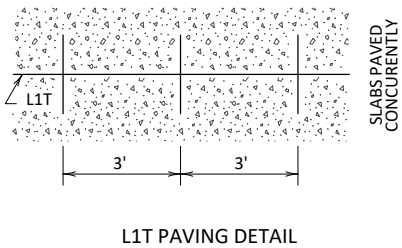
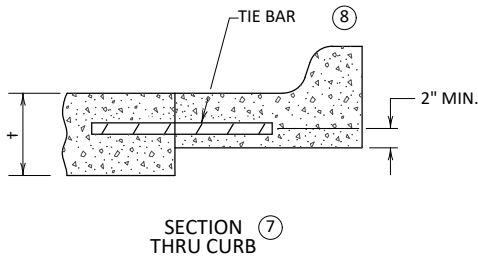
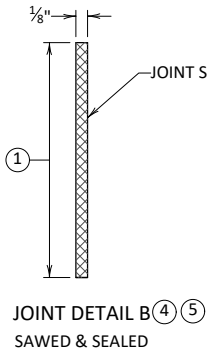
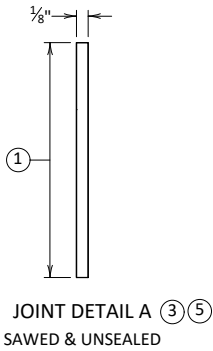
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C1.10



LONGITUDINAL JOINT REFERENCE, DETAIL & SEALER SPECIFICATION TABLE					
JOINT REFERENCE			JOINT DETAIL	JOINT SEALER SPEC	JOINT WIDTH
WITHOUT TIE BARS	WITH TIE BARS	WITH KEYWAY & TIE BARS			
L1U	L1TU		A	UNSEALED	1/8"
L1H	L1TH		B	3725	1/8"
	L2TU	L2KTU	NONE	UNSEALED	
L3U			NONE	UNSEALED	
<div><div>LEGEND</div><div>L = LONGITUDINAL JOINT</div><div>NO. = JOINT REFERENCE</div><div>1 = PAVED CONSTRUCTION JOINT</div><div>2 = TIED CONSTRUCTION JOINT</div><div>3 = BUTTED CONSTRUCTION JOINT</div><div>K = KEYWAY</div><div>T = TIE BARS</div><div>U = UNSEALED</div><div>H = HOT POURED</div></div> <div><div>EXAMPLE</div><div>L2KTU</div></div>					



FIXED FORM KEYWAY TABLE (6)	
t PAVEMENT THICKNESS	D (MINIMUM DEPTH)
< 7"	2 1/2"
7" TO 7 1/2"	3"
8" TO 9 1/2"	4"
≥ 10"	5"

SLIPFORM KEYWAY TABLE (6)	
t PAVEMENT THICKNESS	D (MINIMUM DEPTH)
< 10"	NO KEYWAY
≥ 10"	5"

NOTES:

PROVIDE EPOXY-COATED TIE BARS COMPLYING WITH SPEC. 3301.

FURNISH AND INSTALL ALL JOINT SEALER IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.

SEE STANDARD PLANS 5-297.217 AND 5-297.219 FOR CONCRETE MAINLINE AND RAMP PAVEMENT.

SEE PAVING LAYOUTS IN THE PLANS FOR JOINT CLASS DESIGNATION TO USE AND SPECIAL REINFORCEMENT REQUIRED.

FOR LONGITUDINAL JOINTS SAWED WIDER THAN 1/8", CONTACT THE CONCRETE UNIT FOR SEALING RECOMMENDATIONS.

- (1) JOINT DEPTH AND TOLERANCE: $t/3 \pm 1/4"$.
- (2) BEND TIE BARS 90 DEGREES WHEN INSERTED IN THE L2 JOINTS, EXCEPT WHEN NOTED OTHERWISE IN THE PLANS.
- (3) CLEAN JOINT FACES WITH WATER DURING THE SAW CUTTING OPERATION OR BY WATER BLASTING AFTER SAWING.
- (4) CLEAN AND DRY JOINT FACES BY SANDBLASTING AND AIR BLASTING WHEN SEALING IS REQUIRED.
- (5) JOINT WIDTH TOLERANCE IS $+1/16"$ TO $-1/32"$.

- (6) CONTRACTOR'S OPTION TO USE KEYWAY WHEN:
- PLACING FIXED FORM CONSTRUCTION.
 - PLACING SLIPFORM CONSTRUCTION WHEN $t \geq 10"$.

USE OF KEYWAY FOR ANY OTHER APPLICATION REQUIRES APPROVAL BY THE ENGINEER. OTHER KEYWAY SHAPES MAY BE USED WITH THE APPROVAL OF THE CONCRETE ENGINEER.

- (7) WHEN CURB AND GUTTER IS NOT CONSTRUCTED AT THE SAME DEPTH AS ADJACENT CONCRETE, PLACE TIE BAR A MINIMUM OF 2" ABOVE THE CURB AND GUTTER GRADE.

- (8) PROVIDE NO. 4 TIE BAR, 30" LONG, SPACED AT 3' ON CENTER AT A DEPTH (D) BETWEEN $t/2$ AND $t/2$ PLUS 1".

- (9) DRILL THE HOLE 1/8" WIDER THAN THE NOMINAL OUTSIDE DIAMETER OF THE REINFORCEMENT BAR TO A DEPTH OF 9". INJECT A MnDOT-APPROVED EPOXY OR NON-SHRINK GROUT IN THE BACK OF THE DRILL HOLE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.

- (10) PROVIDE NO. 4 TIE BAR, 18" LONG, SPACED AT 3' ON CENTER AT A DEPTH (D) BETWEEN $t/2$ AND $t/2$ PLUS 1".

LEAD EXPERT OFFICE

CURT TURGEON
DIRECTOR
OFFICE OF MATERIALS & ROAD RESEARCH

PAVEMENT JOINTS
LONGITUDINAL (DESIGN L)

APPROVED:
01-30-2025

THOMAS STYRBICKI
STATE DESIGN ENGINEER

STANDARD PLAN
5-297.221

3 OF 5

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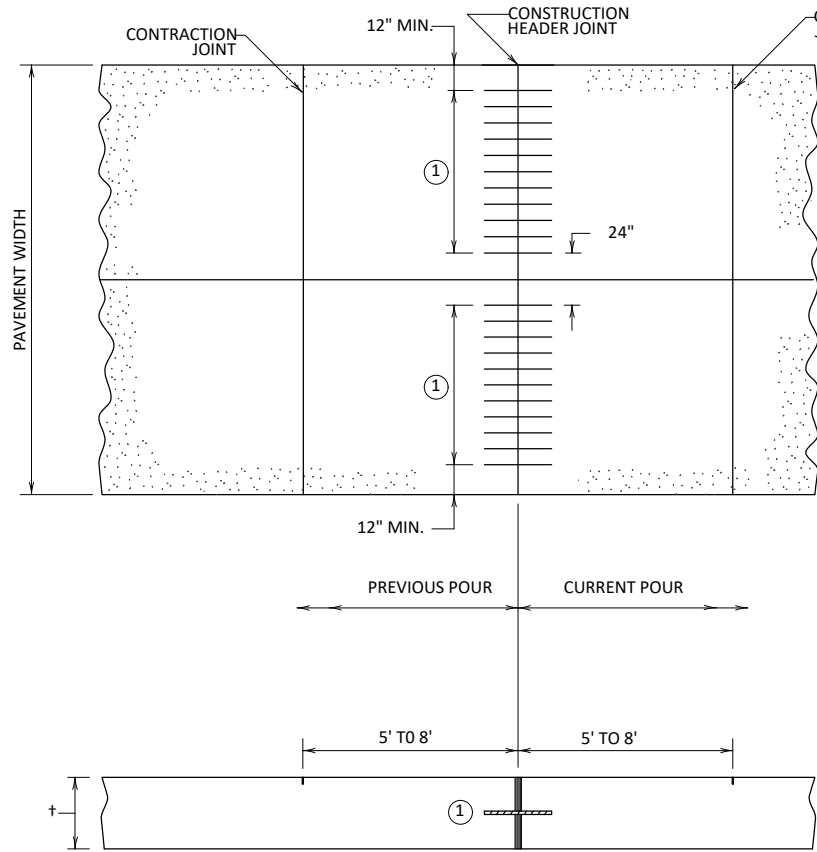
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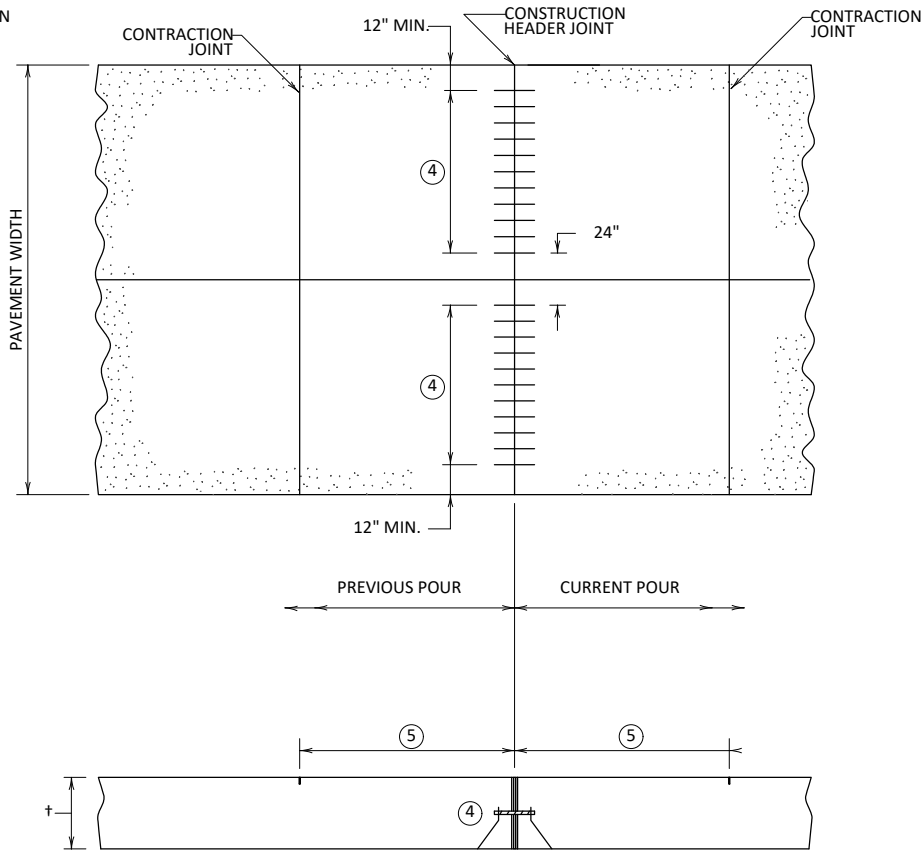
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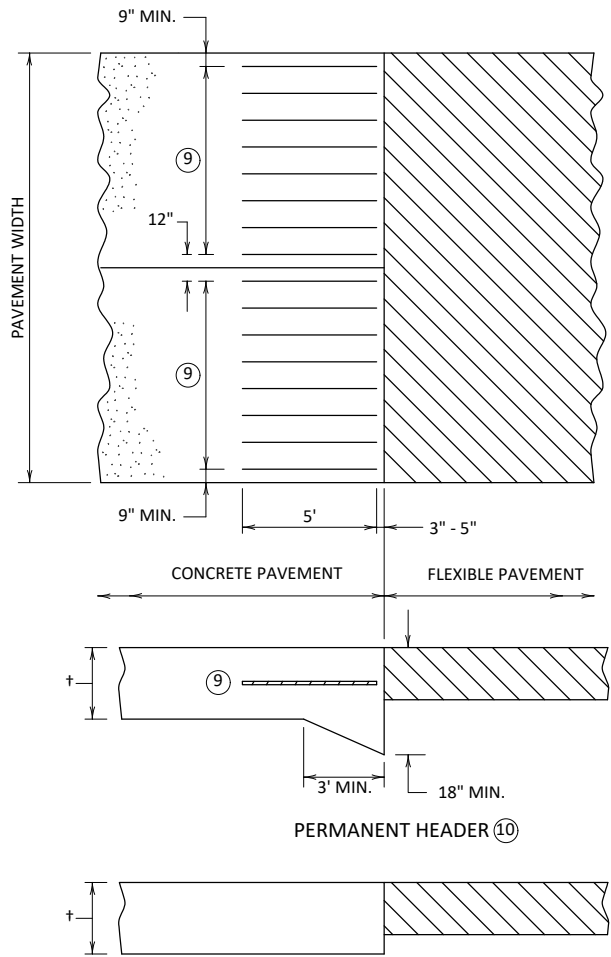
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REINFORCEMENT BAR CONSTRUCTION HEADERS



DOWEL BAR CONSTRUCTION HEADERS



NOTES:

PROVIDE EPOXY-COATED TIE BARS COMPLYING WITH SPEC. 3301.

- ① PROVIDE NO. 4 REINFORCEMENT BARS, 30" LONG, SPREAD 12" ON CENTER AT DEPTH OF $\frac{1}{2} \pm 1"$.

② PAVE PAST THE HEADER LOCATION. REMOVE END OF CONCRETE POUR. SET HEADER BOARD SHAPED TO PAVEMENT CROSS SECTION AND SLOTTED OR DRILLED FOR REINFORCEMENT BARS. INSERT THE REINFORCEMENT BARS AND FINISH THE CONCRETE BEHIND THE BOARD.

③ SET HEADER BOARD SHAPED TO PAVEMENT CROSS SECTION AND SLOTTED OR DRILLED FOR REINFORCEMENT BARS. PLACE THE CONCRETE BEHIND THE BOARD AND INSERT THE REINFORCEMENT BARS. CONSOLIDATE AND FINISH THE CONCRETE BEHIND THE HEADER BOARD.

④ PROVIDE DOWEL BARS IN ACCORDANCE WITH SPEC. 3302 AND THE CONTRACT.

⑤ DISTANCE EQUAL TO OR LESS THAN THE DESIGNED CONTRACTION JOINT SPACING IN ACCORDANCE WITH THE CONTRACT.

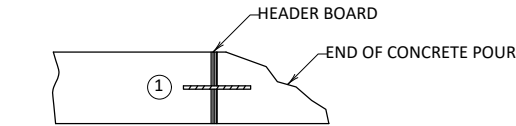
⑥ PLACE DOWEL BAR BASKET AT DESIRED HEADER LOCATION. SET HEADER BOARD SHAPED TO PAVEMENT CROSS SECTION ABOVE AND BELOW THE DOWELS. PAVE PAST THE HEADER LOCATION AND FINISH CONCRETE BEHIND THE HEADER BOARD. THOROUGHLY REMOVE ALL CONCRETE FROM THE EXPOSED DOWELS.

⑦ PLACE DOWEL BAR BASKET AT DESIRED HEADER LOCATION. SET HEADER BOARD SHAPED TO PAVEMENT CROSS SECTION ABOVE AND BELOW THE DOWELS. PLACE, CONSOLIDATE, AND FINISH THE CONCRETE BEHIND THE HEADER BOARD.
- ⑧ DRILL AND GROUT 18" LONG DOWEL OR REINFORCEMENT BARS SPACED AT 12" ON CENTER AT A DEPTH OF $\frac{1}{2} \pm 1"$. DRILL THE HOLE $\frac{1}{8}"$ GREATER THAN THE NOMINAL OUTSIDE DIAMETER OF THE BAR BEING PLACED TO A DEPTH OF 9". INJECT A MnDOT-APPROVED EPOXY OR NON-SHRINK GROUT IN THE BACK OF THE DRILL HOLE IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS.
- FOR DOWEL BAR HEADERS, USE DOWEL BARS HAVING A DIAMETER IN ACCORDANCE WITH SPEC. 3302 AND THE CONTRACT.
- FOR REINFORCEMENT BAR HEADERS, USE NO. 4 REINFORCEMENT BARS.

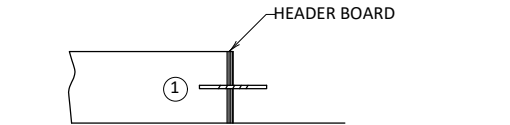
⑨ PROVIDE NO. 7 REINFORCEMENT BARS, 5' LONG, SPACED 18" ON CENTER AT DEPTH OF $\frac{1}{2} \pm 1"$.

⑩ USE PERMANENT HEADER WHEN LONG SECTIONS OF CONCRETE (400' OR GREATER) ABUT BITUMINOUS. CONTACT THE CONCRETE UNIT WHEN FUTURE CONCRETE IS BEING CONSTRUCTED ADJACENT TO AN EXISTING PERMANENT HEADER.

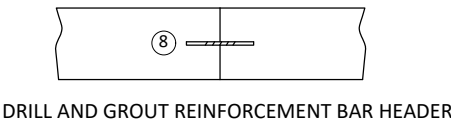
⑪ USE TERMINAL HEADER WHEN SHORT SECTIONS OF CONCRETE (LESS THAN 400') ABUT BITUMINOUS (ON SIDE STREETS, FOR EXAMPLE).



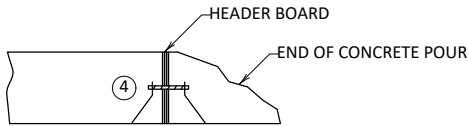
SLIPFORM PLACED REINFORCEMENT BAR HEADER ②



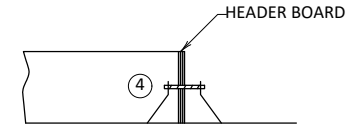
FIXED FORM PLACED REINFORCEMENT BAR HEADER ③



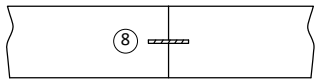
DRILL AND GROUT REINFORCEMENT BAR HEADER



SLIPFORM PLACED DOWEL BAR HEADER ⑥



FIXED FORM PLACED DOWEL BAR HEADER ⑦



DRILL AND GROUT DOWEL BAR HEADER

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PAVEMENT JOINTS
HEADERS

APPROVED:
01-30-2025

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5-297.221

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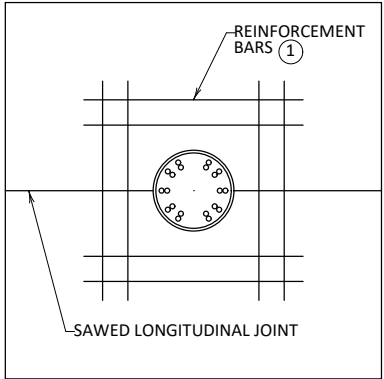
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PAVEMENT JOINTS

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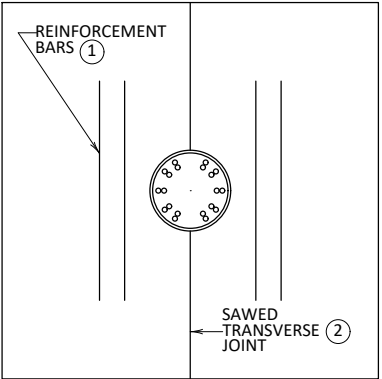
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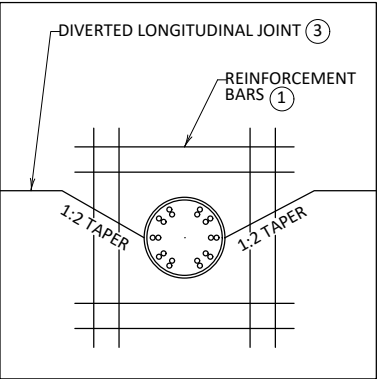
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LIC. NO. 46649 DATE MAY 27, 2025



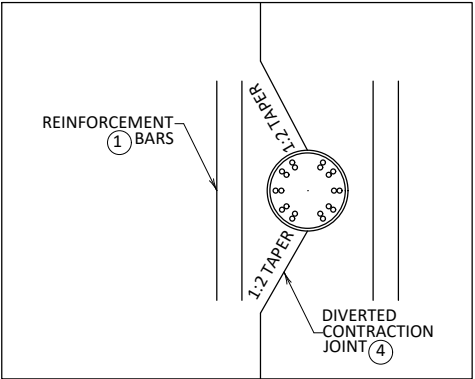
MANHOLE WITH LONGITUDINAL JOINT



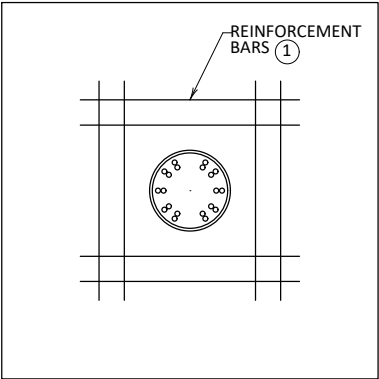
MANHOLE WITH CONTRACTION JOINT



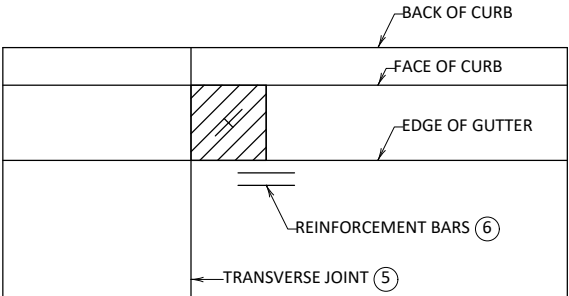
MANHOLE WITH DIVERTED LONGITUDINAL JOINT



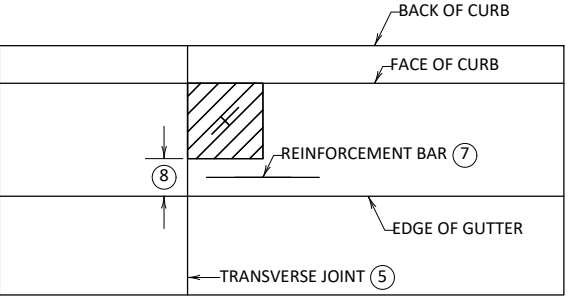
MANHOLE WITH DIVERTED CONTRACTION JOINT



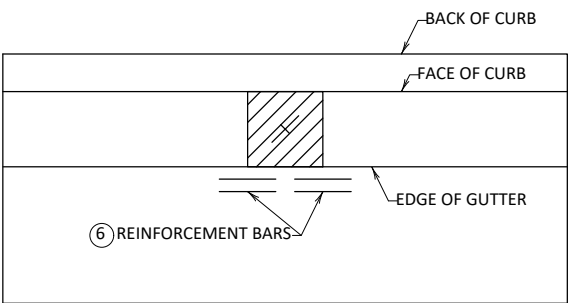
MANHOLE WITH NO JOINTS



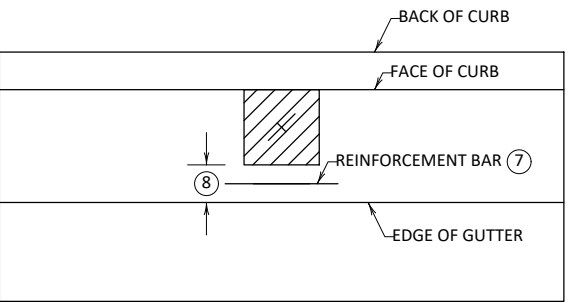
INLET WITH CONTRACTION JOINT



INLET WITH CONTRACTION JOINT (GUTTER WIDTHS GREATER THAN 24 ")



INLET WITHOUT CONTRACTION JOINT



INLET WITHOUT CONTRACTION JOINT (GUTTER WIDTHS GREATER THAN 24 ")

- NOTES:
- PROVIDE EPOXY-COATED REINFORCEMENT BARS COMPLYING WITH SPEC. 3301.
- DO NOT FORM "BOX-OUTS" OF CASTINGS UNLESS APPROVED BY THE ENGINEER.
- ① PLACE NO. 4 REINFORCEMENT BARS AT 8" AND 16" FROM THE EDGE OF THE MANHOLE. PLACE REINFORCEMENT BARS ON CHAIRS AT A DEPTH OF $\pm 2 \pm \frac{1}{4}$ ".
 - ② ADJUST TRANSVERSE JOINT TO INTERSECT MANHOLE WHERE POSSIBLE.
 - ③ IF DISTANCE BETWEEN THE LONGITUDINAL JOINT AND THE EDGE OF MANHOLE IS 2' OR LESS, DIVERT THE LONGITUDINAL JOINT AT A 2:1 TAPER RATE TO THE CENTER OF THE MANHOLE. IF THE DISTANCE IS GREATER THAN 2', DO NOT DIVERT THE JOINT AND SAW AS NORMAL. PLACE REINFORCEMENT BARS AROUND THE MANHOLE.
 - ④ IF THE DISTANCE FROM THE EDGE OF THE MANHOLE TO THE CONTRACTION JOINT IS LESS THAN 4', DIVERT THE CONTRACTION JOINT AT A 2:1 TAPER TO THE CENTER OF THE MANHOLE. DO NOT DOWEL THE TAPER. IF THE DISTANCE IS 4' OR GREATER, DO NOT DIVERT THE JOINT. WHENEVER POSSIBLE, MOVING THE CONTRACTION JOINT TO A DISTANCE 4' OR GREATER IS PREFERRED OVER DIVERTING THE JOINT.
 - ⑤ ALIGN TRANSVERSE JOINT WITH ONE EDGE OF INLET WHERE PRACTICAL.
 - ⑥ PLACE TWO NO. 4 \times 18" REINFORCEMENT BARS SPACED 4" AND 10" FROM LONGITUDINAL JOINT.
 - ⑦ PLACE NO. 4 \times 36" REINFORCEMENT BAR BETWEEN INLET AND EDGE OF GUTTER.
 - ⑧ FILL IN ANY GAP THAT IS 1" OR LESS USING BACKER ROD AND HOT POUR.

LEAD
EXPERT
OFFICE

CURT TURGEON
DIRECTOR
OFFICE OF MATERIALS
& ROAD RESEARCH

PAVEMENT JOINTS
UTILITY FIXTURES

APPROVED:
01-30-2025

THOMAS STYRBICKI
STATE DESIGN ENGINEER

STANDARD
PLAN
5-297.221

5 OF 5

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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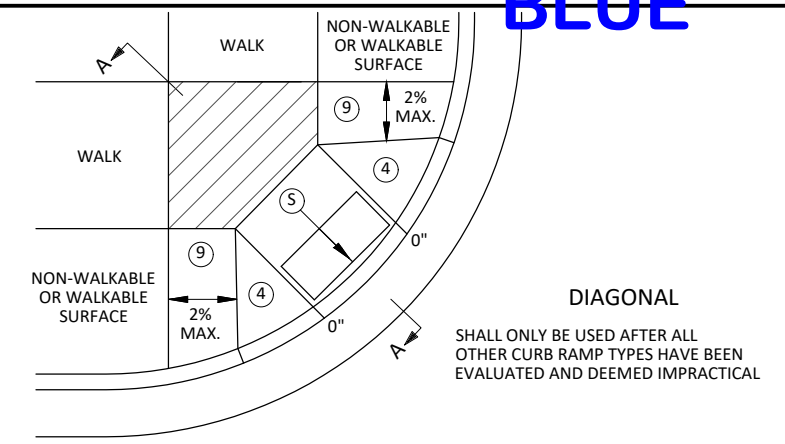
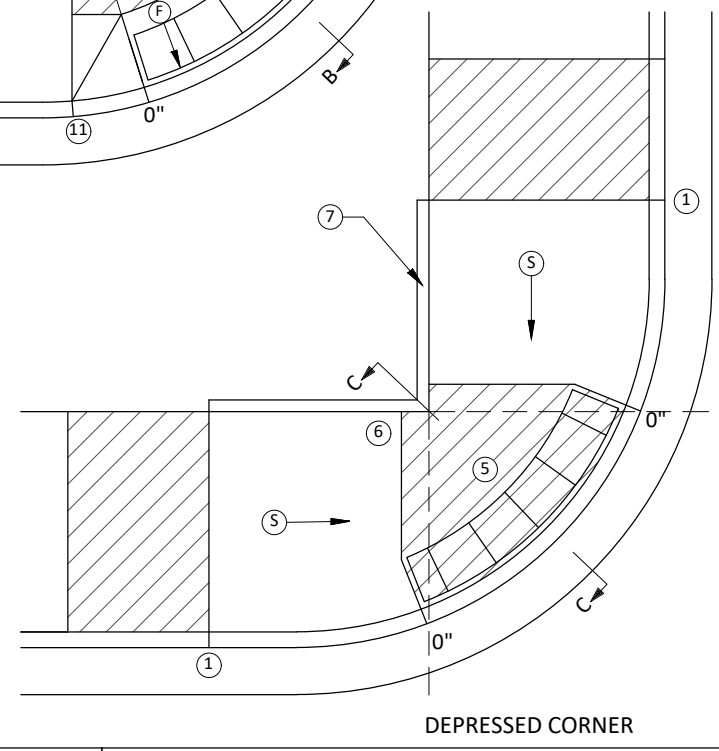
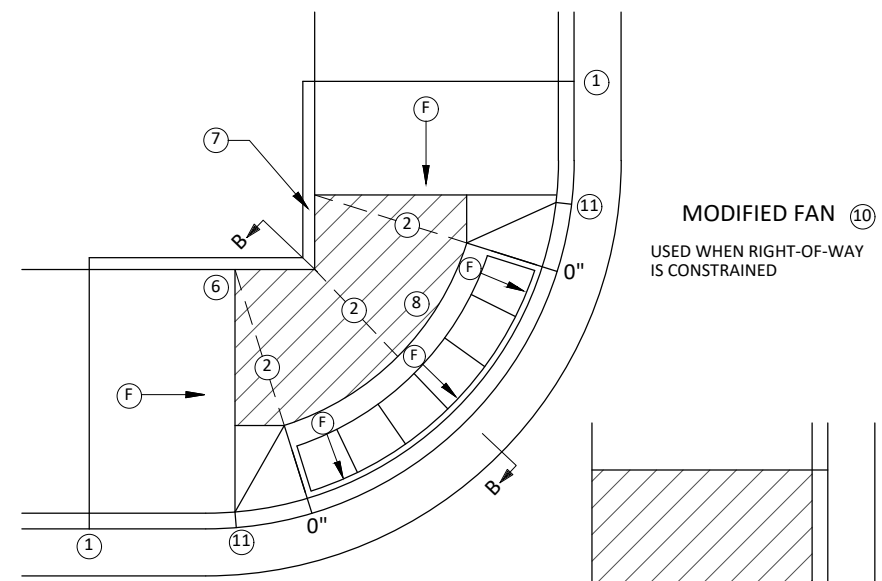
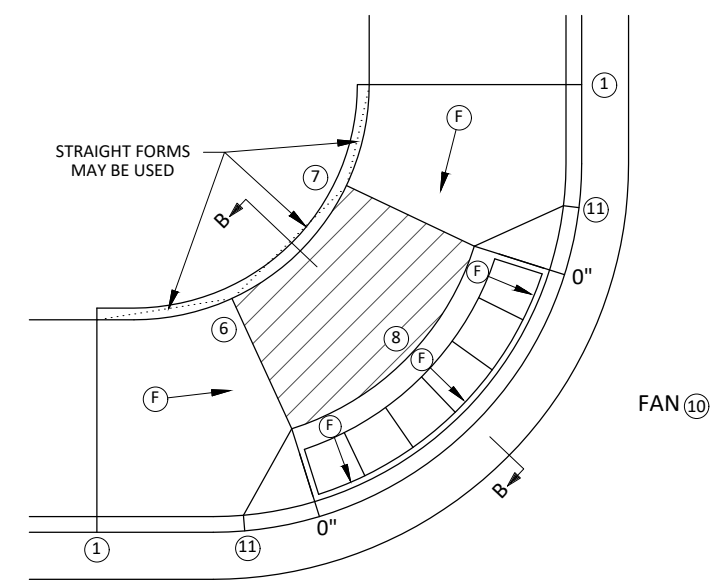
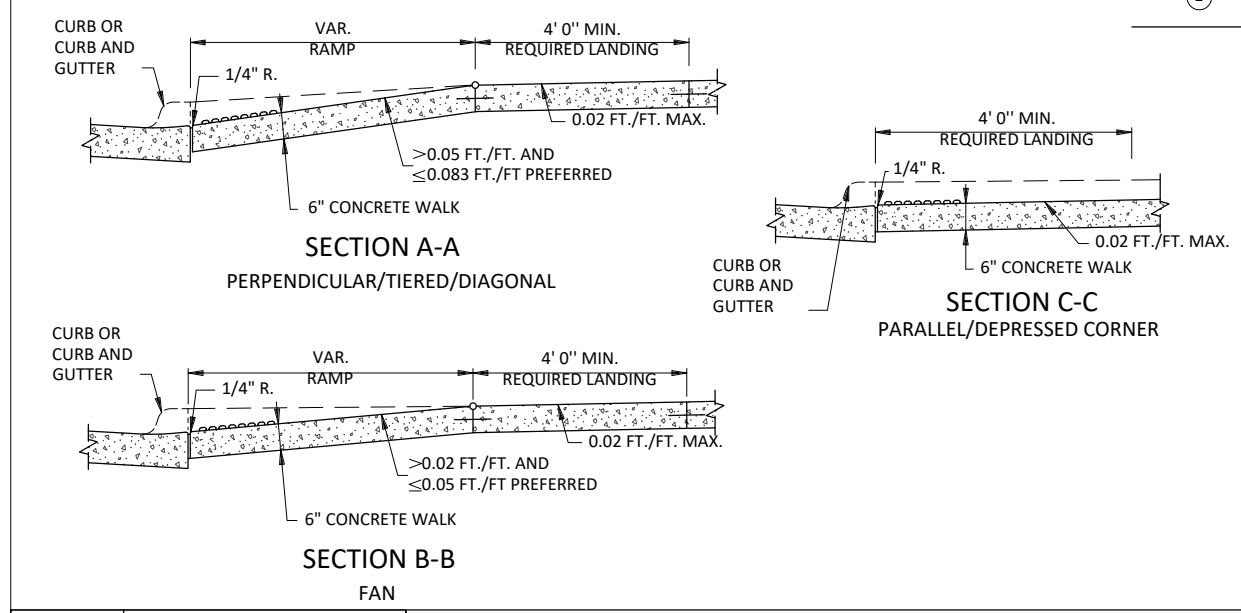
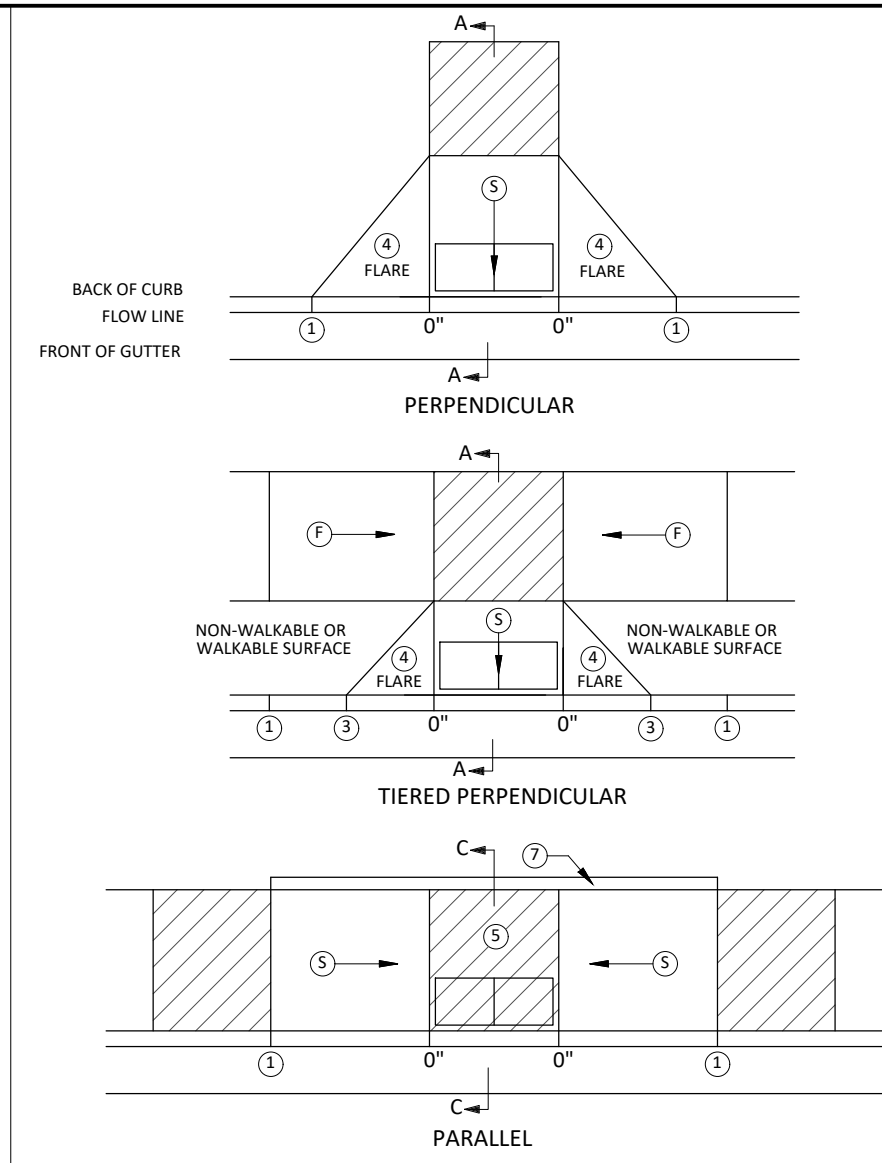


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DRAWN	JVZ	95% REV	05/22/2025	
CHECKED	TLW			
CLIENT PROJ. NO.				

CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STANDARD PLANS
PAVEMENT JOINTS

SHEET

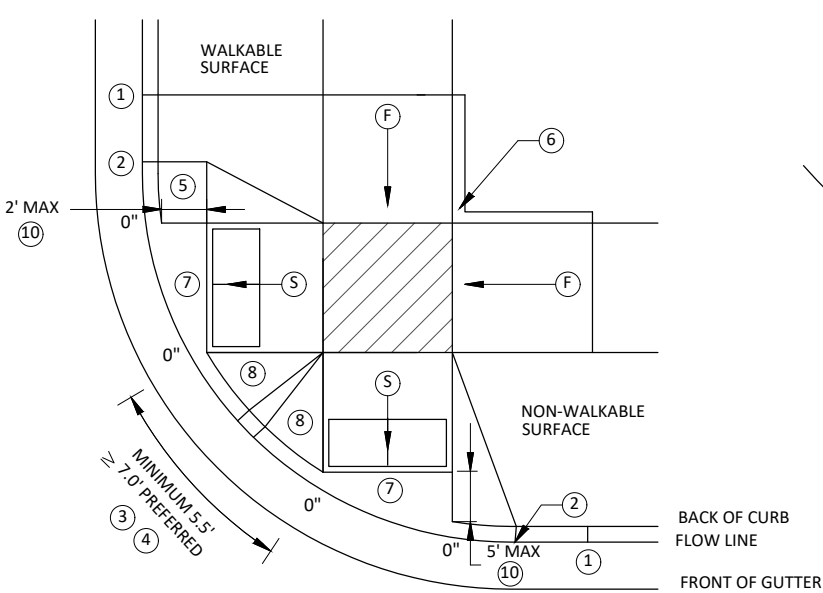
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- NOTES:
- LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMP THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE GREATER THAN 2%.
- INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.
- SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30' OF VERTICAL RISE WHEN THE LONGITUDINAL RUNNING SLOPE IS GREATER THAN 5.0%.
- CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOPS OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.
- ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL. THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH. (EXCEPT AS STATED IN 6 BELOW).
- TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 OF 6 FOR ALL SEPARATELY POURED INITIAL LANDINGS.
- WHEN SIDEWALK IS AT BACK OF CURB, TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE. MAINTAIN POSITIVE BOULEVARD DRAINAGE TO TOP OF CURB.
- ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.
- 4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER THE ENTIRE PAR WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK WITH THE EXCEPTION OF 3" MAXIMUM ON EACH OUTSIDE EDGE WHICH ENSURES THE DETECTABLE WARNINGS ARE ENCASED IN CONCRETE WHEN ADJACENT TO TURF. WHEN ADJACENT TO CONCRETE FLARES 0" - 3" OFFSET IS ALLOWED.
- WHEN DESIGNING OR ORDERING RECTANGULAR DETECTABLE WARNING SURFACES SHOULD BE 6" LESS THAN THE INCOMING PAR. ARC LENGTH OF THE RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.
- RECTANGULAR DETECTABLE WARNINGS SHALL BE SETBACK 3" FROM THE BACK OF CURB. RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB.
- 1 MATCH FULL HEIGHT CURB.
 - 2 4' MINIMUM DEPTH LANDING REQUIRED ACROSS TOP OF RAMP.
 - 3 3" HIGH CURB WHEN USING A 3' LONG RAMP, 4" HIGH CURB WHEN USING A 4' LONG RAMP.
 - 4 SEE SHEET 4 OF 6, TYPICAL SIDE TREATMENT OPTIONS, FOR DETAILS ON FLARES AND RETURNED CURBS.
 - 5 DETECTABLE WARNINGS MAY BE PART OF THE 4' X 4' MIN. LANDING AREA IF IT IS NOT FEASIBLE TO CONSTRUCT THE LANDING OUTSIDE OF THE DETECTABLE WARNING AREA.
 - 6 THE GRADE BREAK SHALL BE PERPENDICULAR TO THE BACK OF WALK. THIS WILL ENSURE THAT THE GRADE BREAK IS PERPENDICULAR TO THE DIRECTION OF TRAVEL. (TYPICAL FOR ALL)
 - 7 WHEN ADJACENT TO GRASS, GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS LESS THAN 5% RUNNING SLOPE SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
 - 8 A 7' MIN TOP RADIUS GRADE BREAK IS REQUIRED TO BE CONSTRUCTIBLE.
 - 9 PAVE FULL WALK WIDTH.
 - 10 "S" SLOPES ON FANS SHALL ONLY BE USED WHEN ALL OTHER FEASIBLE OPTIONS HAVE BEEN EVALUATED AND DEEMED IMPRACTICAL.
 - 11 INTERMEDIATE CURB HEIGHTS TAPER SHALL RISE AT 8-10% TO A MINIMUM 3" CURB HEIGHT. REDUCE INTERMEDIATE CURB HEIGHT TO 2+ INCHES IF NECESSARY TO MATCH ADJACENT BOULEVARD OR SIDEWALK GRADES.

LEGEND	
THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.	
	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
	INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.
	LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PARS.
	X" CURB HEIGHT

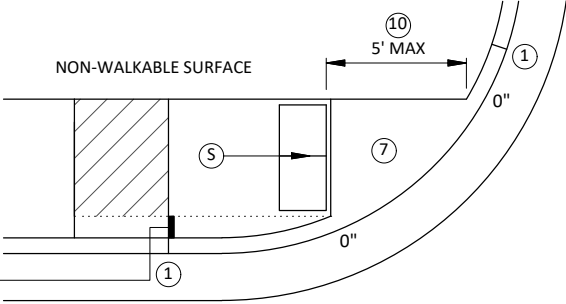
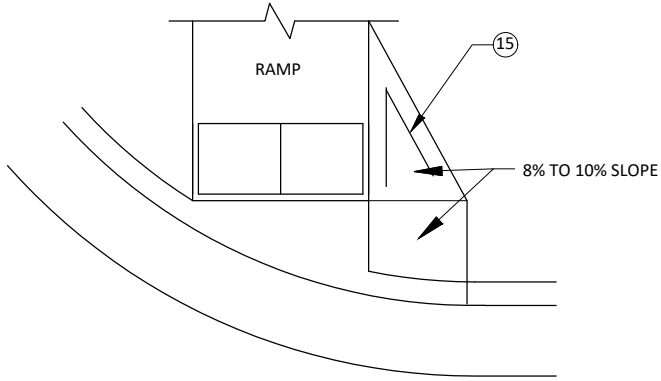
LEAD EXPERT OFFICE	JEFFREY PERKINS OPERATIONS DIVISION		PEDESTRIAN CURB RAMP DETAILS	APPROVED: 11-04-2021 REVISED:	THOMAS STYRBICKI STATE DESIGN ENGINEER	STANDARD PLAN 5-297.250	1 OF 6
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.		BOLTON & MENK		CITY OF WORTHINGTON, MINNESOTA EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS STANDARD PLANS PEDESTRIAN CURB RAMP DETAILS		SHEET C1.14	
TRAVIS L. WINTER LIC. NO. 46649 DATE MAY 27, 2025		1501 SOUTH STATE ST, SUITE 100 FAIRMONT, MN 56031 Phone: (507) 238-4738 Email: Fairmont@bolton-menk.com www.bolton-menk.com		DESIGNED: JTO DRAWN: JVZ CHECKED: TLW CLIENT PROJ. NO.		NO. 95% REV 05/22/2025	



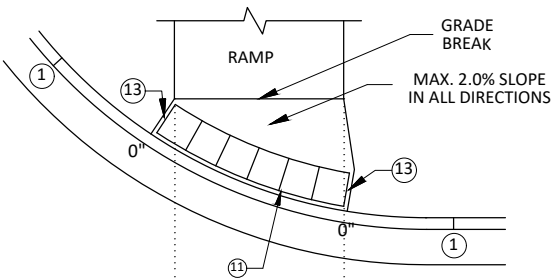
COMBINED DIRECTIONAL

DIRECTIONAL RAMP WALKABLE FLARE

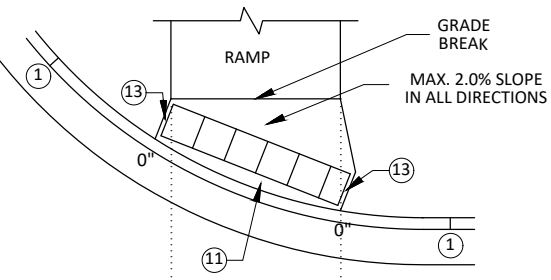
IF NON-CONCRETE BLVD. IS CONSTRUCTED AND IS LESS THAN 2' IN WIDTH AT TOP OF CURB TRANSITION, PAVE CONCRETE RAMP WIDTH TO ADJACENT BACK OF CURB.



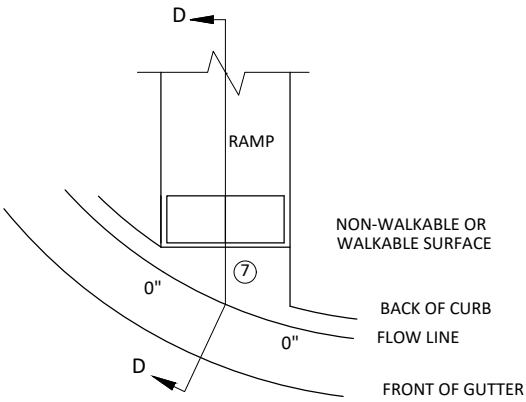
STANDARD ONE-WAY DIRECTIONAL



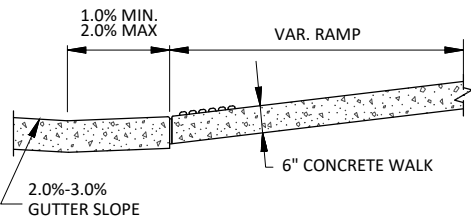
DETECTABLE WARNING PLACEMENT WHEN SETBACK CRITERIA IS EXCEEDED



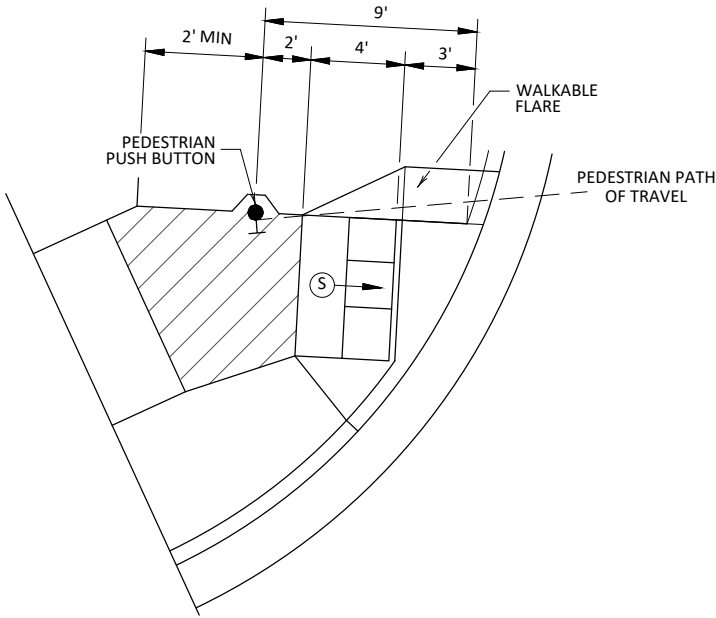
ONE-WAY DIRECTIONAL WITH DETECTABLE WARNING AT BACK OF CURB



CURB FOR DIRECTIONAL RAMPS



SECTION D-D



SEMI-DIRECTIONAL RAMP

3' DOME SETBACK, 4' LONG RAMP AND PUSH BUTTON 9' FROM THE BACK OF CURB

PRIMARYLY USED FOR APS APPLICATIONS WHERE THE PAR DOES NOT CONTINUE PAST THE PUSH BUTTON (DEAD-END SIDEWALK)

NOTES:

LANDINGS SHALL BE LOCATED ANYWHERE THE PEDESTRIAN ACCESS ROUTE (PAR) CHANGES DIRECTION, AT THE TOP OF RAMPS THAT HAVE RUNNING SLOPES GREATER THAN 5.0%, AND IF THE APPROACHING WALK IS INVERSE GRADE.

INITIAL CURB RAMP LANDINGS SHALL BE CONSTRUCTED WITHIN 15' FROM THE BACK OF CURB, WITH 6' FROM THE BACK OF CURB BEING THE PREFERRED DISTANCE, ONLY APPLICABLE WHEN THE INITIAL RAMP RUNNING SLOPE IS OVER 5.0%.

SECONDARY CURB RAMP LANDINGS ARE REQUIRED FOR EVERY 30' OF VERTICAL RISE WHEN THE LONGITUDINAL SLOPE IS GREATER THAN 5.0%.

CONTRACTION JOINTS SHALL BE CONSTRUCTED ALONG ALL GRADE BREAKS WITHIN THE PAR. 1/4" DEEP VISUAL JOINTS SHALL BE USED AT THE TOP GRADE BREAK OF CONCRETE FLARES ADJACENT TO WALKABLE SURFACES.

ALL GRADE BREAKS WITHIN THE PAR SHALL BE PERPENDICULAR TO THE PATH OF TRAVEL. THUS BOTH SIDES OF A SLOPED WALKING SURFACE MUST BE EQUAL LENGTH.

TO ENSURE INITIAL RAMPS AND INITIAL LANDINGS ARE PROPERLY CONSTRUCTED, LANDINGS SHALL BE CAST SEPARATELY. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON SHEET 6 AND THE ADA SPECIAL PROVISION (PROSECUTION OF WORK).

TOP OF CURB SHALL MATCH PROPOSED ADJACENT WALK GRADE.

WHEN THE BOULEVARD IS 4' WIDE OR LESS, THE TOP OF CURB TAPER SHALL MATCH THE RAMP SLOPES TO REDUCE NEGATIVE SIDEWALK SLOPES FROM THE TOP BACK OF CURB TO THE PAR.

ALL RAMP TYPES SHOULD HAVE A MINIMUM 3' LONG RAMP LENGTH.

4' MINIMUM WIDTH OF DETECTABLE WARNING IS REQUIRED FOR ALL RAMPS. DETECTABLE WARNINGS SHALL CONTINUOUSLY EXTEND FOR A MIN. OF 24" IN THE PATH OF TRAVEL. DETECTABLE WARNING TO COVER THE ENTIRE PAR WIDTH OF SHARED-USE PATHS AND THE ENTIRE PAR WIDTH OF THE WALK WITH THE EXCEPTION OF 3" MAXIMUM ON EACH OUTSIDE EDGE WHICH ENSURES THE DETECTABLE WARNINGS ARE ENCASED IN CONCRETE WHEN ADJACENT TO TURF. WHEN ADJACENT TO CONCRETE FLARES 0" - 3" OFFSET IS ALLOWED.

WHEN DESIGNING OR ORDERING RECTANGULAR DETECTABLE WARNING SURFACES SHOULD BE 6" LESS THAN THE INCOMING PAR. ARC LENGTH OF THE RADIAL DETECTABLE WARNINGS SHOULD NOT BE GREATER THAN 20 FEET.

RADIAL DETECTABLE WARNINGS SHALL BE SETBACK 3" MINIMUM TO 6" MAXIMUM FROM THE BACK OF CURB. SEE NOTES 10 & 11 FOR INFORMATION REGARDING RECTANGULAR DETECTABLE WARNING PLACEMENT.

- 1 MATCH FULL CURB HEIGHT.
- 2 3" HIGH CURB WHEN USING A 3' LONG RAMP
4" HIGH CURB WHEN USING A 4' LONG RAMP.
- 3 3" MINIMUM CURB HEIGHT (5.5' MIN. DISTANCE REQUIRED BETWEEN DOMES)
4" PREFERRED (7' MIN. DISTANCE REQUIRED BETWEEN DOMES).
- 4 THE "BUMP" IN BETWEEN THE RAMPS SHOULD NOT BE IN THE PATH OF TRAVEL FOR COMBINED DIRECTIONAL RAMPS. IF THIS OCCURS MODIFY THE RAMP LOCATION OR SWITCH RAMP TO A FAN/DEPRESSED CORNER.
- 5 WHEN USING CONCRETE PAVED FLARES ON THE OUTSIDE OF DIRECTIONAL RAMPS, AND ADJACENT TO A WALKABLE SURFACE, DIRECTIONAL RAMP FLARES SHALL BE USED. SEE THE DETAIL ON THIS SHEET.
- 6 GRADING SHALL ALWAYS BE USED WHEN FEASIBLE. V CURB, IF USED, SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS. WHEN ADJACENT TO PARKING LOTS, CONCRETE OR BITUMINOUS TAPERS SHOULD BE USED OVER V CURB TO REDUCE TRIPPING HAZARDS AND FACILITATE SNOW & ICE REMOVAL.
- 7 MAX. 2.0% SLOPE IN ALL DIRECTIONS IN FRONT OF GRADE BREAK AND DRAIN TO FLOW LINE. SHALL BE CONSTRUCTED INTEGRAL WITH CURB AND GUTTER.
- 8 8% TO 10% WALKABLE FLARE.
- 9 PLACE DOMES AT THE BACK OF CURB WHEN ALLOWABLE SETBACK CRITERIA IS EXCEEDED.
- 10 FRONT EDGE OF DETECTABLE WARNING SHALL BE SET BACK 2' MAXIMUM WHEN ADJACENT TO WALKABLE SURFACE, AND 5' MAXIMUM WHEN ADJACENT TO NON-WALKABLE SURFACE WITH ONE CORNER SET 3" FROM BACK OF CURB. A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.
- 11 RECTANGULAR DETECTABLE WARNINGS MAY BE SETBACK UP TO 9' FROM THE BACK OF CURB WITH CORNERS SET 3" FROM BACK OF CURB. IF 9" SETBACK IS EXCEEDED USE RADIAL DETECTABLE WARNINGS.
- 12 FOR DIRECTIONAL RAMPS WITH THE DETECTABLE WARNINGS PLACED AT THE BACK OF CURB, THE DETECTABLE WARNINGS SHALL COVER THE ENTIRE WIDTH OF THE WALK/PATH. THIS ENSURES A DETECTABLE EDGE AND HELPS ELIMINATE THE CURB TAPER OBSTRUCTING THE PATH OF PEDESTRIAN TRAVEL.
- 13 THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE BACK OF CURB. MAINTAIN 3" BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- 14 TO BE USED FOR ALL DIRECTIONAL RAMPS, EXCEPT WHERE DOMES ARE PLACED ALONG THE BACK OF CURB.
- 15 PLACE 2 NO. 4 BARS 4 INCHES FROM SIDE OF FORMS WITH A MINIMUM 2 INCHES OF CONCRETE COVER ALONG EACH SIDE OF FLARE (INCIDENTAL).

LEGEND

THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.



INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.



INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE GREATER THAN 2.0% AND LESS THAN 5.0% IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL NOT EXCEED 2.0%.



LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PAR.

X" CURB HEIGHT

LEAD
EXPERT
OFFICE

JEFFREY PERKINS
OPERATIONS DIVISION

PEDESTRIAN CURB RAMP DETAILS

APPROVED: 11-04-2021
REVISED:

THOMAS STYRBICKI
STATE DESIGN ENGINEER

STANDARD
PLAN
5-297.250

2 OF 6

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

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LIC. NO. 46649 DATE MAY 27, 2025



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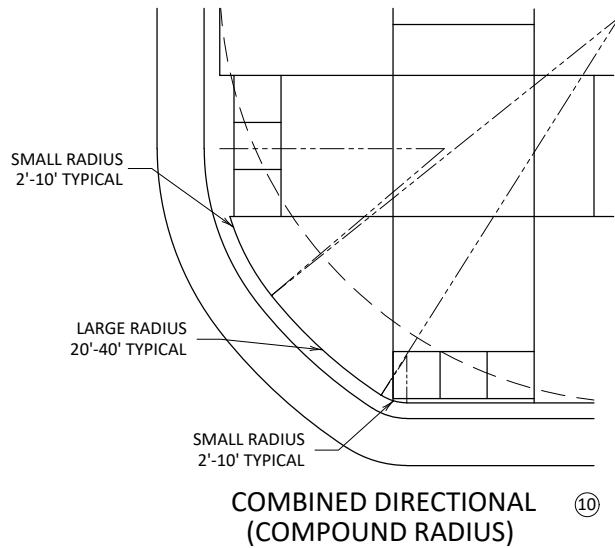
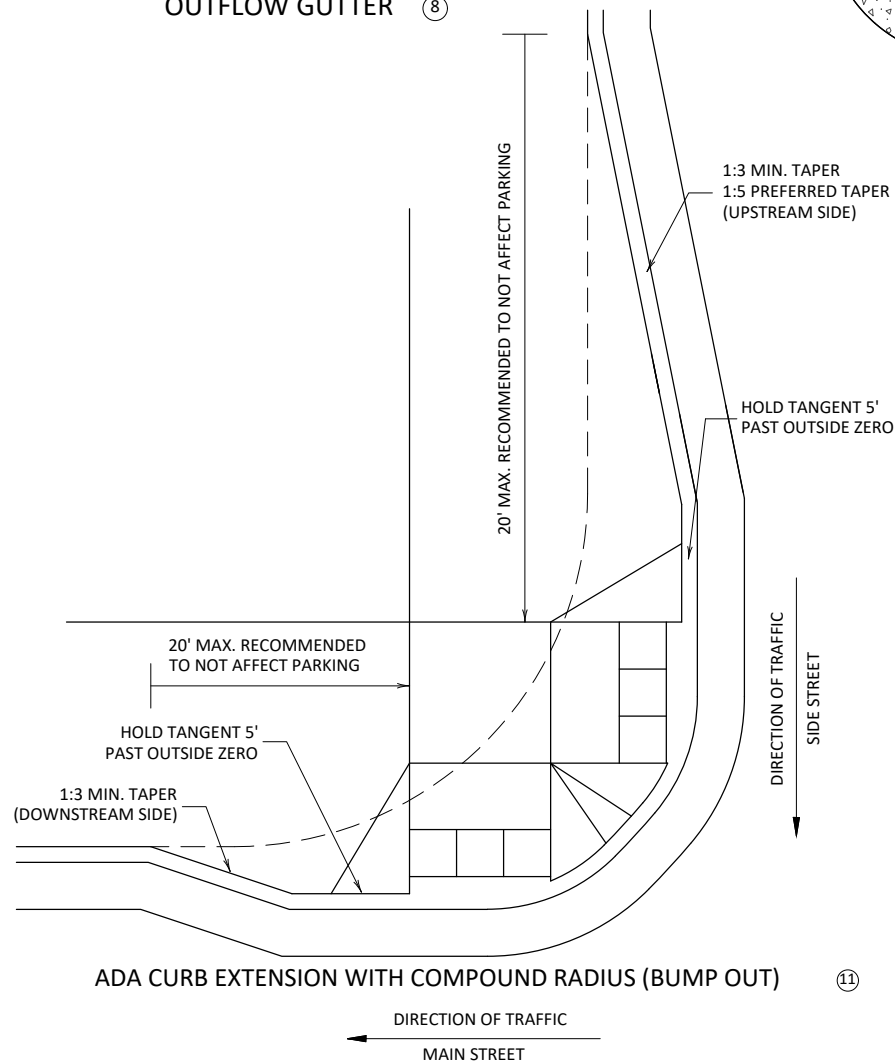
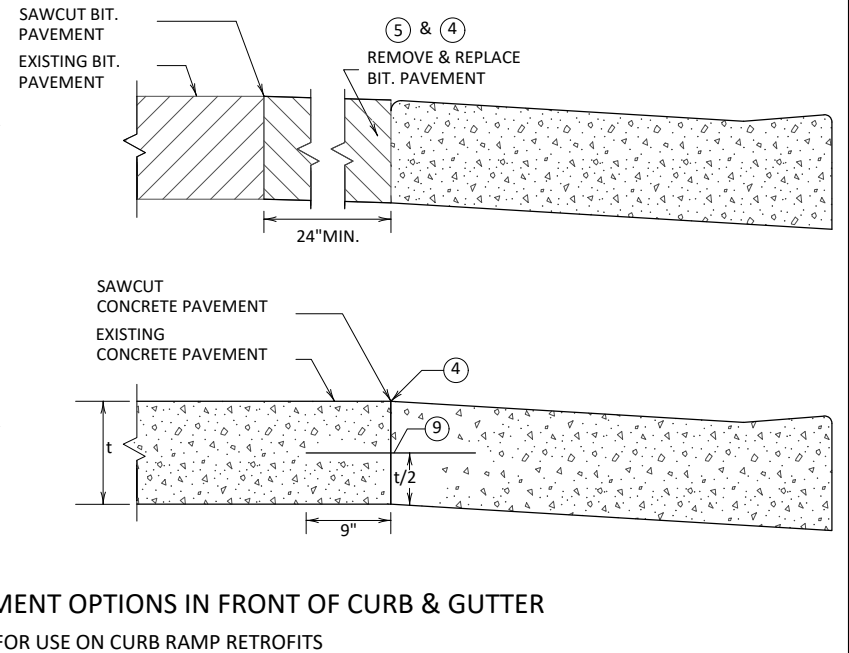
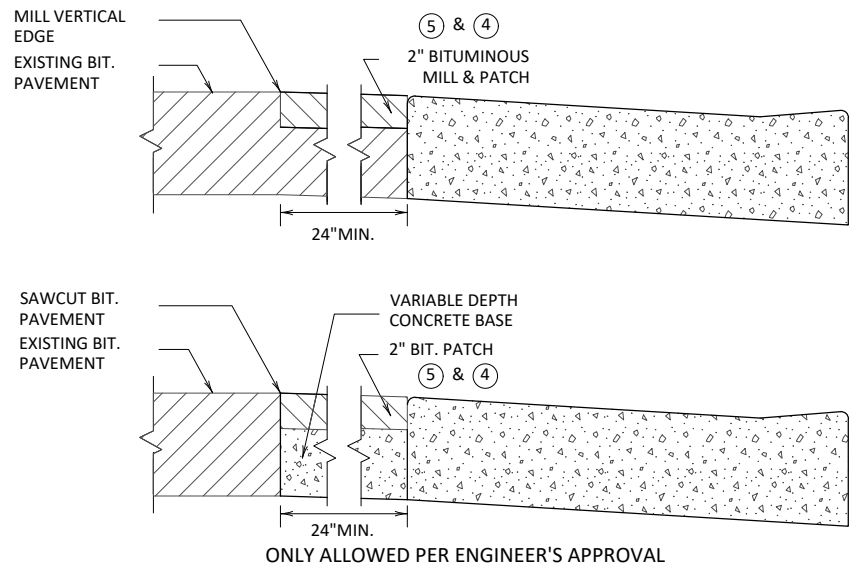
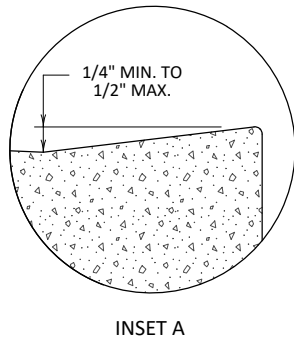
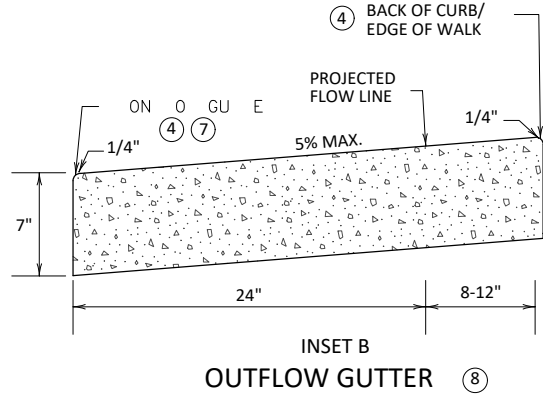
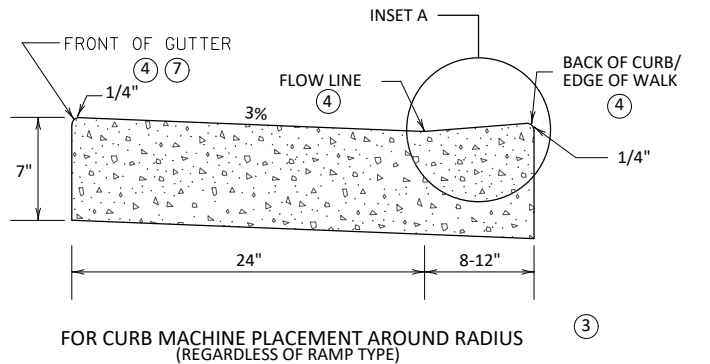
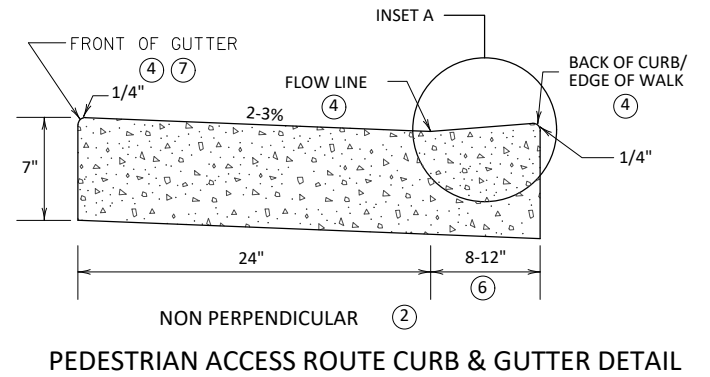
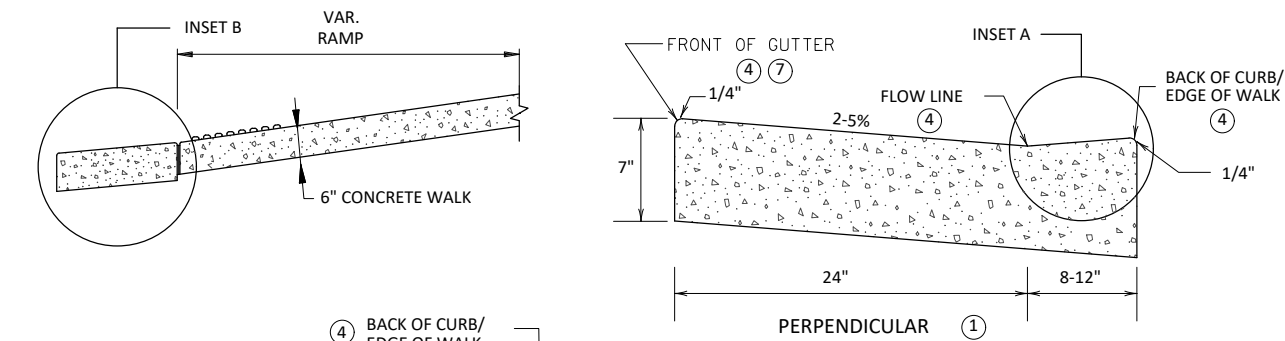
CITY OF WORTHINGTON, MINNESOTA

EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS

STANDARD PLANS
PEDESTRIAN CURB RAMP DETAILS

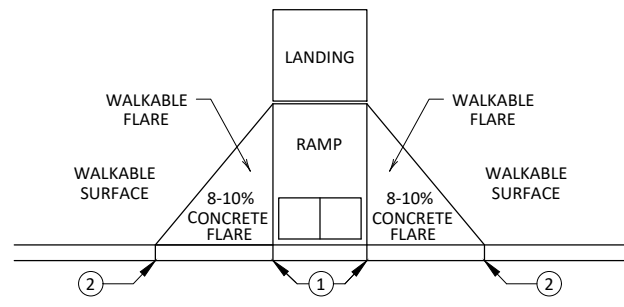
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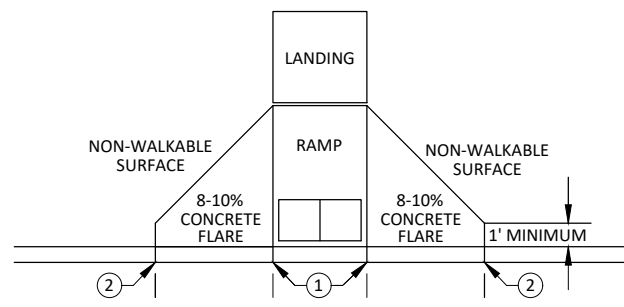


- NOTES:
- POSITIVE FLOW LINE DRAINAGE SHALL BE MAINTAINED THROUGH THE PEDESTRIAN ACCESS ROUTE (PAR) AT A 2% MAXIMUM.
- NO PONDING SHALL BE PRESENT IN THE PAR.
- ANY VERTICAL LIP THAT OCCURS AT THE FLOW LINE SHALL NOT BE GREATER THAN 1/4 INCH.
- ① FOR USE AT CURB CUTS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: PERPENDICULAR, TIERED PERPENDICULAR, PARALLEL, AND DIAGONAL RAMP.
- ② FOR USE AT CURB RAMPS WHERE THE PEDESTRIAN'S PATH OF TRAVEL IS ASSUMED NON PERPENDICULAR TO THE GUTTER FLOW LINE. RAMP TYPES INCLUDE: FANS & DEPRESSED CORNERS.
- ③ BEGIN GUTTER SLOPE TRANSITION 10' OUTSIDE OF ALL CURB RAMPS.
- ④ THERE SHALL BE NO VERTICAL DISCONTINUITIES GREATER THAN 1/4".
- ⑤ ELEVATION CHANGE TAKES PLACE FROM THE EXISTING TO NEW FRONT OF GUTTER. PATCH IS USED TO MATCH THE NEW GUTTER FACE INTO THE EXISTING ROADWAY.
- ⑥ VARIABLE WIDTH FOR DIRECTIONAL CURB APPLICATIONS. SEE SHEET 2 FOR DIRECTIONAL CURB SLOPE REQUIREMENTS.
- ⑦ TOP FRONT OF GUTTER SHALL BE CONSTRUCTED FLUSH WITH PROPOSED ADJACENT PAVEMENT ELEVATION. TOP 1.5" OF THE GUTTER FACE MUST BE A FORMED EDGE. PAR GUTTER SHALL NOT BE OVERLAID.
- ⑧ SHOULD BE USED AT VERTICALLY CONSTRAINED AREAS WHEN AT A DRAINAGE HIGH POINT OR SUPER ELEVATED ROADWAY SEGMENTS.
- ⑨ DRILL AND GROUT NO. 4 EPOXY-COATED 18" LONG TIE BARS AT 30" CENTER TO CENTER INTO EXISTING CONCRETE PAVEMENT 1' MINIMUM FROM ALL JOINTS.
- ⑩ HELPS PROVIDE TWO SEPARATE RAMPS, REDUCES THE DOME SETBACK LENGTH AND MINIMIZES DIRECTIONAL CURB. THIS RADIUS DESIGN CLOSELY FOLLOWS THE TURNING VEHICLE PATH WHILE OPTIMIZING CURB RAMP LENGTH.
- ⑪ CURB EXTENSIONS SHOULD BE USED IN VERTICALLY CONSTRAINED AREAS, USUALLY IN DOWNTOWN ROADWAY SEGMENTS WHERE ON-STREET PARKING IS AVAILABLE. CURB EXTENSIONS SHOULD BE CONSIDERED FOR APS INTERSECTIONS WHERE SPACE IS LIMITED. PUSH BUTTONS MUST MEET APS CRITERIA AS DESCRIBED IN THE PUSH BUTTON LOCATION DETAIL SHEET.

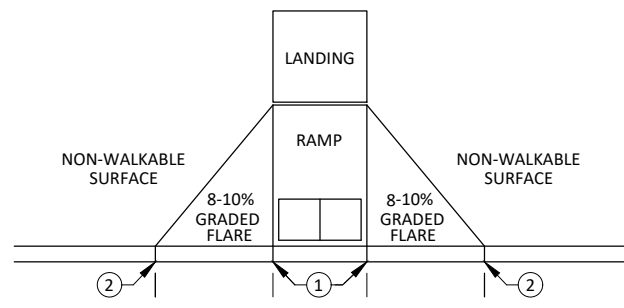
LEAD EXPERT OFFICE	JEFFREY PERKINS OPERATIONS DIVISION	PEDESTRIAN CURB RAMP DETAILS	APPROVED: 11-04-2021 REVISED:	STANDARD PLAN 5-297.250	3 OF 6
I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.		DESIGNED: JTO DRAWN: JVZ CHECKED: TLW CLIENT PROJ. NO.		CITY OF WORTHINGTON, MINNESOTA EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS STANDARD PLANS PEDESTRIAN CURB RAMP DETAILS	
TRAVIS L. WINTER LIC. NO. 46649 DATE MAY 27, 2025		1501 SOUTH STATE ST, SUITE 100 FAIRMONT, MN 56031 Phone: (507) 238-4738 Email: Fairmont@bolton-menk.com www.bolton-menk.com		SHEET C1.16	



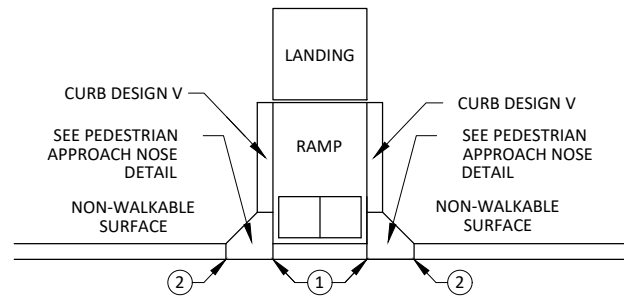
PAVED FLARES
ADJACENT TO WALKABLE SURFACE



PAVED FLARES
ADJACENT TO NON-WALKABLE SURFACE

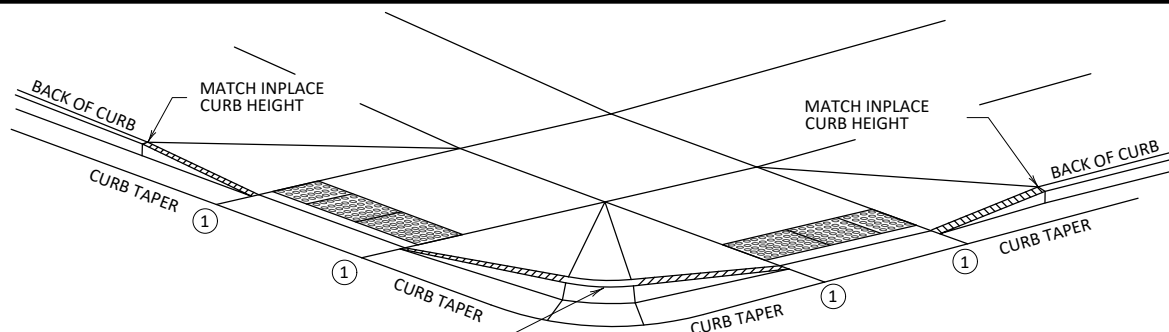


GRADED FLARES



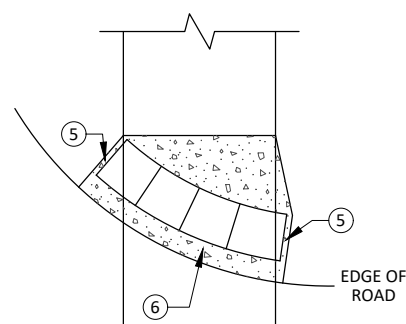
RETURNED CURB ④

TYPICAL SIDE TREATMENT OPTIONS ③ ⑩

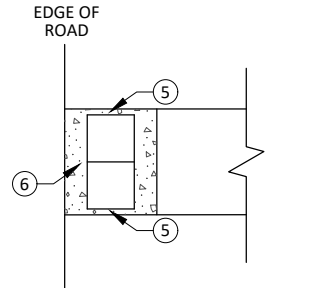


3" MINIMUM CURB HEIGHT, 4" PREFERRED
(MEASURED AT FRONT FACE OF CURB)
FOR A MIN. 6" LENGTH (MEASURED ALONG FLOW LINE)

DETECTABLE EDGE WITH
CURB AND GUTTER ⑦

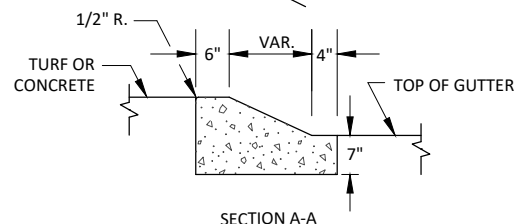
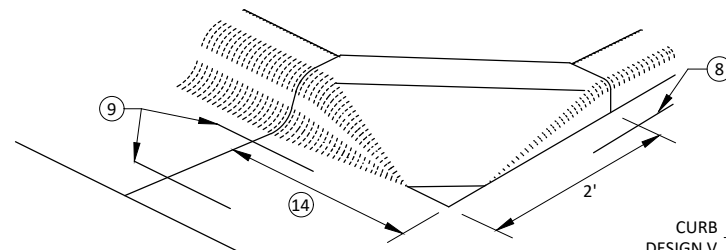


RADIAL DETECTABLE WARNING

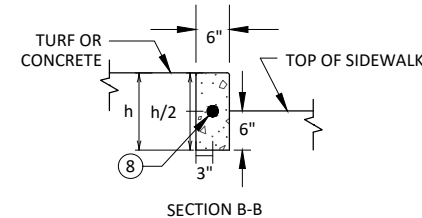


RECTANGULAR DETECTABLE WARNING

DETECTABLE EDGE WITHOUT CURB AND GUTTER



SECTION A-A



SECTION B-B

PEDESTRIAN APPROACH
NOSE DETAIL
(FOR RETURNED CURB
SIDE TREATMENT)

NOTES:

INTERMEDIATE CURB HEIGHTS TAPER SHALL RISE AT 8-10% TO A MINIMUM 3 INCH CURB HEIGHT. INCREASE CURB TAPER LENGTH AT LESS THAN 8% OR REDUCE INTERMEDIATE CURB HEIGHT TO 2+ INCHES IF NECESSARY TO MATCH ADJACENT BOULEVARD OR SIDEWALK GRADES.

SEE STANDARD PLATE 7038 AND THIS SHEET FOR ADDITIONAL DETAILS ON DETECTABLE WARNING.

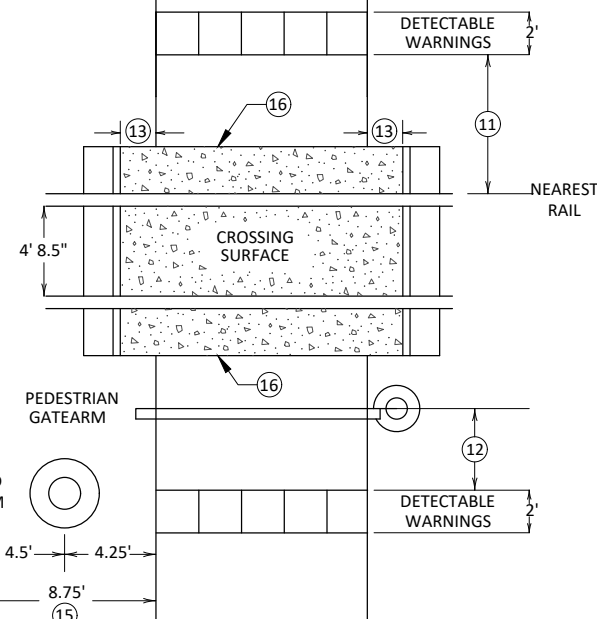
A WALKABLE SURFACE IS DEFINED AS A PAVED SURFACE ADJACENT TO A CURB RAMP WITHOUT RAISED OBSTACLES THAT COULD MISTAKENLY BE TRAVERSED BY A USER WHO IS VISUALLY IMPAIRED.

CONCRETE FLARE LENGTHS ADJACENT TO NON-WALKABLE SURFACES SHOULD BE LESS THAN 8' LONG MEASURED ALONG THE RAMPS FROM THE BACK OF CURB.

- ① 0" CURB HEIGHT. SEE INSET A ON SHEET 3 OF 6.
- ② FULL CURB HEIGHT.
- ③ SIDE TREATMENTS ARE APPLICABLE TO ALL RAMP TYPES AND SHOULD BE IMPLEMENTED AS NEEDED AS FIELD CONDITIONS DICTATE. THE ENGINEER SHALL DETERMINE THE RAMP SIDE TREATMENTS BASED ON MAINTENANCE OF BOTH ROADWAY AND SIDEWALK, ADJACENT PROPERTY CONSIDERATIONS, AND MITIGATING CONSTRUCTION IMPACTS.
- ④ TYPICALLY USED FOR MEDIANS AND ISLANDS.
- ⑤ WHEN NO CONCRETE FLARES ARE PROPOSED, THE CONCRETE WALK SHALL BE FORMED AND CONSTRUCTED PERPENDICULAR TO THE EDGE OF ROADWAY. MAINTAIN 3" MAX. BETWEEN EDGE OF DOMES AND EDGE OF CONCRETE.
- ⑥ IF NO CURB AND GUTTER IS PLACED IN RURAL SECTIONS, DETECTABLE WARNINGS SHALL BE PLACED 1' FROM THE EDGE OF BITUMINOUS ROADWAY AND/OR BITUMINOUS SHARED-USE PATH TO PROVIDE VISUAL CONTRAST.
- ⑦ ALL CONSTRUCTED CURBS MUST HAVE A CONTINUOUS DETECTABLE EDGE FOR THE VISUALLY IMPAIRED. THIS DETECTABLE EDGE REQUIRES DETECTABLE WARNINGS WHEREVER THERE IS ZERO-INCH HIGH CURB. CURB TAPERS ARE CONSIDERED A DETECTABLE EDGE WHEN THE TAPER STARTS WITHIN 3" OF THE EDGE OF THE DETECTABLE WARNINGS. AND UNIFORMLY RISES TO A 3-INCH MINIMUM CURB HEIGHT. ANY CURB NOT PART OF A CURB TAPER AND LESS THAN 3 INCHES IN HEIGHT IS NOT CONSIDERED A DETECTABLE EDGE AND THEREFORE IS NOT COMPLIANT WITH ACCESSIBILITY STANDARDS.
- ⑧ DRILL AND GROUT 1 - NO. 4 12" LONG REINFORCEMENT BAR (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE V CURB.
- ⑨ DRILL AND GROUT 2 - NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) WITH 3" MIN. COVER. REINFORCEMENT BARS ARE NOT NEEDED IF THE APPROACH NOSE IS POURED INTEGRAL WITH THE CURB AND GUTTER.
- ⑩ SIDE TREATMENT EXAMPLES SHOWN ARE WHEN THE INITIAL LANDING IS APPROXIMATELY LEVEL WITH THE FULL HEIGHT CURB (I.E. 6" LONG RAMP FOR 6" HIGH CURB). WHEN THE INITIAL LANDING IS MORE THAN 1" BELOW FULL HEIGHT CURB REFER TO SHEETS 1 & 2 TO MODIFY THE CURB HEIGHT TAPERS AND MAINTAIN POSITIVE BOULEVARD DRAINAGE. CONSTRUCT THESE TAPERS AT 0"-3" AT 8-10%, THEN LESS THAN 5% FROM 3" CURB TO FULL CURB HEIGHT.
- ⑪ NEAREST EDGE OF DETECTABLE WARNING SURFACES SHALL BE PLACED 12' MINIMUM TO 15' MAXIMUM FROM THE NEAREST RAIL. FOR SKEWED RAILWAYS IN NO INSTANCE SHALL THE DETECTABLE WARNING BE CLOSER THAN 12' MEASURED PERPENDICULAR TO THE NEAREST RAIL.
- ⑫ WHEN PEDESTRIAN GATES ARE PROVIDED, DETECTABLE WARNING SURFACES SHALL BE PLACED ON THE SIDE OF THE GATES OPPOSITE THE RAIL, 2' FROM THE APPROACHING SIDE OF THE GATE ARM. THIS CRITERIA GOVERNS OVER NOTE .
- ⑬ CROSSING SURFACE SHALL EXTEND 2' MINIMUM PAST THE OUTSIDE EDGE OF WALK OR SHARED-USE PATH.
- ⑭ 3' FOR MEDIANS AND SPLITTER ISLANDS. NOSE CAN BE REDUCED TO 2' ON FREE RIGHT ISLANDS.
- ⑮ SIDEWALK TO BE PLACED 8.75' MIN. FROM THE FACE OF CURB/PROJECTED FACE OF CURB. THIS ENSURES MIN. CLEARANCE BETWEEN THE SIDEWALK AND GATE ARM COUNTERWEIGHT SUPPORTS.
- ⑯ CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE.

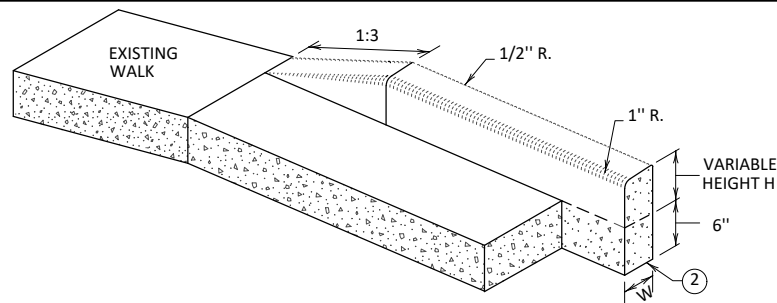
FACE OF CURB/PROJECTED
FACE OF CURB

PLACE DETECTABLE
WARNINGS ENTIRE
WALK/PATH WIDTH

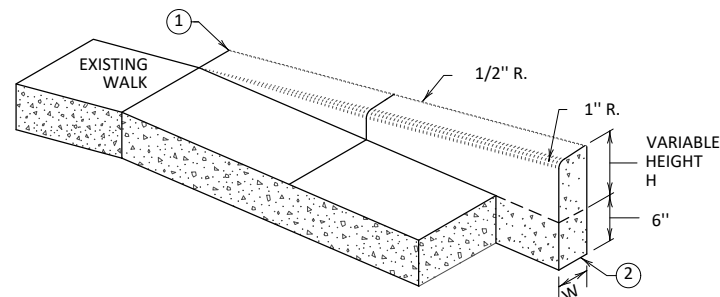


RAILROAD CROSSING
PLAN VIEW

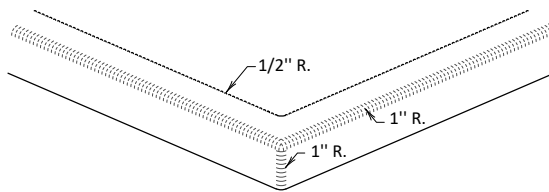
LEAD EXPERT OFFICE	JEFFREY PERKINS OPERATIONS DIVISION	PEDESTRIAN CURB RAMP DETAILS	APPROVED: 11-04-2021 REVISED:	THOMAS STYRBICKI STATE DESIGN ENGINEER	STANDARD PLAN 5-297.250	4 OF 6
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V CURB ADJACENT TO LANDSCAPE
CURB WITHIN SIDEWALK LIMITS

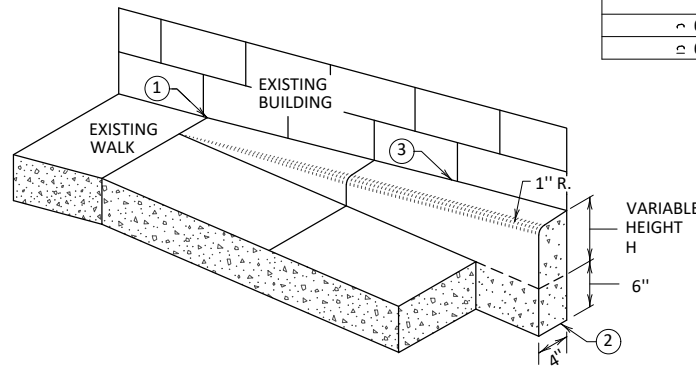


V CURB ADJACENT TO LANDSCAPE
CURB OUTSIDE SIDEWALK LIMITS

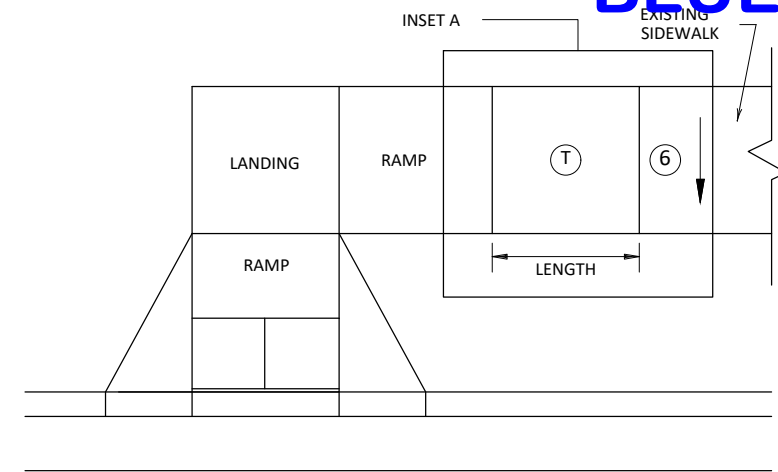


V CURB INTERSECTION

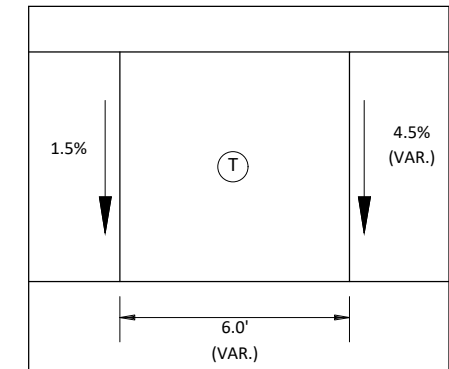
CONCRETE CURB DESIGN V	
CURB HEIGHT H	CURB WIDTH W
~ 6"	4"
≥ 6"	6"



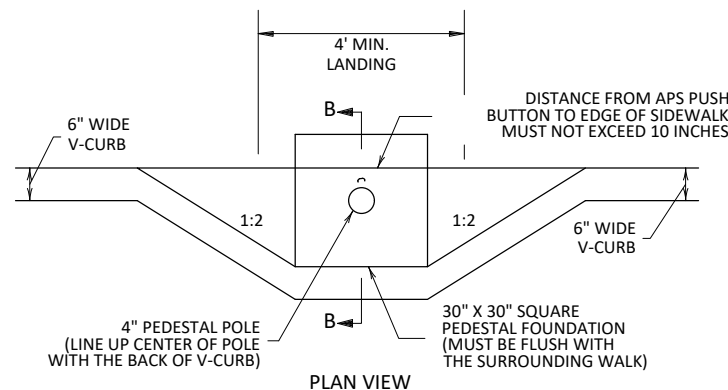
V CURB ADJACENT TO BUILDING
OR BARRIER



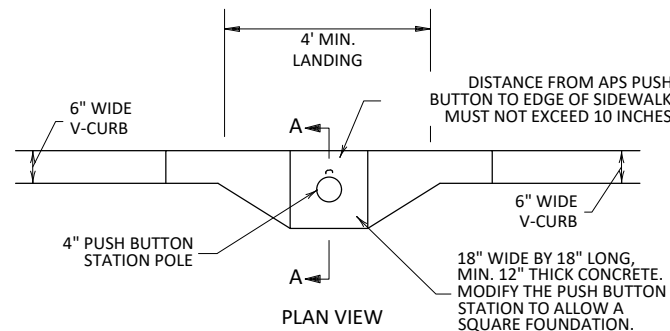
TRANSITION PANEL ④ ⑤



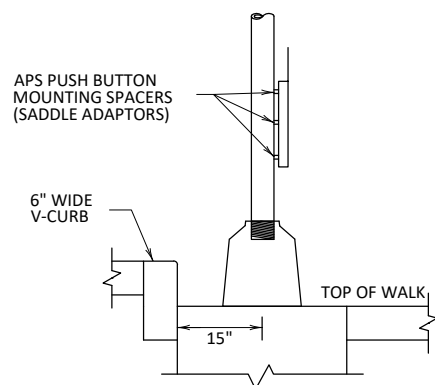
INSET A



PLAN VIEW

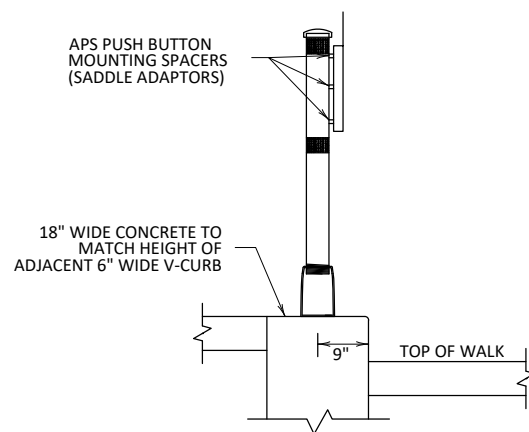


PLAN VIEW



SECTION B-B

SIGNAL PEDESTAL & PUSH BUTTON (V-CURB)



SECTION A-A

PUSH BUTTON STATION (V-CURB)

NOTES:

A WALKABLE FLARE IS AN 8-10% CONCRETE FLARE THAT IS REQUIRED WHEN THE FLARE IS ADJACENT TO A WALKABLE SURFACE, OR WHEN THE PEDESTRIAN PATH OF TRAVEL OF A PUSH BUTTON TRAVERSES THE FLARE.

ALL V CURB CONTRACTION JOINTS SHALL MATCH CONCRETE WALK JOINTS.

WHERE RIGHT-OF-WAY ALLOWS, USE OF V CURB SHOULD BE MINIMIZED. GRADING ADJACENT TURF OR SLOPING ADJACENT PAVEMENT IS PREFERRED.

V CURB SHALL BE PLACED OUTSIDE THE SIDEWALK LIMITS WHEN RIGHT OF WAY ALLOWS.

V CURB NEXT TO BUILDING SHALL BE A 4" WIDTH AND SHALL MATCH PREVIOUS TOP OF SIDEWALK ELEVATIONS.

- ① END TAPERS AT TRANSITION SECTION SHALL MATCH INPLACE SIDEWALK GRADES.
- ② ALL V CURB SHALL MATCH BOTTOM OF ADJACENT WALK.
- ③ CONSTRUCT USING APPROVED EXPANSION MATERIAL PER MNDOT TYPE A-E EXPANSION. LEAVE A MINIMUM 1/2" TOP GAP AND SEAL WITH MNDOT APPROVED SILICONE PER MNDOT SPEC 3722.
- ④ THE MAX. RATE OF CROSS SLOPE TRANSITIONING IS 1' LINEAR FOOT OF SIDEWALK PER HALF PERCENT CROSS SLOPE. WHEN PAR WIDTH IS GREATER THAN 6' OR THE RUNNING SLOPE IS GREATER THAN 5%, DOUBLE THE CALCULATED TRANSITION LENGTH.
- ⑤ TRANSITION PANELS ARE TO ONLY BE USED AFTER THE RAMP, OR IF NEEDED, LANDING ARE AT THE FULL CURB HEIGHT (TYPICAL SECTION).
- ⑥ EXISTING CROSS SLOPE GREATER THAN 2.0%.

LEGEND

THESE LONGITUDINAL SLOPE RANGES SHALL BE THE STARTING POINT. IF SITE CONDITIONS WARRANT, LONGITUDINAL SLOPES UP TO 8.3% OR FLATTER ARE ALLOWED.

- ⑤ INDICATES PEDESTRIAN RAMP - SLOPE SHALL BE BETWEEN 5.0% MINIMUM AND 8.3% MAXIMUM IN THE DIRECTION SHOWN AND THE CROSS SLOPE SHALL NOT EXCEED 2.0%.
- LANDING AREA - 4' X 4' MIN. (5' X 5' MIN. PREFERRED) DIMENSIONS AND MAX 2.0% SLOPE IN ALL DIRECTIONS. LANDING SHALL BE FULL WIDTH OF INCOMING PARS.
- ⑤ TRANSITION PANEL(S) - TO BE USED FOR TRANSITIONING THE CROSS-SLOPE OF A RAMP TO THE EXISTING WALK CROSS-SLOPE. RATE OF TRANSITION SHOULD BE 0.5% PER 1 LINEAR FOOT OF WALK. SEE THIS SHEET FOR ADDITIONAL INFORMATION.

LEAD
EXPERT
OFFICE

JEFFREY PERKINS
OPERATIONS DIVISION

PEDESTRIAN CURB RAMP DETAILS

APPROVED: 11-04-2021
REVISED:

THOMAS STYRBICKI
STATE DESIGN ENGINEER

STANDARD
PLAN
5-297.250

5 OF 6

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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



1501 SOUTH STATE ST, SUITE 100
FAIRMONT, MN 56031
Phone: (507) 238-4738
Email: Fairmont@bolton-menk.com
www.bolton-menk.com



DESIGNED	NO.	ISSUED FOR	DATE
JTO	95%	REV	05/22/2025
DRAWN			
CHECKED			
CLIENT PROJ. NO.			

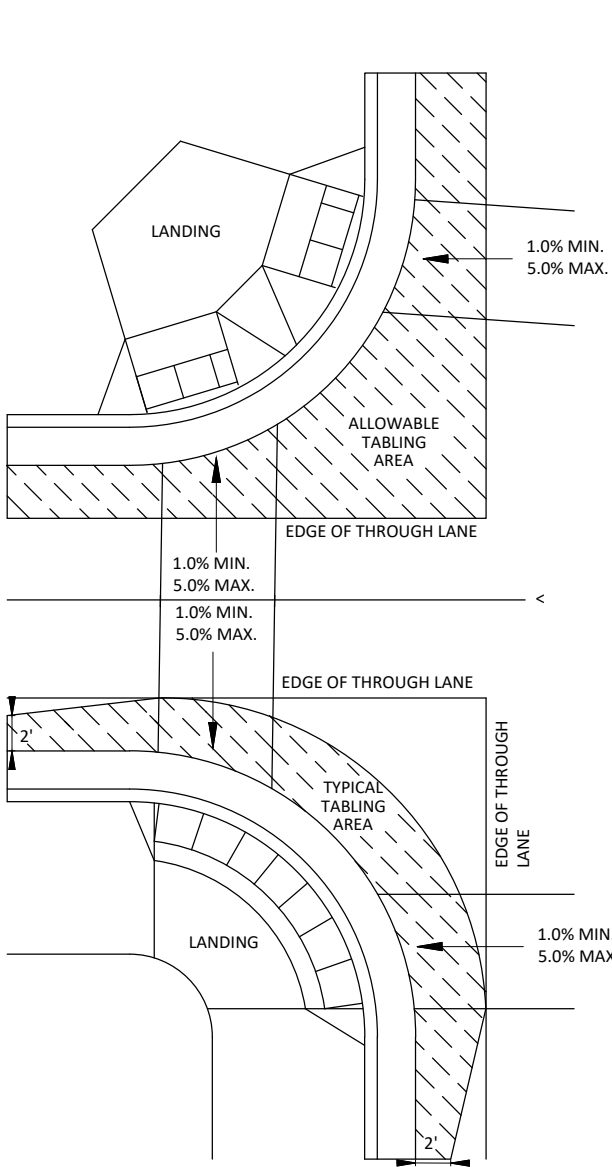
CITY OF WORTHINGTON, MINNESOTA

EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS

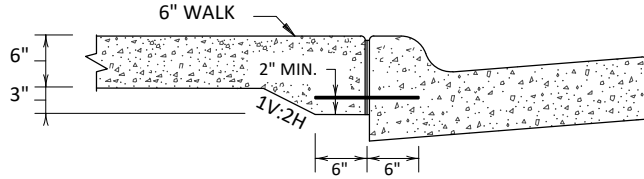
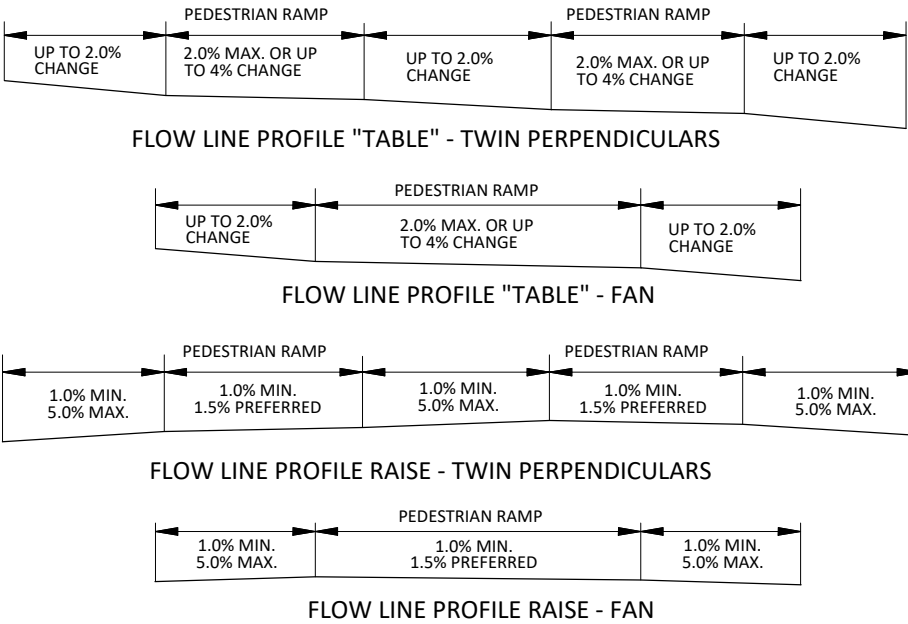
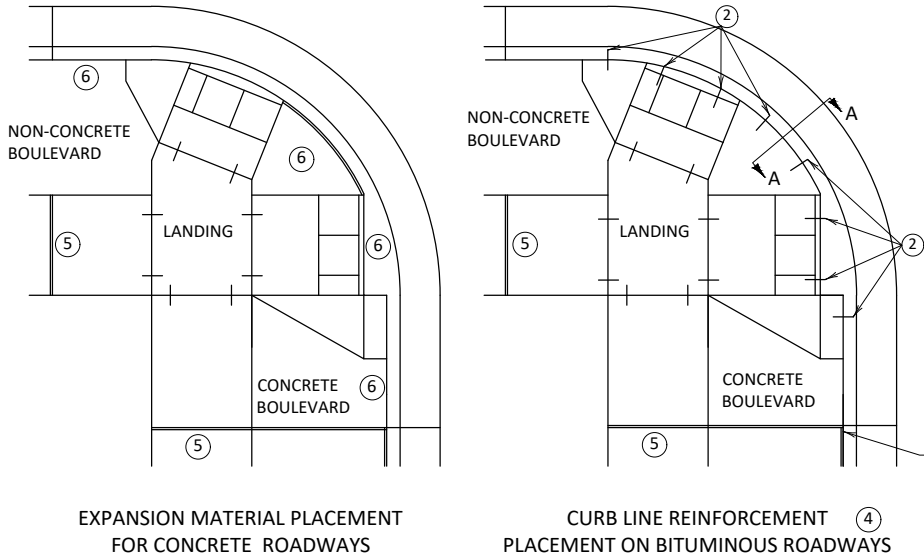
STANDARD PLANS
PEDESTRIAN CURB RAMP DETAILS

SHEET

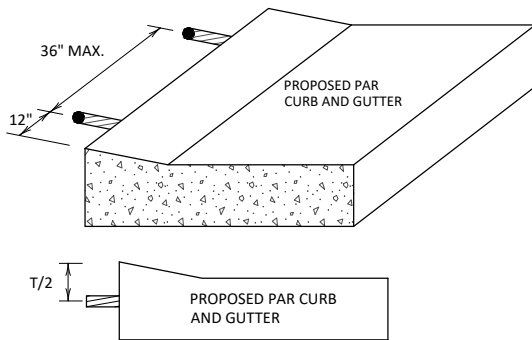
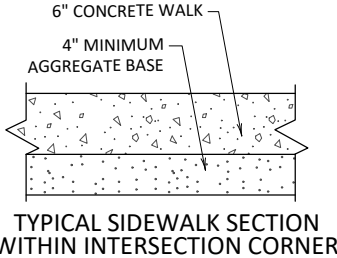
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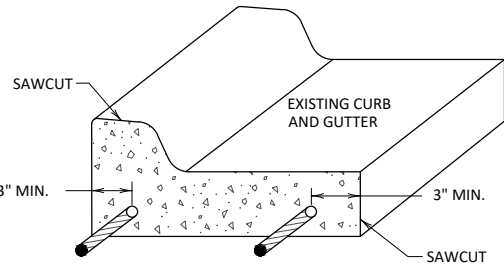
CURB LINE AND ROAD CROSSING ADJUSTMENTS



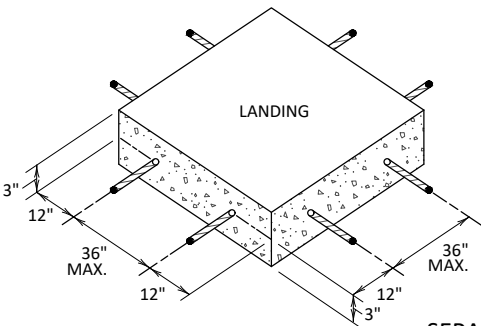
SECTION VIEW A-A
THICKENED SECTION
THROUGH CURB RAMP FLARES



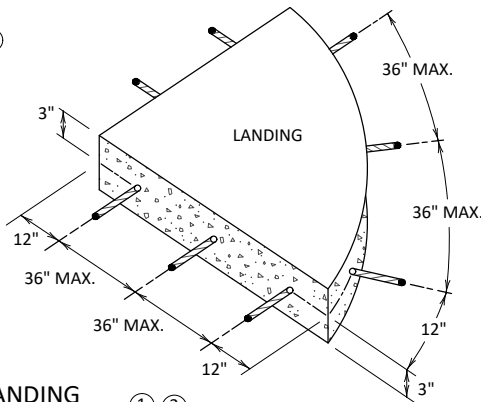
CURB RAMP REINFORCEMENT DETAILS ② ④



CURB AND GUTTER
REINFORCEMENT ③



SEPARATE LANDING
POUR REINFORCEMENT ① ②



GENERAL NOTES:

"TABLING" OF CROSSWALKS MEANS MAINTAINING LESS THAN 2% CROSS SLOPE WITHIN A CROSSWALK, IS REQUIRED WHEN A ROADWAY IS IN A STOP OR YIELD CONDITION AND THE PROJECT SCOPE ALLOWS.

RECONSTRUCTION PROJECTS: ON FULL PAVEMENT REPLACEMENT PROJECTS "TABLING" OF ENTIRE CROSSWALK SHALL OCCUR WHEN FEASIBLE.

MILL & OVERLAY PROJECTS: "TABLING" OF FLOW LINES, IN FRONT OF THE PEDESTRIAN RAMP, IS REQUIRED WHEN THE EXISTING FLOW LINE IS GREATER THAN 2%. WARPING OF THE BITUMINOUS PAVEMENT CAN NOT EXTEND INTO THE THROUGH LANE. TABLE THE FLOW LINE TO 2% OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA;
1) 1.0% MIN. CROSS-SLOPE OF THE ROAD
2) 5.0% MAX. CROSS-SLOPE OF THE ROAD
3) "TABLE" FLOW LINE UP TO 4% CHANGE FROM EXISTING SLOPE IN FRONT OF PEDESTRIAN RAMP
4) UP TO 2% CHANGE IN FLOW LINE FROM EXISTING SLOPE BEYOND THE PEDESTRIAN CURB RAMP

STAND-ALONE ADA RETROFITS: FOLLOW MILL & OVERLAY CRITERIA ABOVE HOWEVER ALL PAVEMENT WARPING IS DONE WITH BITUMINOUS PATCHING ON BITUMINOUS ROADWAYS AND FULL-DEPTH APRON REPLACEMENT ON CONCRETE ROADWAYS.

RAISING OF CURB LINES SHOULD OCCUR IN VERTICALLY CONSTRAINED AREAS. RAISE THE CURB LINES ENOUGH TO ALLOW COMPLIANT RAMPS OR AS MUCH AS POSSIBLE WHILE ADHERING TO THE FOLLOWING CRITERIA;
1) 1.0% MIN. AND 5.0% MAXIMUM CROSS-SLOPE OF THE ROAD
2) 1.0% MIN. FLOW LINE (ON EITHER SIDE OF PEDESTRIAN RAMP) TO MAINTAIN POSITIVE DRAINAGE
3) 5.0% RECOMMENDED MAX. FLOW LINE
4) LONGITUDINAL THROUGH LANE ROADWAY TAPERS SHOULD BE 1" VERTICAL PER 15' HORIZONTAL

NOTES:

- ① TO ENSURE RAMPS AND LANDINGS ARE PROPERLY CONSTRUCTED, ALL INITIAL LANDINGS AT A TOP OF A RAMPED SURFACE (RUNNING SLOPE GREATER THAN 2%) SHALL BE FORMED AND PLACED SEPARATELY IN AN INDEPENDENT CONCRETE POUR. FOLLOW SIDEWALK REINFORCEMENT DETAILS ON THIS SHEET FOR ALL SEPARATELY POURED INITIAL LANDINGS.
- ② DRILL AND GROUT NO. 4 12" LONG REINFORCEMENT BARS (EPOXY COATED) AT 36" MAXIMUM CENTER TO CENTER MINIMUM 12" SPACING FROM CONSTRUCTION JOINTS. BARS TO BE ADJUSTED TO MATCH RAMP GRADE. BARS TO BE PAID BY EACH.
- ③ DRILL AND GROUT 2 - NO. 4 X 12" LONG (6" EMBEDDED) REINFORCEMENT BARS (EPOXY COATED). REINFORCEMENT REQUIRED FOR ALL CONSTRUCTION JOINTS. BARS TO BE PAID BY EACH.
- ④ THIS CURB LINE REINFORCEMENT DETAIL SHALL BE USED ON BITUMINOUS ROADWAYS. FOR CONCRETE ROADWAYS, SEE NOTE 6.
- ⑤ CONSTRUCT WITH EXPANSION MATERIAL PER MNDOT SPECIFICATION 3702 TYPES A-E. EXPANSION MATERIAL SHALL MATCH FULL HEIGHT OF ADJACENT CONCRETE.
- ⑥ USE AN APPROVED TYPE F (1/4 INCH THICK) SEPARATION MATERIAL. SEPARATION MATERIAL SHALL MATCH FULL HEIGHT DIMENSION OF ADJACENT CONCRETE.

LEAD
EXPERT
OFFICE

JEFFREY PERKINS
OPERATIONS DIVISION

PEDESTRIAN CURB RAMP DETAILS

APPROVED: 11-04-2021
REVISED:

THOMAS STYRBICKI
STATE DESIGN ENGINEER

STANDARD
PLAN
5-297.250

6 OF 6

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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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Email: Fairmont@bolton-menk.com
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DRAWN	JVZ	95%	REV	05/22/2025
CHECKED	TLW			
CLIENT PROJ. NO.				

CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STANDARD PLANS
PEDESTRIAN CURB RAMP DETAILS

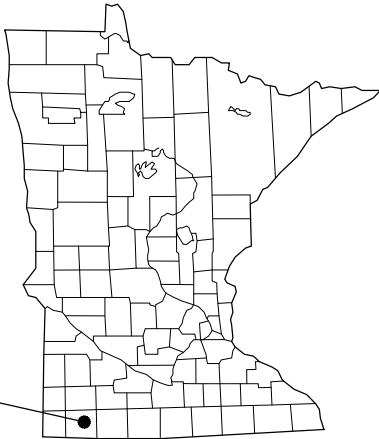
SHEET

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STORMWATER
POLLUTION PREVENTION
PLAN (SWPPP)

EAST FLOWER LANE
STREET AND UTILITY IMPROVEMENTS
CITY OF WORTHINGTON
NOBLES COUNTY, MINNESOTA

PROJECT LOCATION



RESPONSIBLE PARTIES:
The Contractor and Owner will be joint applicants under the MPCA's General Stormwater Permit for Construction Activity as required by the National Pollutant Discharge Elimination System (NPDES) Phase II program.

The Contractor shall provide one or more trained Construction SWPPP Manager(s) knowledgeable and experienced in the application of erosion prevention and sediment control BMPs that will oversee the implementation of the SWPPP, and the installation, inspection and maintenance of the erosion prevention and sediment control BMPs.

A Construction SWPPP Manager must be available for an on-site inspection within 72 hours upon request by the MPCA.

	COMPANY	CONTACT PERSON	PHONE
OWNER:	City of Worthington	Hyunmyeong Goo	507-372-8600
SWPPP DESIGNER:	Bolton & Menk, Inc.	Justin Olson, PE	507-238-4738
CONTRACTOR:	TBD	TBD	TBD
CONSTRUCTION SWPPP MANAGER:	TBD	TBD	TBD
PARTY RESPONSIBLE FOR LONG TERM O&M:	City of Worthington	Hyunmyeong Goo	507-372-8600

The SWPPP Designer, Construction SWPPP Manager, and BMP Installer must have appropriate training. Documentation showing training commensurate with the job duties and responsibilities is required to be included in the SWPPP prior to any work beginning on the site. Training documentation for the SWPPP Designer is included on the Narrative sheet. The Contractor shall attach training documentation to this SWPPP for the Construction SWPPP Manager and BMP Installer prior to the start of construction. This information shall be kept up to date until the project NOT is filed.

ADDITIONAL COMPENSATION
Payment for all work associated with Erosion and Sediment Control shall be as described in the Project Manual. Unless otherwise authorized by the Owner no additional payment shall be made for any work required to administer and maintain the site erosion and sediment control in compliance with the Minnesota Pollution Control Agency (MPCA) - General Stormwater Permit for Construction Activity (MN R100001) including but not limited to inspection, maintenance, and removal of BMPs or addition of BMPs to accommodate Contractor phasing.

DOCUMENT RETENTION
Permittees must make the SWPPP, including all inspection reports, maintenance records, training records and other information required by this permit, available to federal, state, and local officials within three (3) days upon request for the duration of the permit and for three (3) years following the NOT.

GENERAL STORMWATER DISCHARGE REQUIREMENTS
All requirements listed in Section 5.1 of the Permit for the design of the permanent stormwater management system and discharge have been included in the preparation of this SWPPP. These include but are not limited to:

- The expected amount, frequency, intensity, and duration of precipitation.
- The nature of stormwater runoff and run-on at the site
- Peak flow rates and stormwater volumes to minimize erosion at outlets and downstream channel and stream bank erosion.
- The range of soil particle sizes expected to be present on the site.

Permanent stormwater treatment systems for this project have been designed in accordance with the guidance in the MN Stormwater Manual in place at the time of bidding. Copies of the design information and calculations are part of this SWPPP and will be provided in digital format upon written request to the Engineer.

LEGEND

OR

1-MILE BOUNDARY

PROJECT BOUNDARY

IMPAIRED, SPECIAL OR PROTECTED WATERS

NATIONAL WETLANDS INVENTORY

PROJECT AREAS:		
Total Project Size (disturbed area) =	33.0	ACRES
Existing area of impervious surface =	0.4	ACRES
Post construction area of impervious surface =	17.0	ACRES
Total new impervious surface area created =	16.6	ACRES

Planned Construction Start Date:	07/01/2025
Estimated Construction Completion Date:	11/01/2025

PERMANENT STORMWATER MANAGEMENT SYSTEM:
Type of storm water management used if more than 1 acre of new impervious surface is created:

X	Wet Sedimentation Basin
X	Infiltration/Filtration
X	Regional Pond
	Permanent Stormwater Management Not Required

PROJECT LOCATION:					
COUNTY	TOWNSHIP	RANGE	SECTION	LATITUDE	LONGITUDE
NOBLES	T102N	R40W	26	43.6084°	-95.6065°

BMP SUMMARY	QUANTITY	UNIT
Stabilized Construction Exit	1	Lump Sum
Storm Drain Inlet Protection	13	Each
Silt Fence, Type MS	550	Lin Ft
Sediment Control Log Type Straw	180	Lin Ft
Sediment Control Log Type Rock	50	Lin Ft
Rolled Erosion Prevention Category 20	14,160	Sq Yd
Rapid Stabilization Method 3	3	M Gal
Seeding, Wet Ditch Mix	3,000	Sq Yd
Seeding, Southern Boulevard Mix	8,250	Sq Yd
Seeding, Southern Tall Grass Mix	2,910	Sq Yd
Seeding, Temporary	29,320	Sq Yd

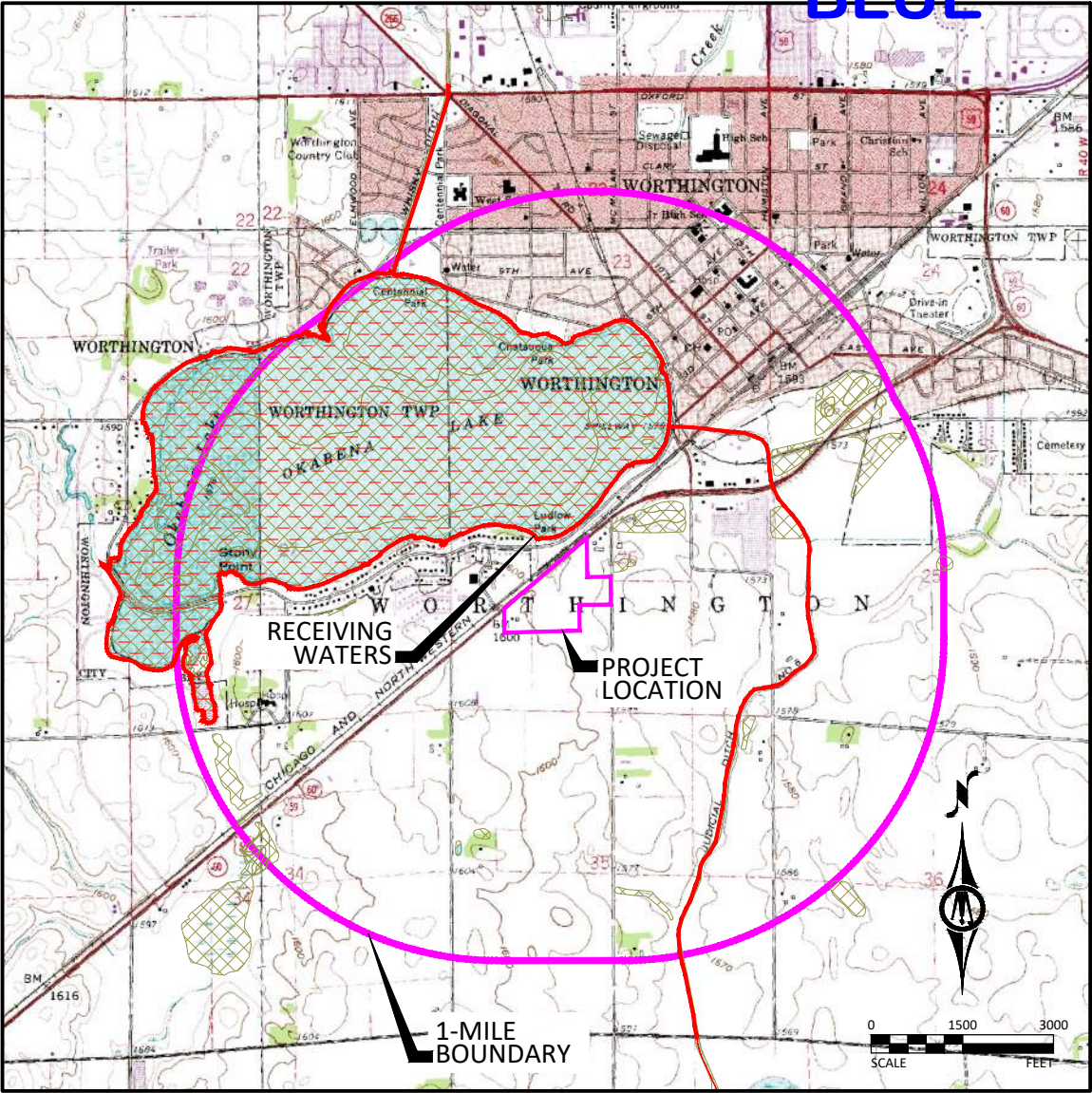
DESCRIPTION OF CONSTRUCTION ACTIVITIES AND STORMWATER MANAGEMENT:
Construction activities include: Site grading, sanitary sewer, watermain, storm sewer, regional stormwater basin construction, temporary erosion and sediment control, street grading & base, street pavement, and turf establishment. This project is the development of existing agricultural land into multi-family housing and commercial space. The regional basin drainage also includes an existing farm site that is included as future redeveloped area.

The majority of stormwater currently surface drains northerly to the south ditch of the existing Flower Lane. After entering the ditch, the stormwater flows through culverts and ditches to Lake Okabena. The remainder of the site drains easterly to the west ditch of Gateway drive. This stormwater continues southeasterly through culverts and overland flow to Judicial Ditch 6. The existing stormwater discharge rates from the project area are 24.5, 45.6, and 79.4 cfs for the 2-, 10-, and 100-year design storms, respectively.

After construction is complete, stormwater will drain over the surface and through new storm sewer infrastructure to permanent stormwater management practices designed to meet local and state regulation. The majority of the site will flow to a regional wet sedimentation basin for treatment and storage. The wet sedimentation basin consists of a forebay for pre-treatment and a pond for permanent storage and water quality volume treatment. The basin is designed to treat 19.9 acres of incoming area flowing to the proposed basin and 13.0 acres of newly constructed impervious area. The regional basin provides 0.95 AC-FT of permanent storage, and has a water quality volume of 1.08 AC-FT. The proposed discharge rates from the site are 15.9, 35.6, and 65.0 cfs for the 2-, 10-, and 100-year design storms, respectively. As the lots of the subdivision are developed, additional permanent water quality treatment practices will be constructed for areas that do not drain to the regional basin.

Temporary sedimentation basins are required for this site for any common drainage locations that serve an area with five or more acres disturbed at one time. Contractor to accommodate requirement in their construction phasing.

Construction BMPs shown on this plan are for the construction of the regional basin and street only. Temporary stabilization of the development lots and future construction BMPs will be completed by the developer.



RECEIVING WATERS:
Receiving waters, including surface water, wetlands, Public Waters, and stormwater ponds, within 1-mile of the project boundary are identified on the USGS 7.5 min quad map above. Receiving waters that are impaired, the impairment, and WLA are listed as follows. All specific BMPs relative to construction activities listed in the permit for special, prohibited, restricted, or impaired have been incorporated into this plan. All specific BMPs listed in approved TMDLs and those BMPs listed for construction related waste load allocations have also been incorporated.

NAME OF WATER BODY	TYPE (ditch, pond, wetland, lake, etc.)	Special, Prohibited, Restricted Water ¹	Flows to Impaired Water Within 1-Mile ²	USEPA Approved Construction Related TMDL ³
Lake Okabena	Lake	No	Yes	Yes
Judicial Ditch 6 (Lake Okabena Outflow)	Ditch	No	Yes	None

¹ Special, prohibited, and restricted waters are listed in Section 23 of the MN Construction Stormwater General Permit (MNR100001).
² Identified as impaired under section 303 (d) of the federal Clean Water Act for phosphorus, turbidity, TSS, dissolved oxygen, and/or aquatic biota.
³ Construction Related TMDLs include those related to: phosphorus, turbidity, TSS, dissolved oxygen, and/or aquatic biota.

IMPLEMENTATION SCHEDULE AND PHASING: The Contractor is required to provide an updated schedule and site management plan meeting the minimum requirements of Section 1717 of the Minnesota Standard Specifications for Construction.

- Submit SWPPP Updates to Engineer. Submittal shall include any requested changes to the SWPPP, including but not limited to: Trained Personnel, Locations for Stockpiles, Concrete Washout, Sanitation Facilities, Types and Locations of Erosion & Sediment Control. Failure to submit updates shall be considered acceptance of the SWPPP as designed with no changes.
- Install perimeter sediment control, inlet protection, and construction exit.
- Construct stormwater basin and perform site grading earthwork.
- Install new utilities and structures.
- Install base and street pavement
- Complete turf establishment on all areas disturbed by construction.
- Add additional temporary BMPs as necessary during construction based on inspection reports.
- Development of the lots within the subdivision will be phased and may be completed over the course of several years. All areas within the subdivision will be stabilized pending development.
- Ensure final stabilization measures are complete.
- Provide digital copy of all Field SWPPP Documentation including Inspection Reports and SWPPP Revisions to the Owner.
- Submit Notice of Termination (NOT) to MPCA. NOTE: The NOT must be submitted to MPCA before Final Stabilization is considered complete.

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.		
TRAVIS L. WINTER		
LIC. NO. 46649	DATE	MAY 27, 2025



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CHECKED	TLW			
CLIENT PROJ. NO.				

CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STORMWATER POLLUTION PREVENTION PLAN
PROJECT INFORMATION & LOCATION MAP

SHEET
C2.01

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Information contained in this SWPPP narrative sheet summarizes requirements of the GENERAL PERMIT AUTHORIZATION TO DISCHARGE STORMWATER ASSOCIATED WITH CONSTRUCTION ACTIVITY UNDER THE NATIONAL POLLUTANT DISCHARGE ELIMINATION SYSTEM/STATE DISPOSAL SYSTEM PROGRAM - Permit No: MN R100001 (Permit) as they apply to this project. All provisions of the Permit including those not specifically cited herein shall apply to this project. The Contractor is responsible to be familiar with and comply with all conditions of the permit. The full text of the Permit is available at: <https://www.pca.state.mn.us/sites/default/files/wq-strm2-80a.pdf>

SWPPP AMENDMENTS AND SUBMITTALS

Contractor must prepare and submit to the Engineer a SWPPP amendment as necessary to include additional Best Management Practices (BMPs) to correct problems identified or address the following situations.

1. Contact information and training documentation for Construction SWPPP Manager and BMP Installer,

2. There is a change in construction method of phasing, operation, maintenance, weather or seasonal conditions not anticipated during the design of the SWPPP including but not limited to:

a. Types and/or Locations of BMPs

b. Material Storage and Spill Response

c. Fueling Plans

d. Locations for Stockpiles, Concrete Washout, and Sanitation Facilities and

e. Project Phasing

3. It is determined that the SWPPP is not achieving objectives of minimizing pollutants in stormwater discharges associated with construction activity, or

4. The SWPPP is not consistent with the terms and conditions of the permit.

The Contractor may implement SWPPP amendments immediately and is not required to wait for Engineer review of the submittal. The responsibility for completeness of SWPPP amendments and compliance with the Permit lies with the Contractor. Review, comment, or lack of comment by the Engineer on a SWPPP amendment shall not absolve the responsibilities of the Contractor in any way.

If a change order is issued for a design change the SWPPP amendment will be prepared by the Engineer and included in the change order.

In addition to SWPPP amendments, the Contractor shall submit to the Engineer Weekly Erosion and Sediment Control Schedule meeting the requirements of MnDOT 1717.

The Contractor shall keep copies of all SWPPP amendments, Weekly Erosion and Sediment Control Schedules, inspection logs, and maintenance logs with the field copy of the SWPPP. A PDF copy of these documents will be provided along with a copy of the final Field Copy of the SWPPP to the Engineer along with the signed Notice of Termination when final stabilization is complete.

EROSION PREVENTION PRACTICES

Stormwater conveyance channels shall be routed around unstabilized areas. Erosion controls and velocity dissipation devices shall be used at outlets within and along the length of any constructed conveyance channel.

The normal wetted perimeter of all ditches or swales, including storm water management pond slopes, that drain waters from the site must be stabilized within 200' of any property edge or discharge point, including storm sewer inlets, within 24 hours of connection.

Temporary or permanent ditches or swales used as sediment containment during construction do not need to be stabilized during temporary period of use and shall be stabilized within 24 hours after no longer used as sediment containment.

Mulch, hydromulch, tackifier, or similar practice shall not be used in any portion of the wetted perimeter of a temporary or permanent drainage ditch or swale section with a continuous slope of greater than 2 percent.

Energy dissipation shall be installed at all temporary or permanent pipe outlets within 24 hours of connection to a surface water or permanent stormwater treatment system.

The Contractor shall phase construction and use construction methods to the extent practical to minimize exposed soils. The project phasing shall be documented in the Weekly Erosion and Sediment Control Schedule.

SEDIMENT CONTROL PRACTICES

Down gradient BMPs including perimeter BMPs must be in place before up gradient land- disturbing activities begin and shall remain in place until final stabilization.

All BMPs that have been adjusted or removed to accommodate short-term activities shall be re-installed or replaced the earlier of the end of the work day or before the next precipitation event even if the activity is not complete.

Inlet BMPs may be removed for specific safety concerns. The BMPs shall be replaced as soon as the safety concern is resolved. The removal shall be documented in the SWPPP as a SWPPP amendment.

Temporary stockpiles must have sediment control BMPs. The Contractor shall prepare and submit to the Engineer a SWPPP amendment showing the location of temporary stockpiles and the BMPs for each stockpile. The SWPPP amendment must meet the minimum requirements of Section 9 of the Permit.

Soil compaction shall be minimized and topsoil shall be preserved, unless infeasible or if construction activities dictate soil compaction or topsoil stripping.

The use of polymers, flocculants, or other sedimentation treatment chemicals are not proposed as part of this SWPPP as designed by the Engineer. If methods or phasing of construction require the use of any of these chemicals, the Contractor shall prepare and submit to the Engineer a SWPPP amendment that meets the minimum requirements of Section 9 of the Permit.

TEMPORARY SEDIMENTATION BASINS

A temporary sedimentation basin has not been included in this SWPPP as designed by the Engineer. If a basin is later determined to be desirable or necessary the Contractor shall prepare and submit to the Engineer a SWPPP amendment. Temporary sedimentation basins shall meet or exceed the minimum requirements of Section 14 of the Permit and shall include a basin draining plan meeting or exceeding the minimum requirements of Section 10 of the Permit. Where the site discharges to Special and/or Impaired Waters the SWPPP amendment shall also meet or exceed the minimum requirements of Section 23 of the permit.

DEWATERING

A dewatering plan has not been included in this SWPPP as designed by the Engineer. If dewatering is required for this project, the Contractor shall prepare and submit to the Engineer a SWPPP amendment. All dewatering shall meet or exceed the minimum requirements of Section 10 of the Permit.

POLLUTION PREVENTION

Products and materials that have the potential to leach pollutants that are stored on the site must be stored in a manner designed to minimize contact with stormwater. Materials that are not a source of potential contamination to stormwater or that are designed for exposure to stormwater are not required to be covered.

Hazardous materials including but not limited to pesticides, fertilizer, petroleum products, curing compounds and toxic waste must be properly stored and protected from stormwater exposure as recommended by the manufacturer in an access restricted area.

Solid waste must be stored, collected and disposed of in compliance with Minnesota Administrative Rules Chapter 7035.

Portable toilets must be positioned so that they are secure and will not be tipped or knocked over. Sanitary waste must be disposed of properly in accordance with Minn. R. CH 7041.

Exterior vehicle or equipment washing on the project site shall be limited to a defined area of the site. No engine degreasing is allowed on site. A sign must be installed adjacent to each washout facility that requires site personnel to utilize the proper facilities for disposal of concrete and other washout wastes.

The Contractor shall prepare and submit a SWPPP amendment detailing the location and BMPs proposed for storage of materials, solid waste, portable toilets, and exterior vehicle or equipment washing on the site. The SWPPP amendment shall include a spill prevention and response plan that is appropriate for the materials proposed to be on the site. The SWPPP amendment shall meet or exceed the minimum requirements of Section 12 of the Permit.

INSPECTION & MAINTENANCE

A trained person shall routinely inspect the entire construction site at the time interval indicated on this sheet of the SWPPP during active construction and within 24-hours after a rainfall event greater than 0.5 inches in 24 hours. Following an inspection that occurs within 24-hours after a rainfall event, the next inspection must be conducted at the time interval indicated in the Receiving Waters Table found on the SITE PLAN AND INFORMATION SHEET of the SWPPP.

All inspections and maintenance conducted during construction must be recorded on the day it is completed and must be retained with the SWPPP. Inspection report forms are available in the Project Specifications. Inspection report forms other than those provided shall be approved by the engineer.

The Contractor may request a change in inspection schedule for the following conditions:

- a. Inspections of areas with permanent cover to be reduced to once per month,

b. Inspections of areas that have permanent cover and have had no construction activity for 12 months to be suspended until construction resumes,

c. Inspections of areas where construction is suspended due to frozen ground conditions, inspections to be suspended until the earlier of within 24 hours of runoff occurring, or upon resuming construction.

No change in inspection schedule shall occur until authorized by the Engineer.

Inspections must include:

1. All erosion prevention and sediment control BMPs and Pollution Prevention Management Measures to ensure integrity and effectiveness.

2. Surface waters, including drainage ditches and conveyance systems for evidence of erosion and sediment deposition.

3. Construction site vehicle exit locations, streets and curb and gutter systems within and adjacent to the project for sedimentation from erosion or tracked sediment from vehicles.

4. Infiltration areas to ensure that no sediment from ongoing construction activity is reaching the infiltration area and that equipment is not being driven across the infiltration area.

All non-functioning BMPs and those BMPs where sediment reaches one-half (1/2) of the depth of the BMP, or in the case of sediment basins one-half (1/2) of the storage volume, must be repaired, replaced, or supplemented by the end of the next business day after discovery, or as soon as field conditions allow.

Permittees must repair, replace or supplement all nonfunctional BMPs with functional BMPs by the end of the next business day after discovery, or as soon as field conditions allow.

Any sediment that escapes the site must be removed and the area stabilized within 7 calendar days of discovery unless precluded by legal, regulatory, or physical access in which case the work shall be completed within 7 calendar days of authorization. Paved surfaces such as streets shall have any escaped or tracked sediment removed by the end of the day that it is discovered. Sediment release, other than paved surfaces that can be cleaned up with street sweeping shall be reported immediately upon discovery to the Engineer.

PUBLIC WATER RESTRICTIONS:

For public waters that have been promulgated "work in water restrictions" during fish spawning time frames, all exposed soil areas that are within 200 feet of the water's edge, and drain to these waters must complete stabilization within 24-hours during the time period. MN DNR permits are not valid for work in waters that are designated as infested waters unless accompanied by an Infested Waters Permit or written notification has been obtained from MN DNR stating that such permit is not required. There is no exception for pre-existing permits. If a MN DNR Permit has been issued for the project and the water is later designated as infested, the Contractor shall halt all work covered by the MN DNR Permit until an Infested Waters Permit is obtained or that written notification is obtained stating that such permit is not required.

FINAL STABILIZATION

Final Stabilization is not complete until all the following requirements have been met:

1. Substantial Completion has been reached and no ground disturbing activities are anticipated.

2. Permanent cover has been installed with an established minimum uniform perennial vegetation density of 70 percent of its expected final growth. Vegetation is not required in areas where no vegetation is proposed by this project such as impervious surfaces or the base of a sand filter.

3. Accumulated sediment has been removed from all permanent stormwater treatment systems as necessary to ensure the system is operating as designed.

4. All sediment has been removed from conveyance systems

5. All temporary synthetic erosion prevention and sediment control BMPs have been removed. BMPs designated on the SWPPP to remain to decompose on-site may remain.

6. For residential construction only, permit coverage terminates on individual lots if the structures are finished and temporary erosion prevention and downgradient perimeter control is complete, the residence sells to the homeowner, and the permittee distributes the MPCA's "Homeowner Fact Sheet" to the homeowner.

7. For agricultural land only (e.g., pipelines across cropland), the disturbed land must be returned to its preconstruction agricultural use prior to submitting the NOT.

SITE STABILIZATION COMPLETION:

Stabilization of exposed soils shall begin immediately and shall be completed after the construction activity has temporarily or permanently ceased no later than:	7 calendar days
--	-----------------

SITE INSPECTION INTERVAL:

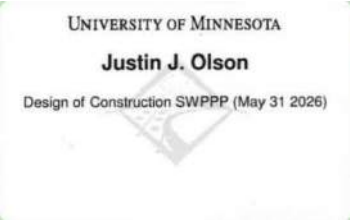
A trained person shall routinely inspect the entire construction site during active construction at an interval of no more than:	7 calendar days
--	-----------------

SPECIAL ENVIRONMENTAL CONSIDERATIONS AND PERMITS:

1)	Was an environmental review required for this project or any part of a common plan of development or sale that includes all or any portion of this project?	NO
2)	Does any portion of the site have the potential to affect threatened or endangered species or their critical habitat?	NO
3)	Does any portion of this site discharge to a Calcareous fen.	NO
4)	Will any portion of the site potentially affect properties listed on the National Register of Historic Places or a known or discovered archeological site?	NO
5)	Have any Karst features have been identified in the project vicinity?	NO
6)	Is compliance with temporary or permanent stormwater management design requirements infeasible for this project?	NO
7)	Has the MN DNR promulgated "work in water restrictions" for any Public Water this site discharges to during fish spawning?	NO

TYPE OF PERMIT	PERMITTING AGENCY	PERMIT STATUS AND CONDITIONS
Construction Stormwater NPDES	MPCA	To be submitted following project award by City

SWPPP DESIGNER TRAINING DOCUMENTATION:



1501 SOUTH STATE ST, SUITE 100
FAIRMONT, MN 56031
Phone: (507) 238-4738
Email: Fairmont@bolton-menk.com
www.bolton-menk.com

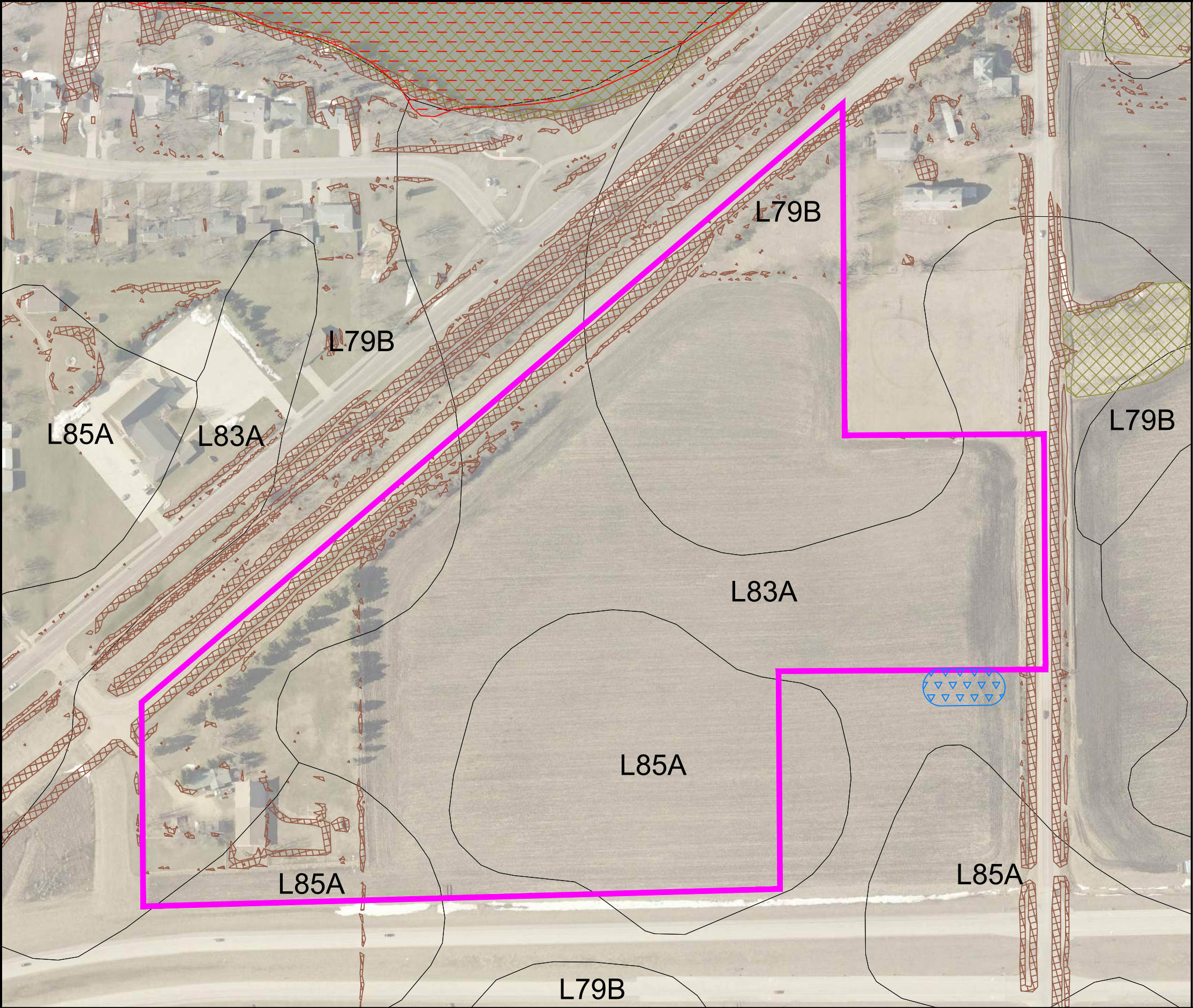


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DRAWN	JVZ	95% REV	05/22/2025	
CHECKED	TLW			
CLIENT PROJ. NO.				

CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STORMWATER POLLUTION PREVENTION PLAN
NARRATIVE

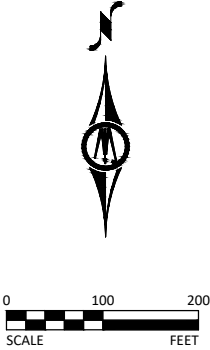
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LEGEND

- PROJECT BOUNDARY
- SOIL TYPE
- IMPAIRED, SPECIAL OR PROTECTED WATERS
- NATIONAL WETLANDS INVENTORY
- STEEP SLOPES (>33.3%)
- EXISTING STORMWATER POND



SOIL TYPE SUMMARY			
Map Unit Symbol	Soil Name	Hyd. Soil Group	Erodibility
L79B	Clarion loam, 2 to 6 percent slopes	C	NHEL
L83A	Webster clay Loam, 0 to 2 percent slopes	D	NHEL
L85A	Nicollet clay loam, 1 to 3 percent slopes	D	NHEL

NHEL - Not Highly Erodible Land
PHEL - Potentially Highly Erodible Land
HEL - Highly Erodible Land

LOCATION OF SWPPP REQUIREMENTS IN PROJECT PLAN	
DESCRIPTION	SHEET NO.
SITE MAP	C2.01
DIRECTION OF FLOW	C2.06 - C2.07
FINAL STABILIZATION	C2.06 - C2.07
SOILS	C2.03
DRAINAGE STRUCTURES	C1.05 - C1.06
DRAINAGE TABULATION	C1.01
STORM SEWER PLAN & PROFILE SHEETS	C5.01 - C5.03
EROSION & SEDIMENT CONTROL DETAILS	C2.04 - C2.05
EROSION CONTROL TABULATION	C2.01
TURF ESTABLISHMENT TABULATION	C2.01
NARRATIVE & NOTES	C2.01 - C2.02

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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025

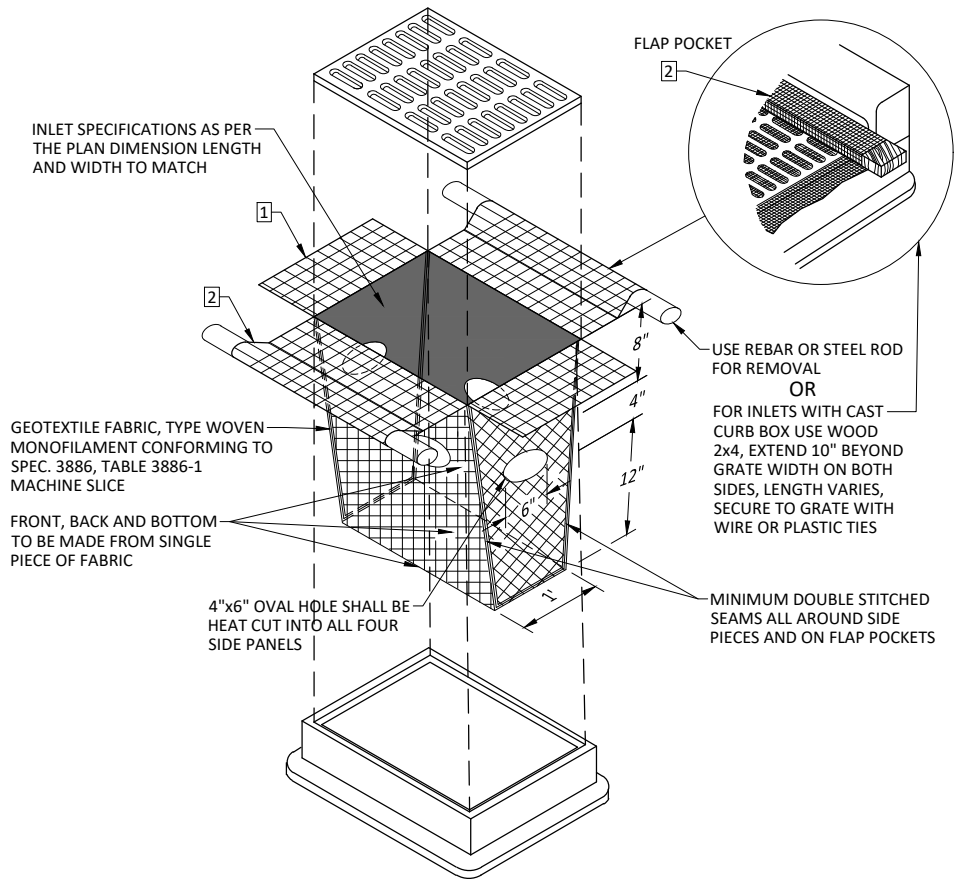


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CITY OF WORTHINGTON, MINNESOTA	SHEET C2.03
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS	
STORMWATER POLLUTION PREVENTION PLAN	
SITE & SOILS MAP	



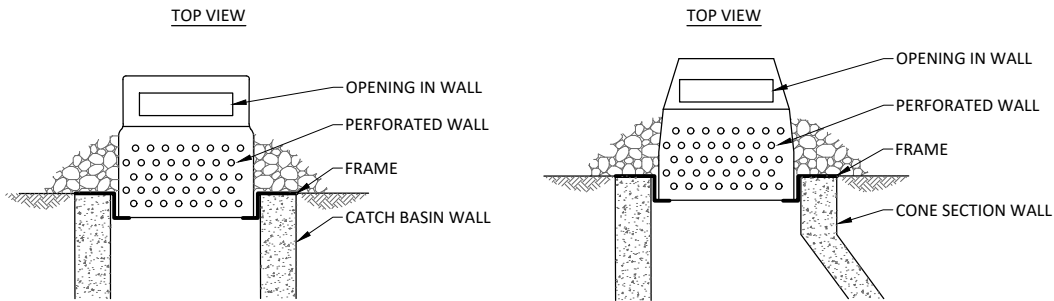
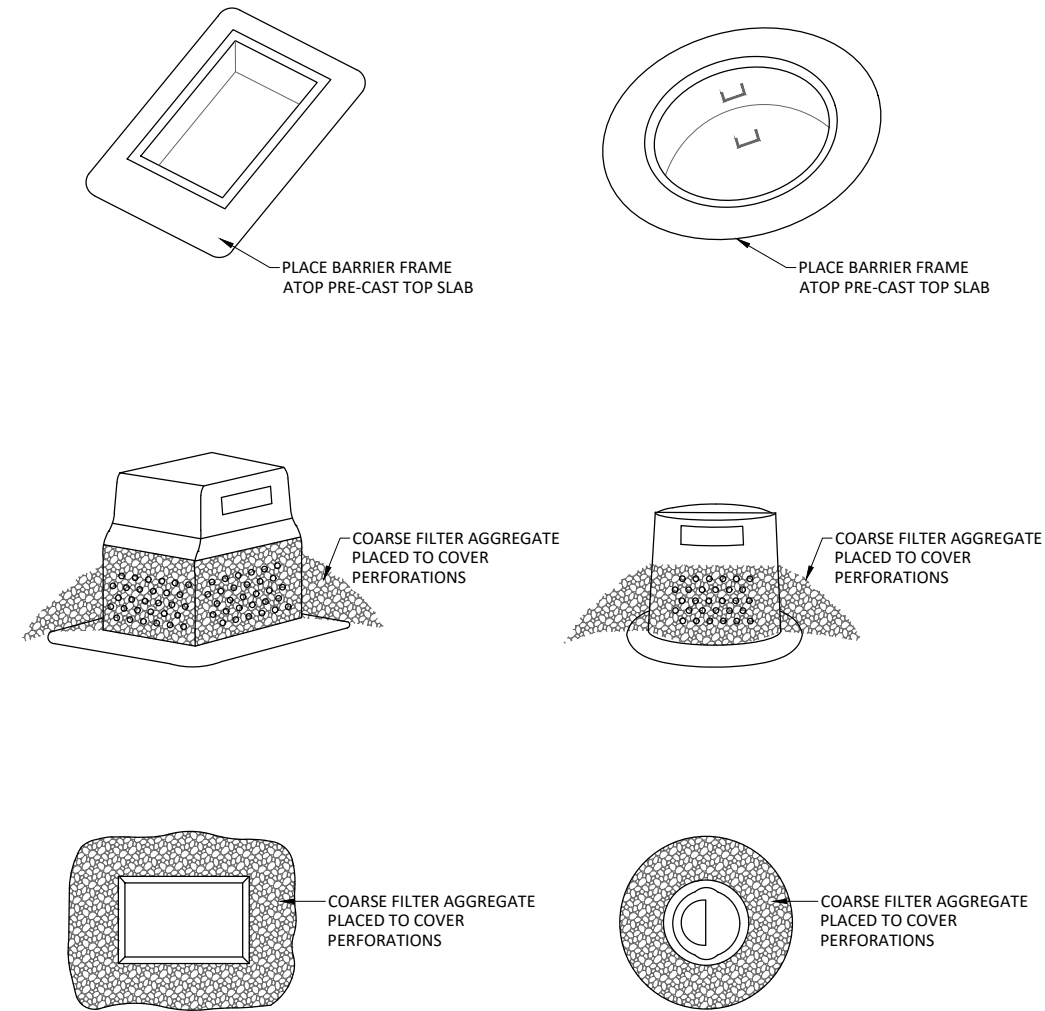
- NOTES:
- 1 INLET PROTECTION DEVICES SHALL BE MAINTAINED OR REPLACED AT THE DIRECTION OF THE ENGINEER. MANUFACTURED ALTERNATIVES APPROVED AND LISTED ON THE DEPARTMENTS EROSION CONTROL PRODUCT ACCEPTABILITY LIST MAY BE SUBSTITUTED. WHEN REMOVING OR MAINTAINING INLET PROTECTION, CARE SHALL BE TAKEN SO THAT THE SEDIMENT TRAPPED ON THE GEOTEXTILE FABRIC DOES NOT FALL IN THE INLET. ANY MATERIAL FALLING INTO THE INLET SHALL BE REMOVED IMMEDIATELY.
 - 2 FINISHED SIZE, INCLUDING POCKETS WHERE REQUIRED, SHALL EXTEND A MINIMUM OF 10" AROUND THE PERIMETER TO FACILITATE MAINTENANCE OR REMOVAL.

FLAP POCKETS SHALL BE LARGE ENOUGH TO ACCEPT WOOD 2x4.

INSTALLATION NOTES:
DO NOT INSTALL PROTECTION IN INLETS SHALLOWER THAN 30", MEASURED FROM THE BOTTOM OF THE INLET TO THE TOP OF THE GRATE.

TRIM EXCESS FABRIC IN THE FLOW LINE TO WITHIN 3" OF THE GRATE.

THE INSTALLED BAG SHALL HAVE A MINIMUM SIDE CLEARANCE, BETWEEN THE INLET AND THE BAG, MEASURED AT THE BOTTOM OF THE OVERFLOW HOLES, OF 3". WHERE NECESSARY THE CONTRACTOR SHALL CLINCH THE BAG, USING PLASTIC ZIP TIES, TO ACHIEVE THE 3" CLEARANCE. THE TIES SHALL BE PLACED AT A MAXIMUM OF 4" FROM THE BOTTOM OF THE BAG.



**INLET PROTECTION
GEOTEXTILE BAG**
NOT TO SCALE

LAST REVISION:
04-2021

PLATE NO.
3-104



**INLET PROTECTION
PERFORATED WALL**
NOT TO SCALE

LAST REVISION:
04-2021

PLATE NO.
3-107

I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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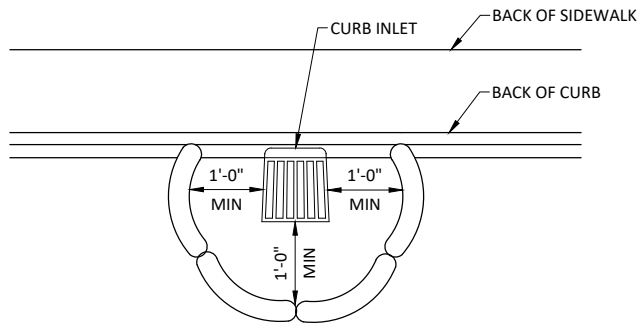


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CLIENT PROJ. NO.			

CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STORMWATER POLLUTION PREVENTION PLAN
EROSION CONTROL DETAILS

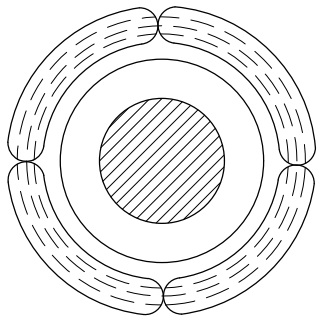
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NOTE:
THIS INLET PROTECTION IS USED DURING
ROUGH GRADING ONLY, USE BEFORE ROAD
IS OPEN TO TRAFFIC OR IS PAVED

PAYMENT SHALL INCLUDE ALL MATERIALS,
FILLING OF LOG, PLACEMENT,
MAINTENANCE & REMOVAL;
80% OF BID PRICE SHALL BE PAID UPON
PROPER PLACEMENT WITH THE FINAL 20%
PAID UPON REMOVAL

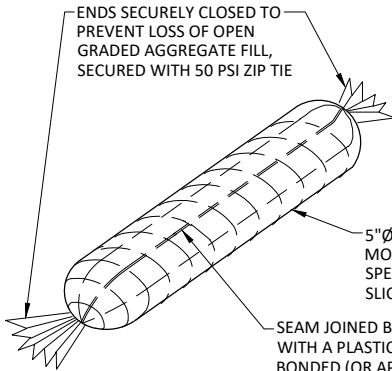


INLET PROTECTION
WITH ROCK LOG

FILL ROCK LOG WITH 45 LBS. OF OPEN
GRADED AGGREGATE CONSISTING OF
SOUND, DURABLE PARTICLES OF CRUSHED
QUARRY ROCK OR GRAVEL CONFORMING
TO THE FOLLOWING GRADATION.

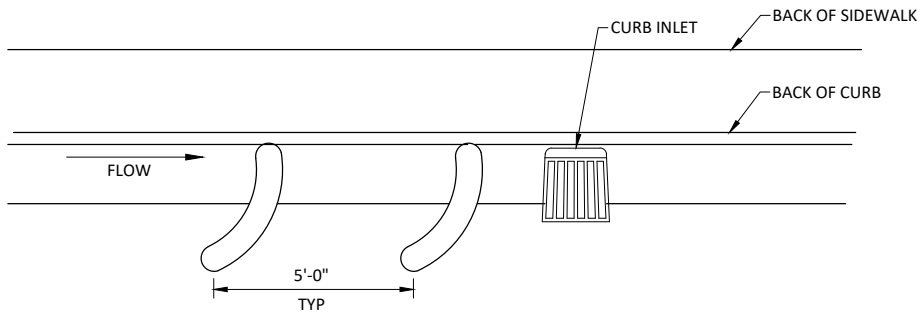
GRADATION	
SIEVE SIZE	PERCENT PASSING
1 1/2"	100
1"	95-100
3/4"	65-95
3/8"	30-65
NO 4	10-35
NO 10	3-20
NO 40	0-8
NO 200	0-3

NOTE:
CRUSHED CONCRETE OR BITUMINOUS
SHALL NOT BE USED FOR OPEN
GRADED AGGREGATE.



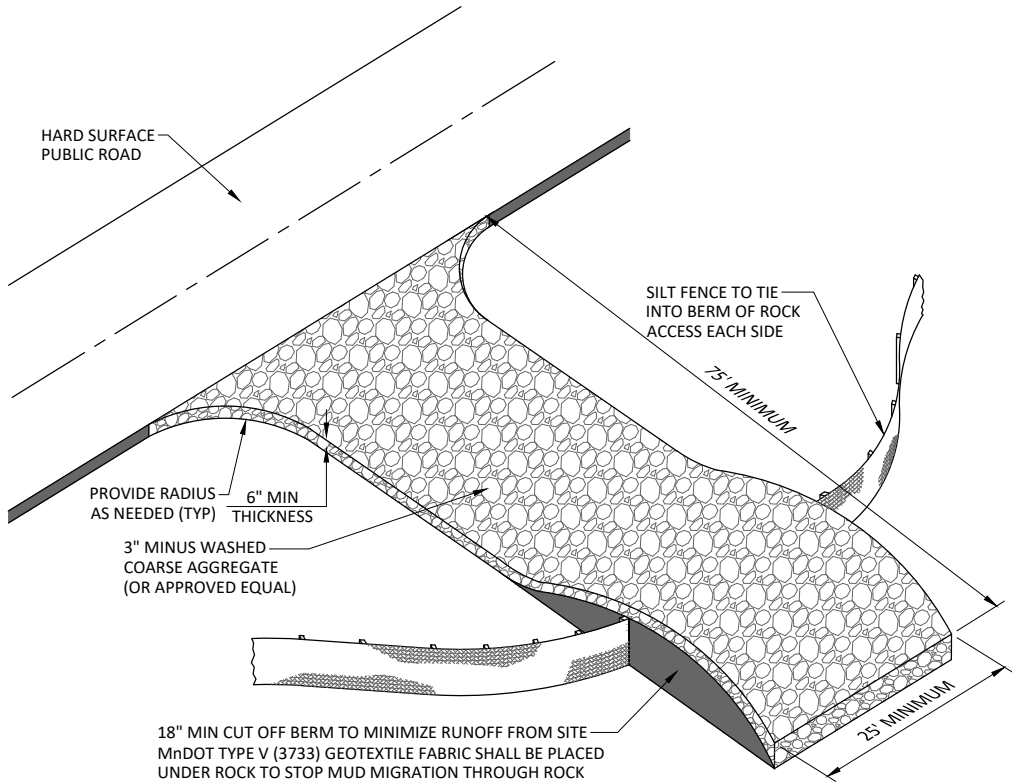
SEAM JOINED BY TWO ROWS OF STITCHING
WITH A PLASTIC MESH BACKING OR HEAT
BONDED (OR APPROVED EQUAL)

INLET PROTECTION
ROCK BAG
NOT TO SCALE

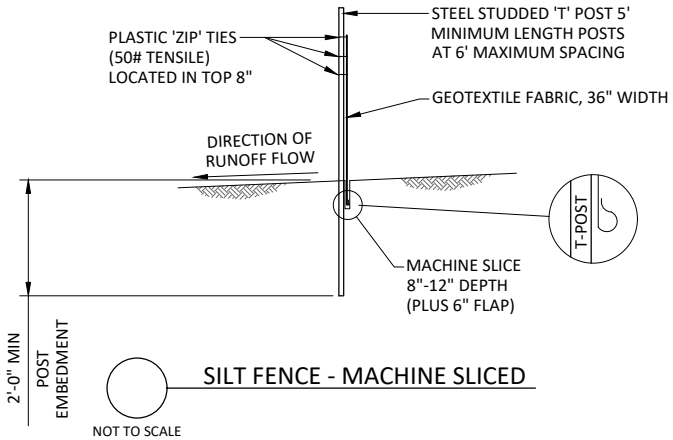


TYPICAL PLACEMENT ALONG CURB

SEDIMENT CONTROL LOG
ROCK BAG
NOT TO SCALE



STABILIZED CONSTRUCTION EXIT
NOT TO SCALE



SILT FENCE - MACHINE SLICED
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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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Email: Fairmont@bolton-menk.com
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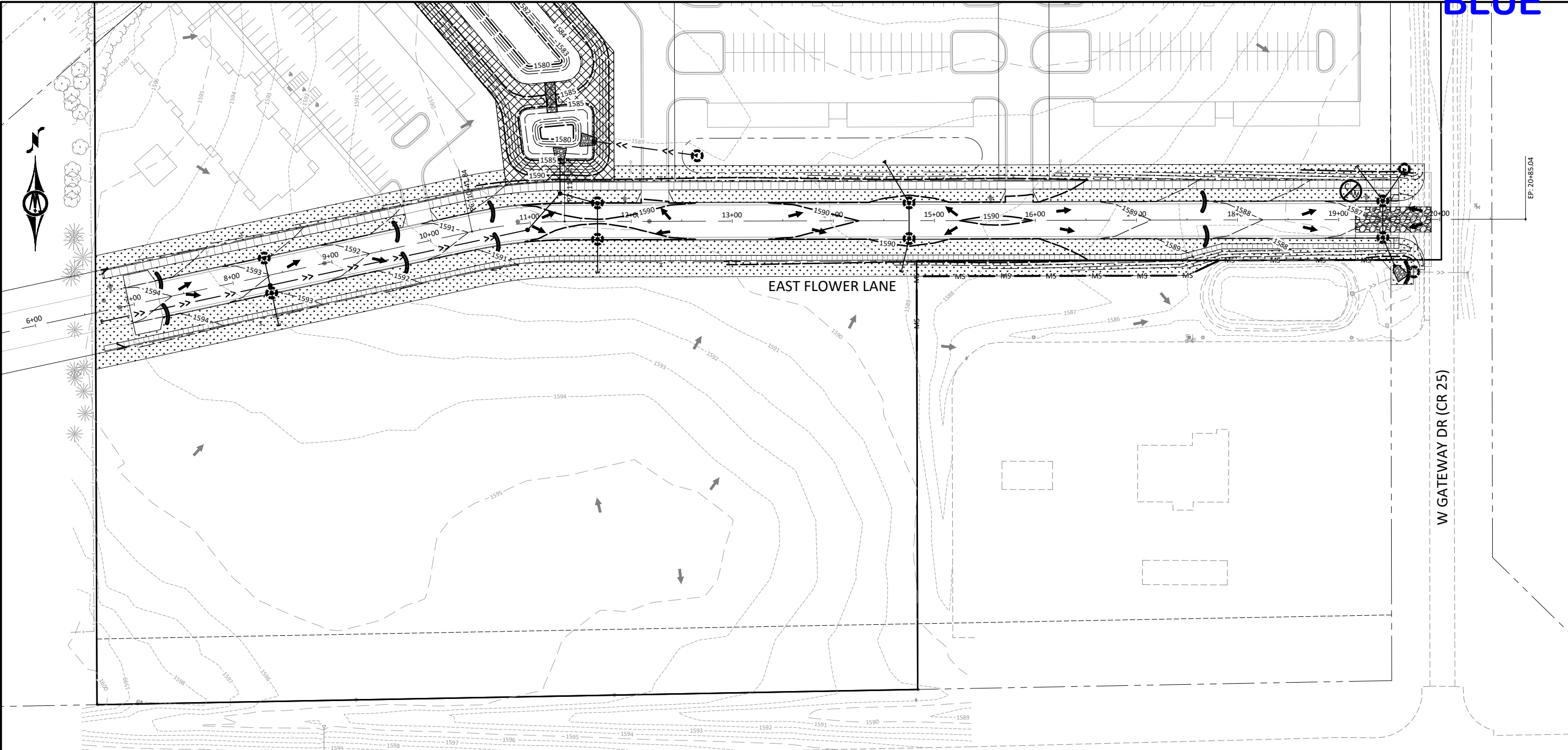


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CLIENT PROJ. NO.				

CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STORMWATER POLLUTION PREVENTION PLAN
EROSION CONTROL DETAILS

SHEET

C2.05



EROSION CONTROL LEGEND

MS	MACHINE SLICED SILT FENCE		WET DITCH MIX WITH CAT 20 ROLLED EROSION PREVENTION PRODUCT
	RIP RAP		SOUTHERN BOULEVARD SEED WITH CAT 20 ROLLED EROSION PREVENTION PRODUCT
	INLET PROTECTION		SOUTHERN TALLGRASS MIX WITH CAT 20 ROLLED EROSION PREVENTION PRODUCT
	DITCH CHECK - BIOLOG		STABILIZED CONSTRUCTION EXIT
	DITCH CHECK - ROCK		
	EXISTING/PROPOSED DRAINAGE FLOW		
	CONCRETE WASHOUT		

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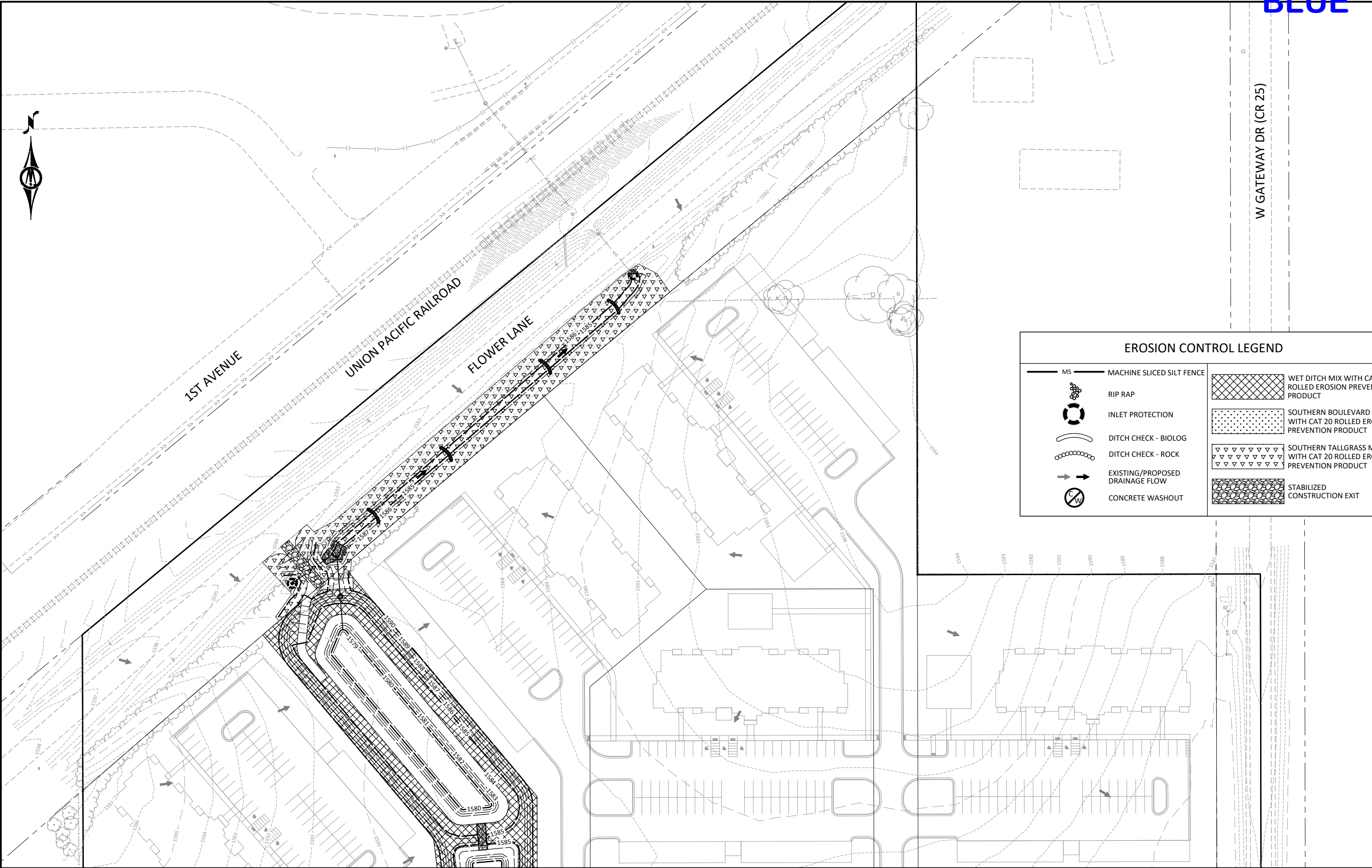


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CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STORMWATER POLLUTION PREVENTION PLAN
SOUTH AREA

SHEET

C2.06



EROSION CONTROL LEGEND

MS

MACHINE SLICED SILT FENCE

RIP RAP

INLET PROTECTION

DITCH CHECK - BIOLOG

DITCH CHECK - ROCK

EXISTING/PROPOSED DRAINAGE FLOW

CONCRETE WASHOUT

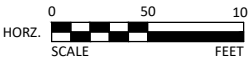
WET DITCH MIX WITH CAT 20 ROLLED EROSION PREVENTION PRODUCT

SOUTHERN BOULEVARD SEED WITH CAT 20 ROLLED EROSION PREVENTION PRODUCT

SOUTHERN TALLGRASS MIX WITH CAT 20 ROLLED EROSION PREVENTION PRODUCT

STABILIZED CONSTRUCTION EXIT

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TRAVIS L. WINTER

46649

MAY 27, 2025

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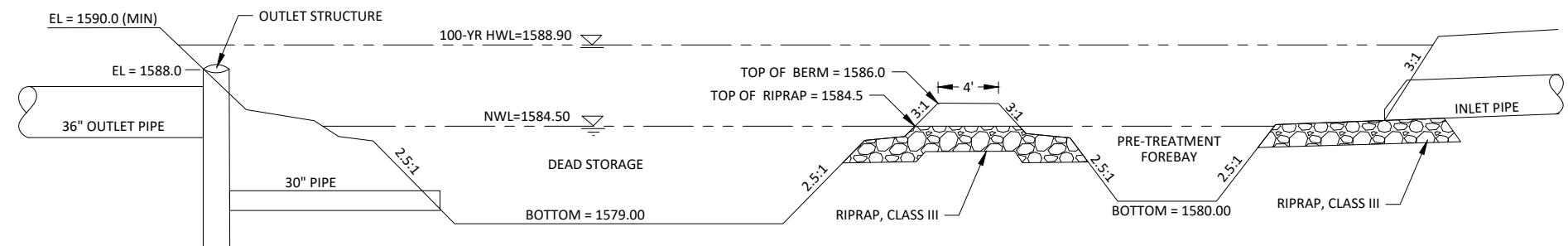
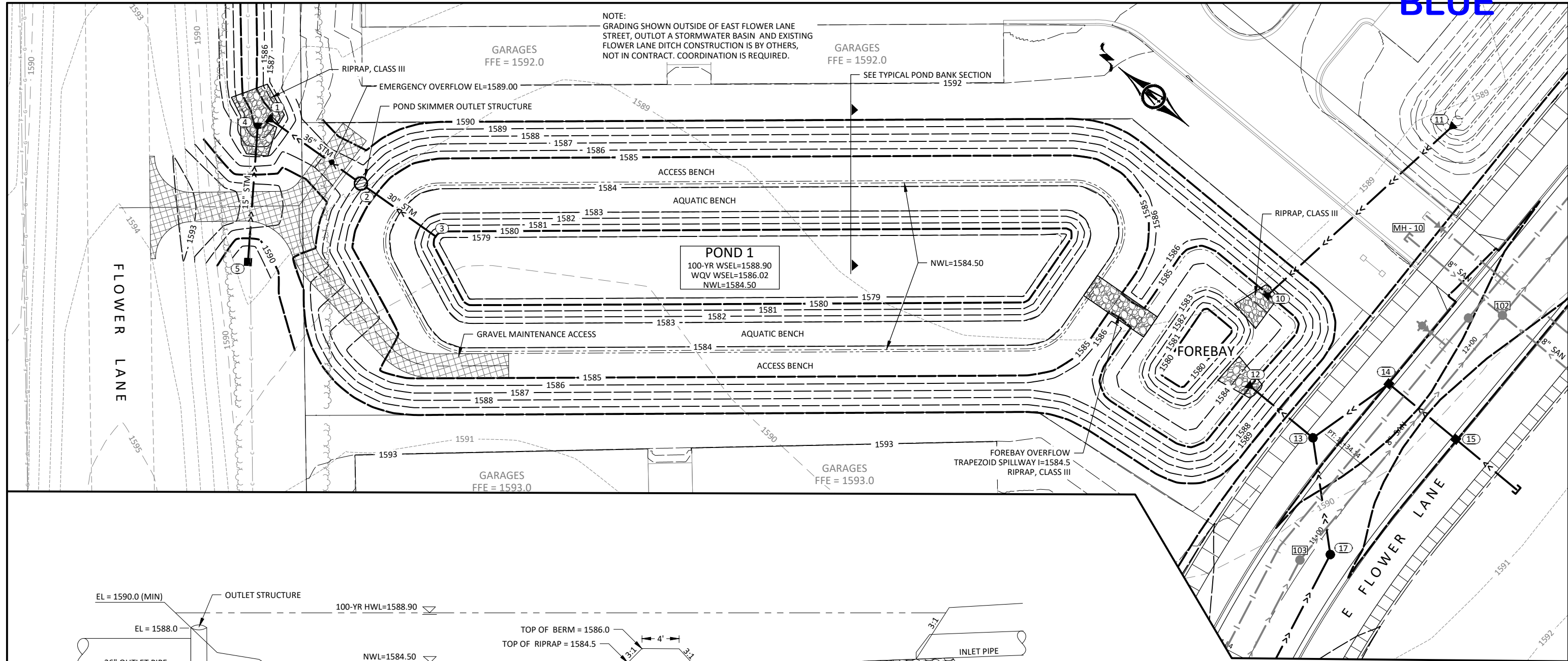
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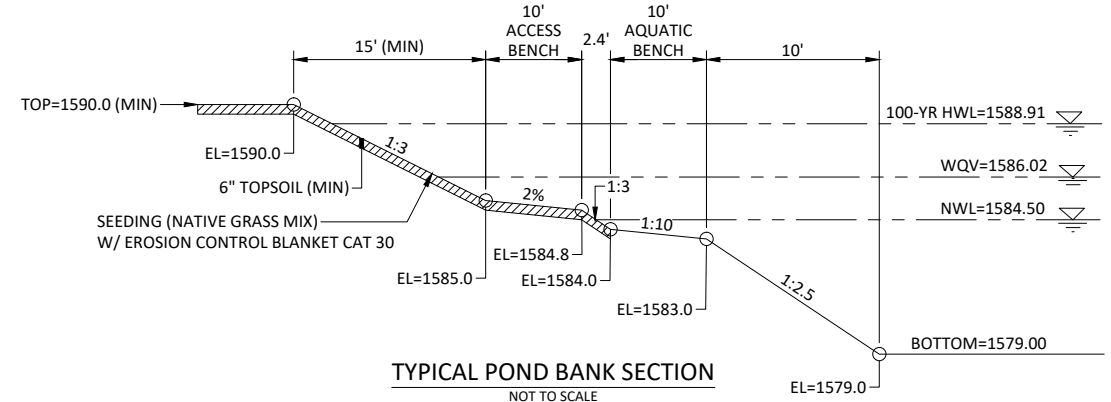
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CITY OF WORTHINGTON, MINNESOTA	SHEET
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS	C2.07
STORMWATER POLLUTION PREVENTION PLAN	
NORTH AREA	

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TYPICAL SECTION - SEDIMENTATION BASIN
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TYPICAL POND BANK SECTION
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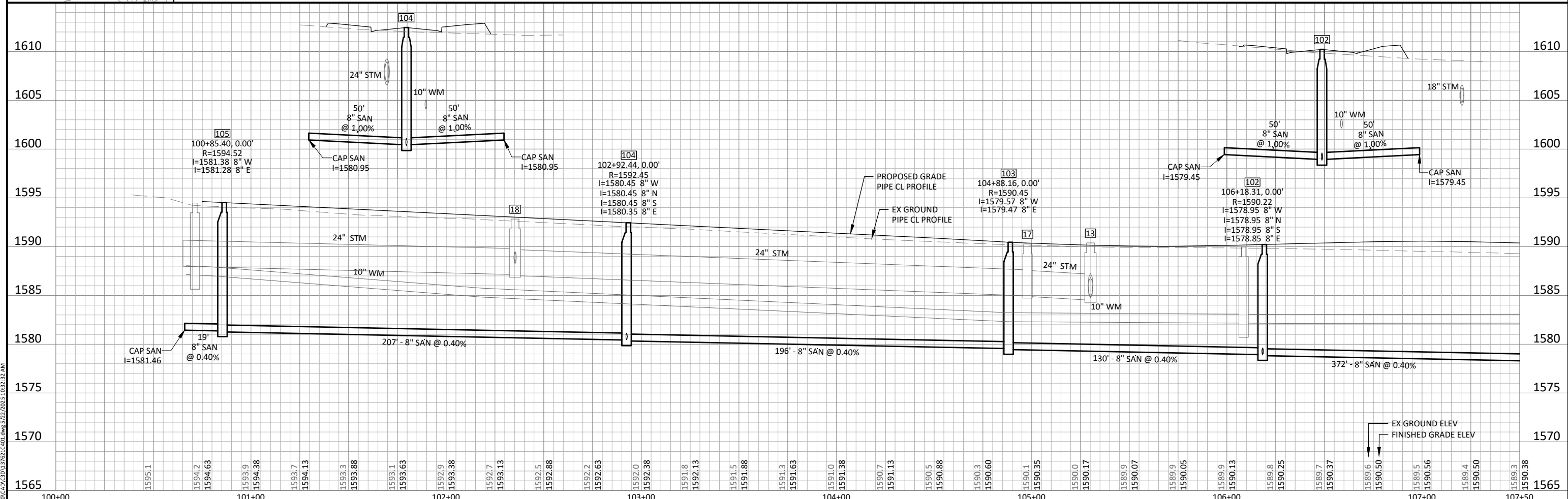
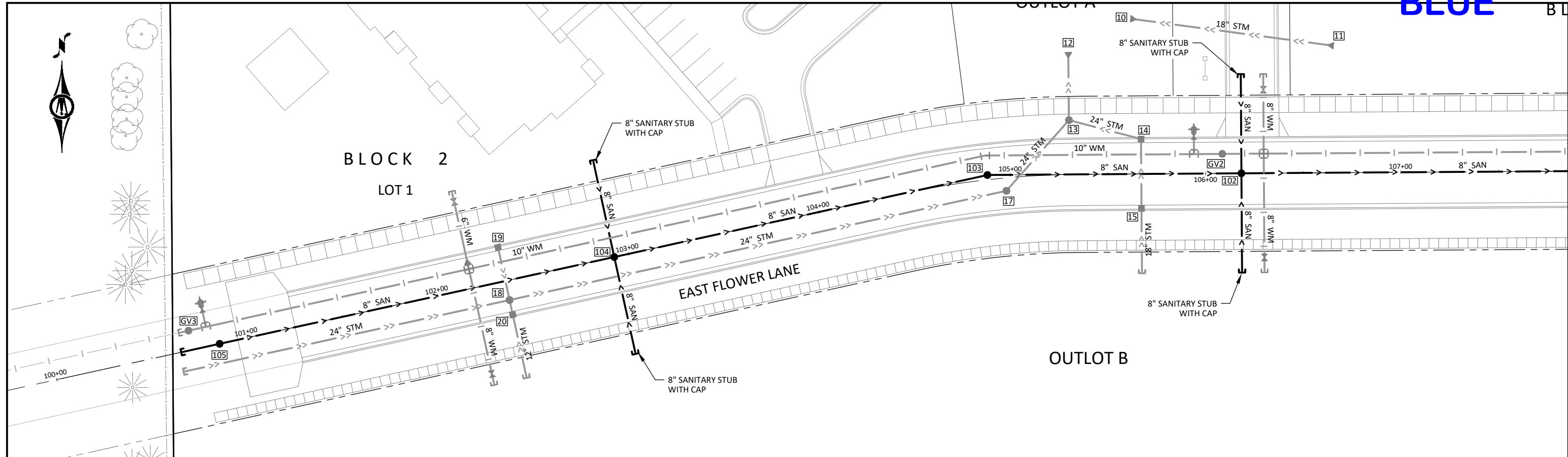


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CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
REGIONAL STORMWATER BASIN
SITE AND GRADING PLAN

SHEET
C3.01

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TRAVIS L. WINTER

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CITY OF WORTHINGTON, MINNESOTA

EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS

SANITARY SEWER PLAN & PROFILE

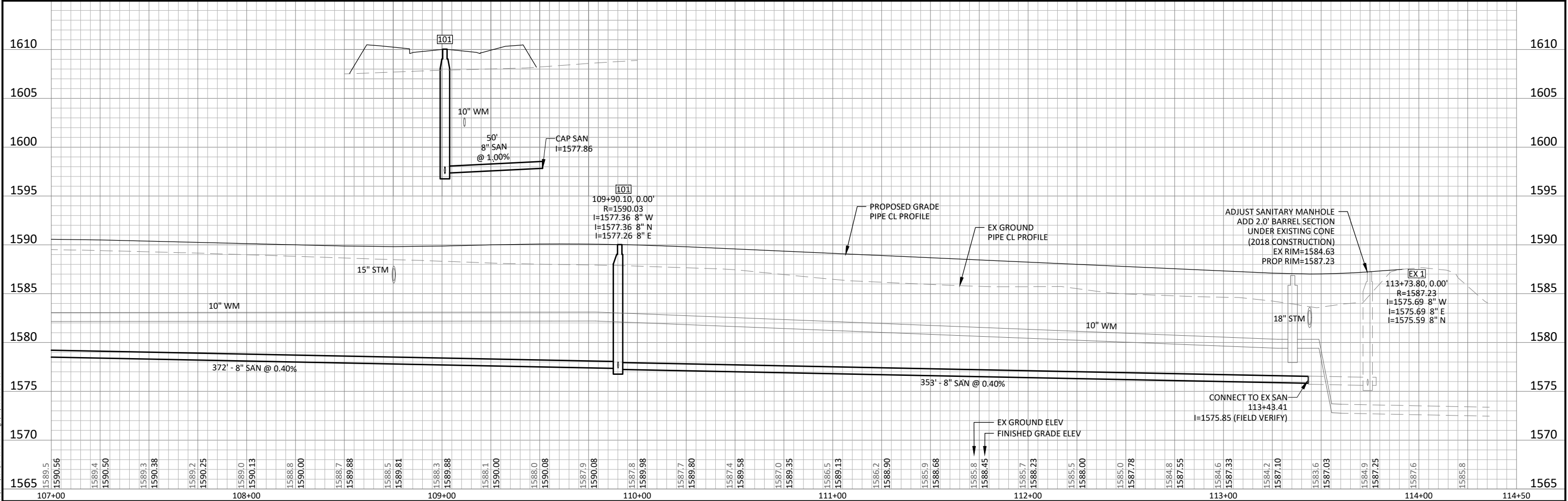
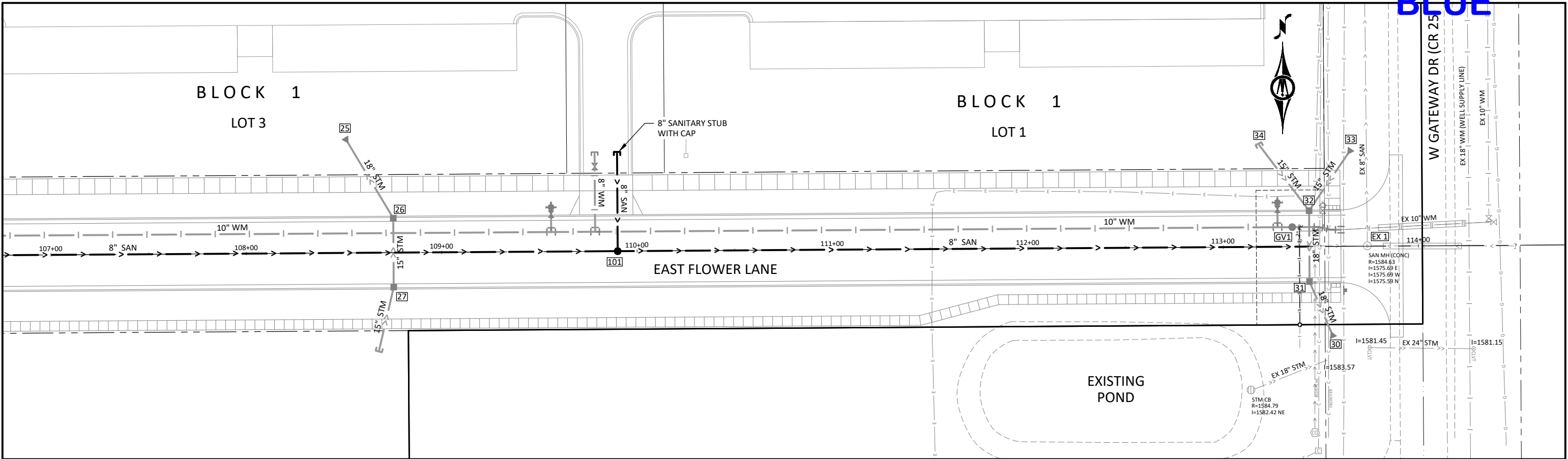
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SHEET

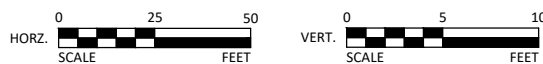
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Exhibit 3A

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TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



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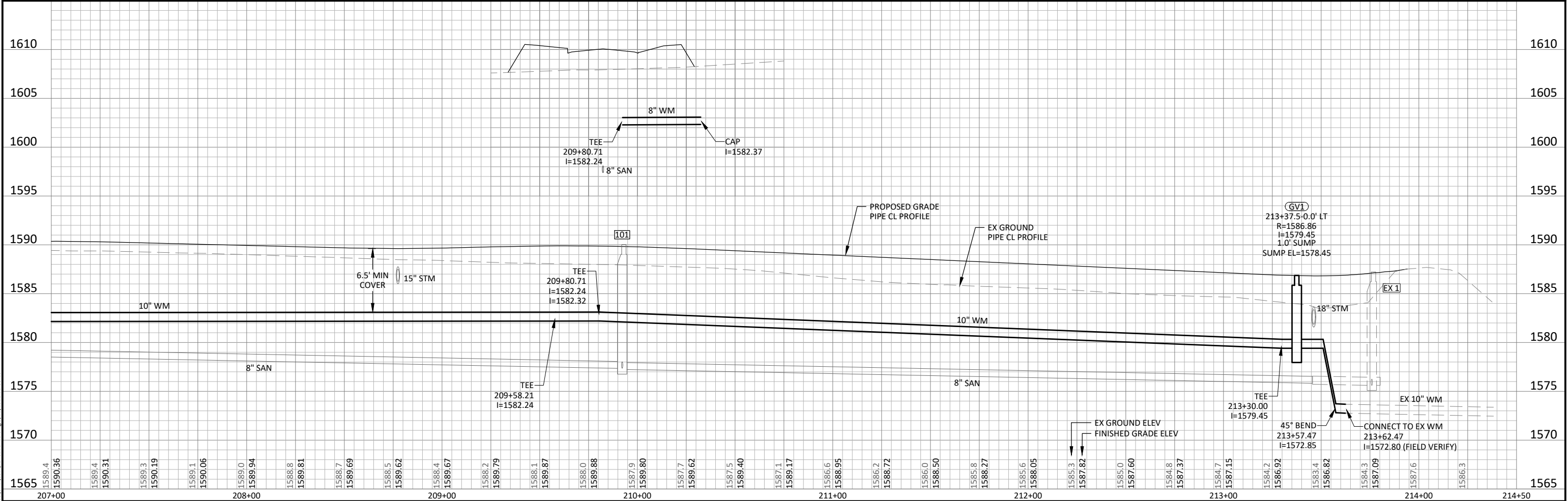
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EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
SANITARY SEWER PLAN & PROFILE
EAST FLOWER LANE

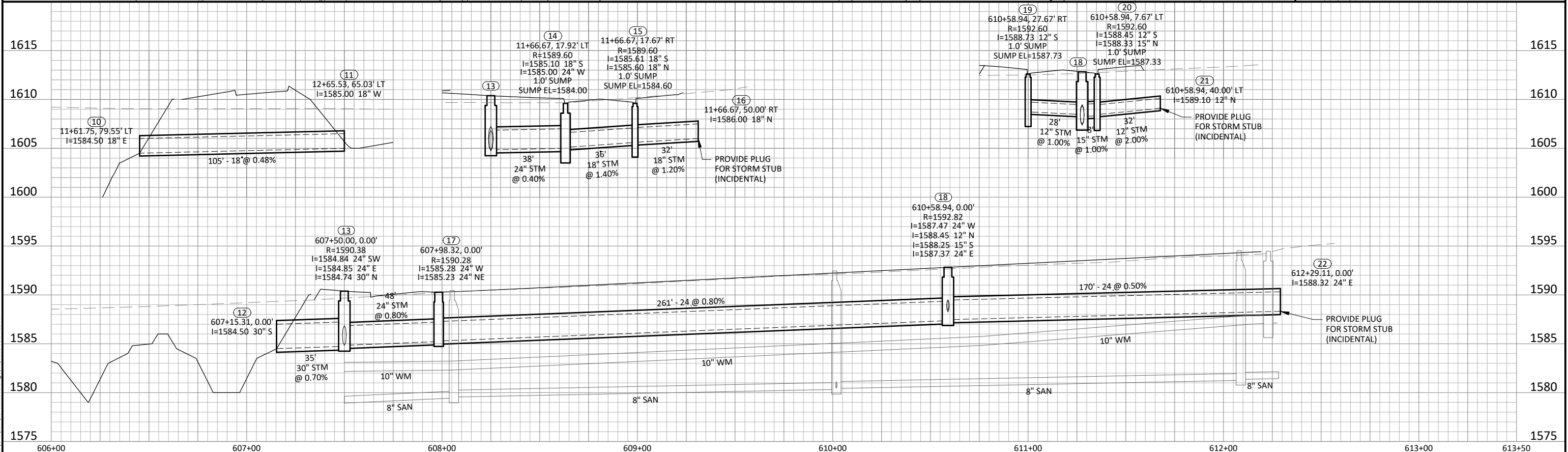
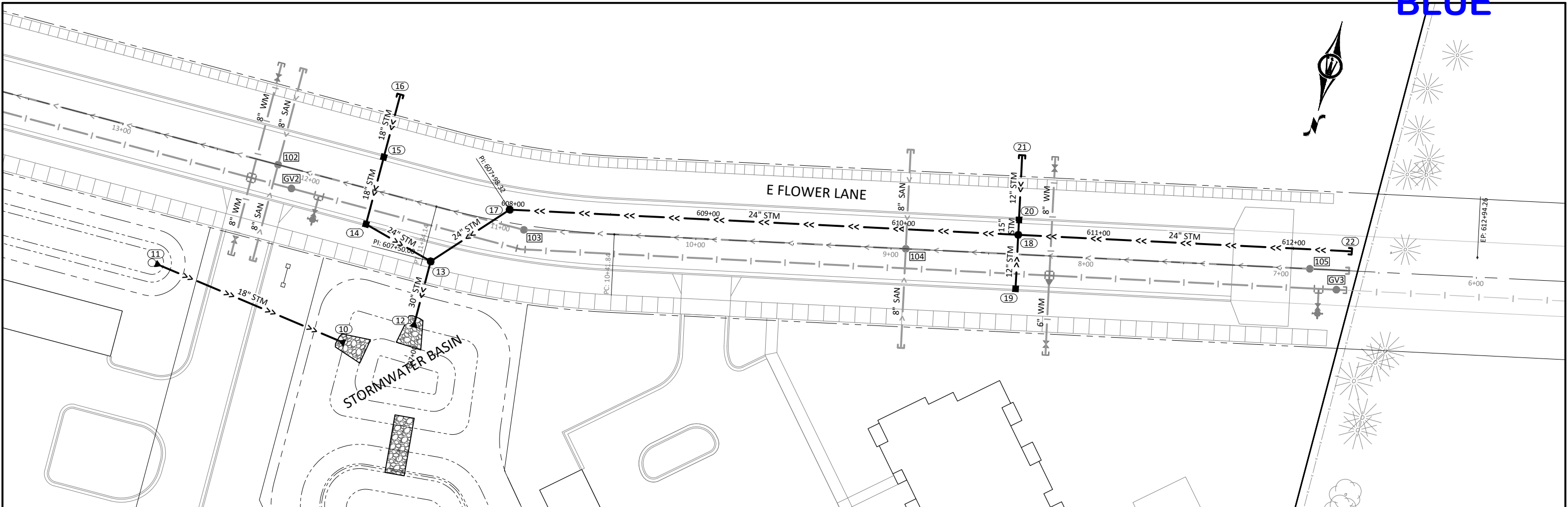
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Exhibit 3A

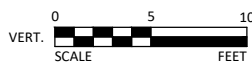
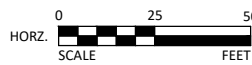


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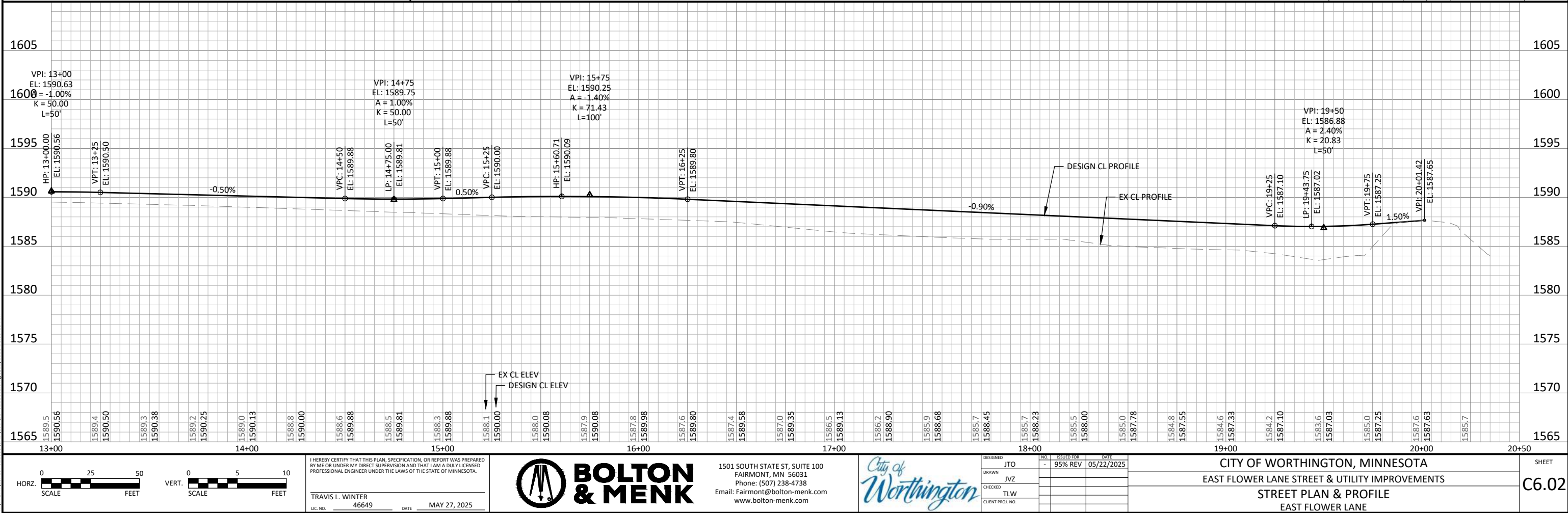
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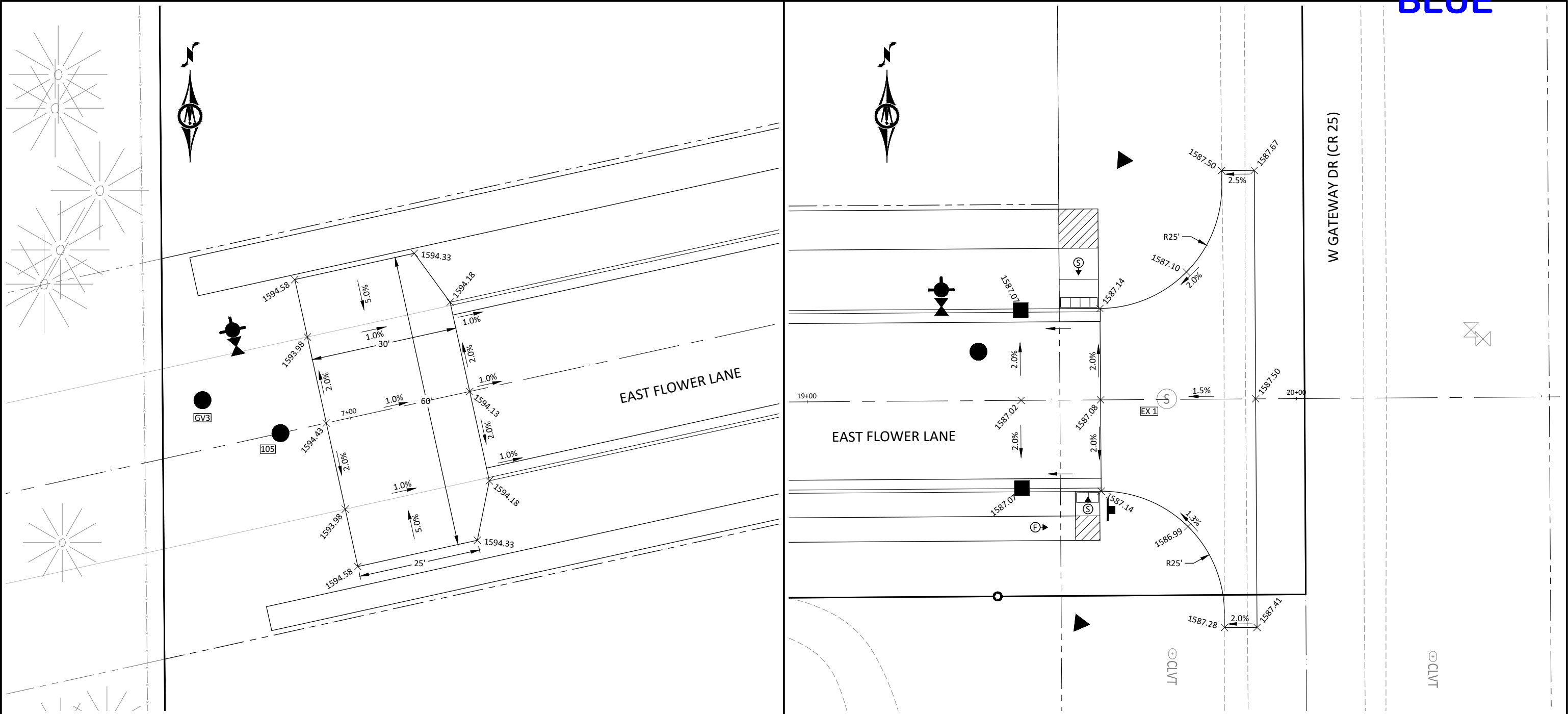
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CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
STORM SEWER PLAN & PROFILE
WEST END FLOWER LANE

SHEET
C5.02

Exhibit 3A





PEDESTRIAN RAMP LEGEND

LANDING AREA - 4' X 4' MIN. DIMENSIONS AND 1.0% OPTIMUM (2.0% MAXIMUM) SLOPE IN ALL DIRECTIONS

TRANSITION PANEL(S) - TO BE USED FOR TRANSITIONING THE CROSS-SLOPE OF A RAMP TO THE EXISTING WALK CROSS-SLOPE. RATE OF TRANSITION SHOULD BE 0.5% PER 1 LINEAR FOOT OF WALK.

PEDESTRIAN RAMP SLOPE SHALL BE 4.0% OPTIMUM (2.0% MIN. AND 5.0% MAX.) IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL BE 1.5% OPTIMUM (2.0% MAX.)

PEDESTRIAN RAMP SLOPE SHALL BE 7.0% OPTIMUM (5.0% MIN. AND 8.3% MAX.) IN THE DIRECTION SHOWN AND CROSS SLOPE SHALL BE 1.5% OPTIMUM (2.0% MAX.)

CURB HEIGHT

TRUNCATED DOMES (SEE MnDOT STANDARD PLATE 7038)

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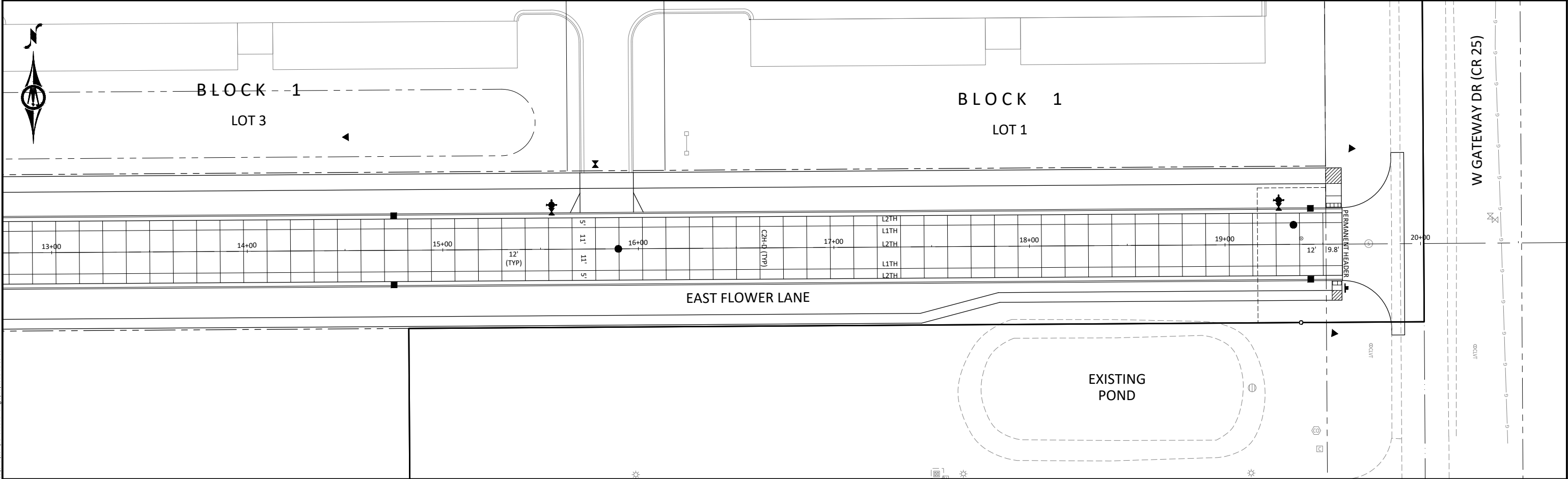
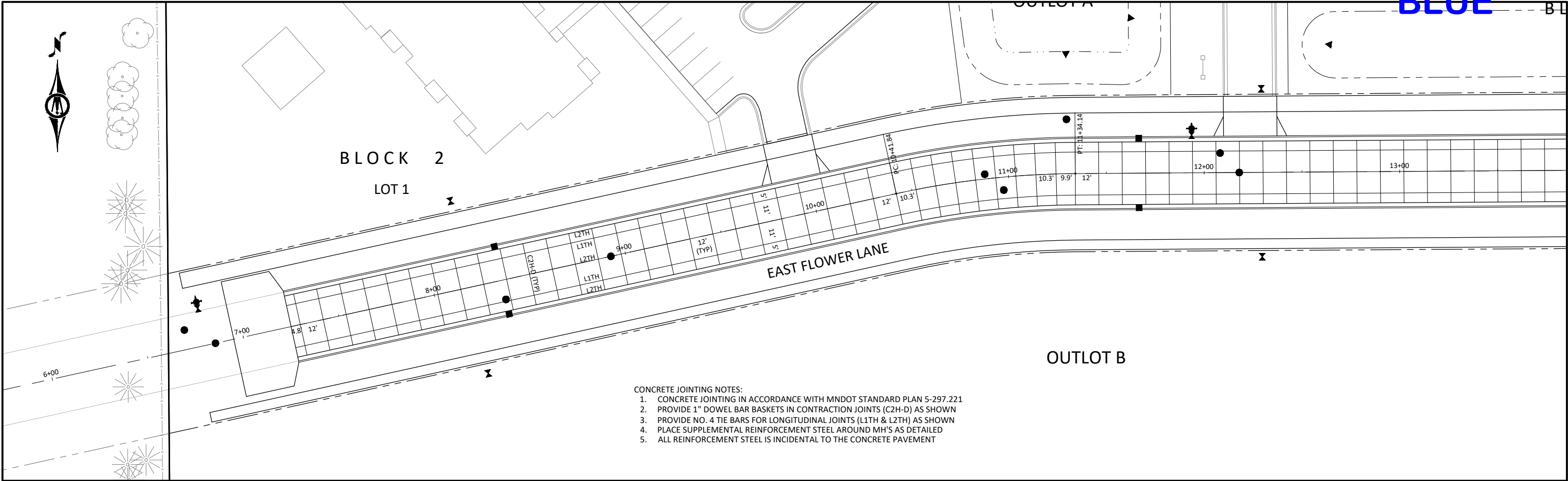
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CITY OF WORTHINGTON, MINNESOTA	SHEET C6.03
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS	
INTERSECTION DETAILS EAST FLOWER LANE & WEST GATEWAY DRIVE	

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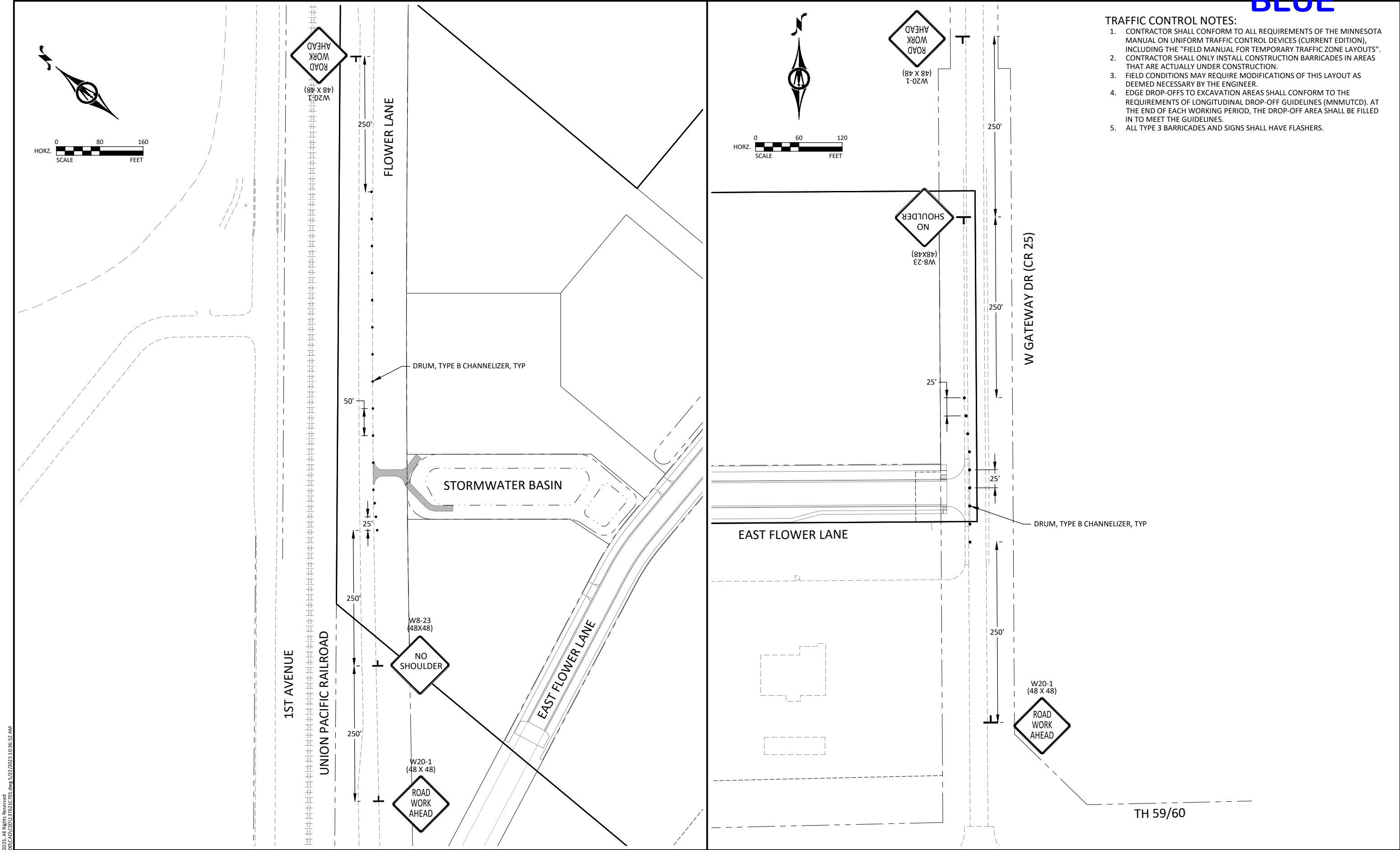
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CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
CONCRETE JOINTING PLAN
6+00 - 20+00

SHEET
C6.04



- TRAFFIC CONTROL NOTES:
1. CONTRACTOR SHALL CONFORM TO ALL REQUIREMENTS OF THE MINNESOTA MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION), INCLUDING THE "FIELD MANUAL FOR TEMPORARY TRAFFIC ZONE LAYOUTS".
 2. CONTRACTOR SHALL ONLY INSTALL CONSTRUCTION BARRICADES IN AREAS THAT ARE ACTUALLY UNDER CONSTRUCTION.
 3. FIELD CONDITIONS MAY REQUIRE MODIFICATIONS OF THIS LAYOUT AS DEEMED NECESSARY BY THE ENGINEER.
 4. EDGE DROP-OFFS TO EXCAVATION AREAS SHALL CONFORM TO THE REQUIREMENTS OF LONGITUDINAL DROP-OFF GUIDELINES (MNMUTCD). AT THE END OF EACH WORKING PERIOD, THE DROP-OFF AREA SHALL BE FILLED IN TO MEET THE GUIDELINES.
 5. ALL TYPE 3 BARRICADES AND SIGNS SHALL HAVE FLASHERS.

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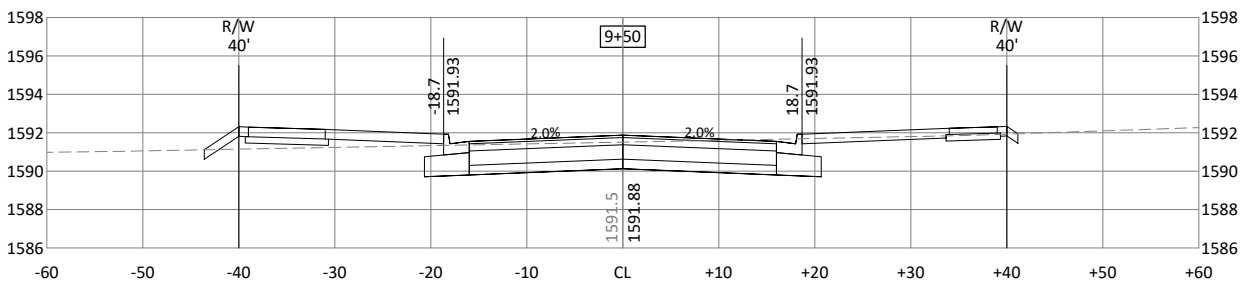
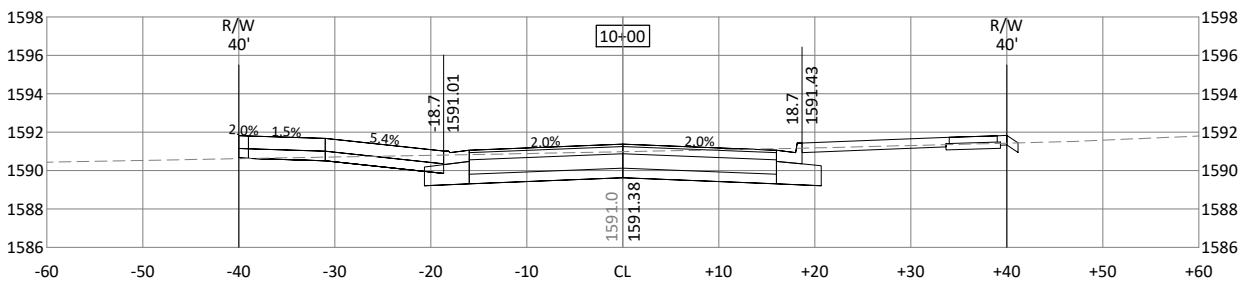
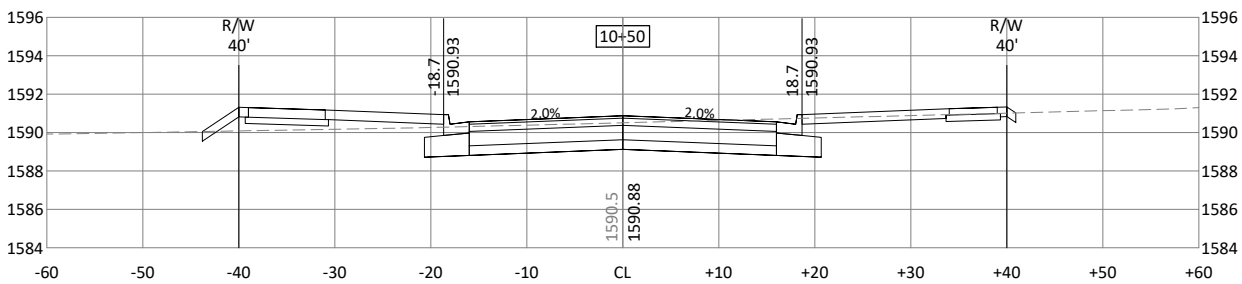
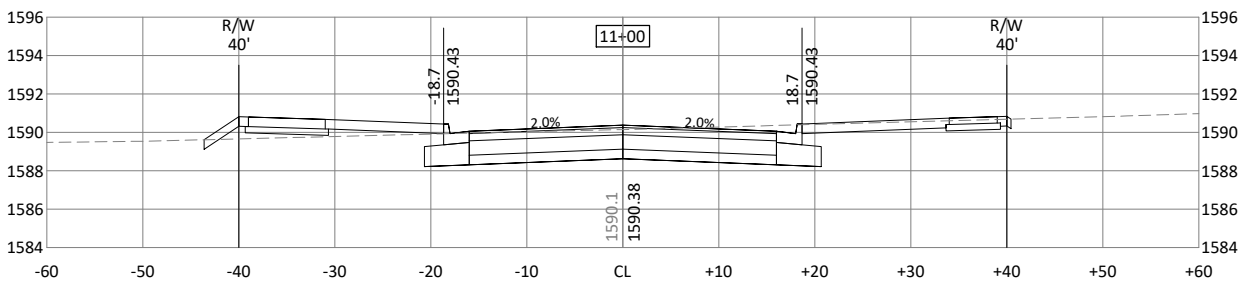
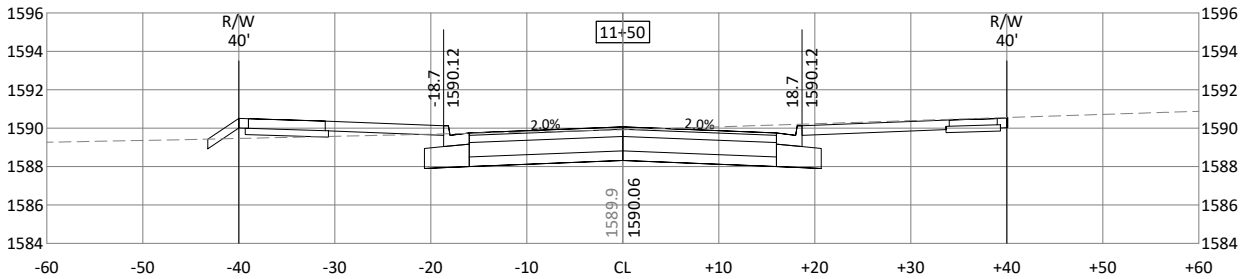
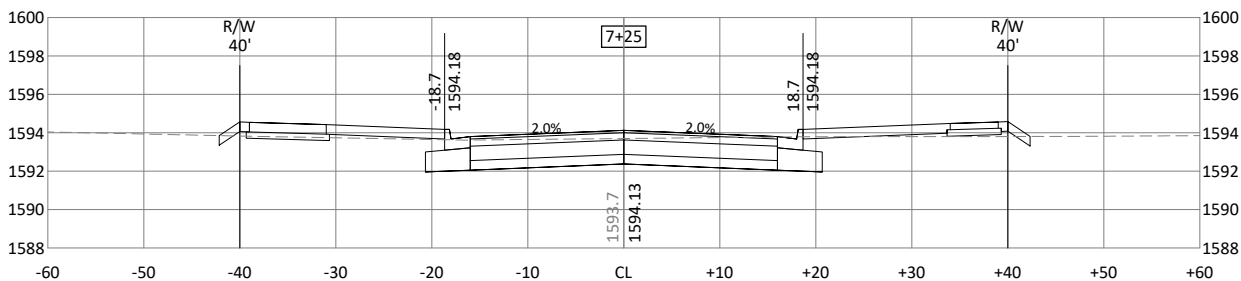
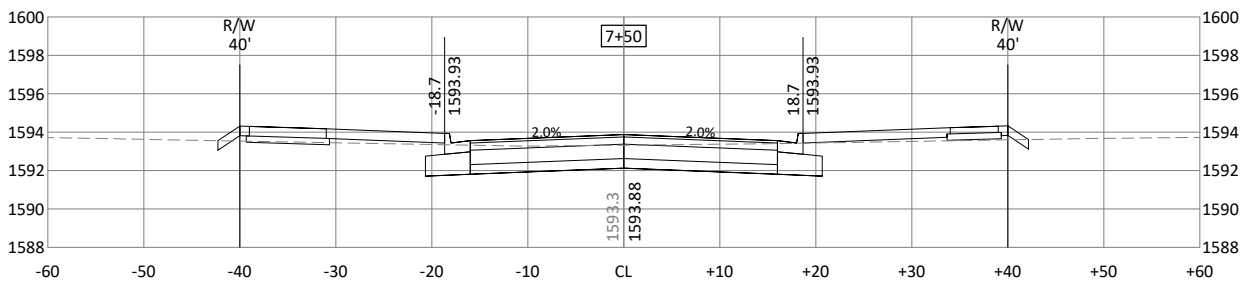
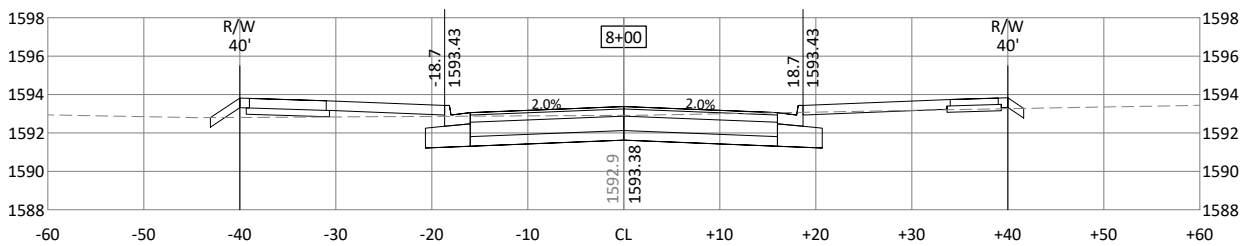
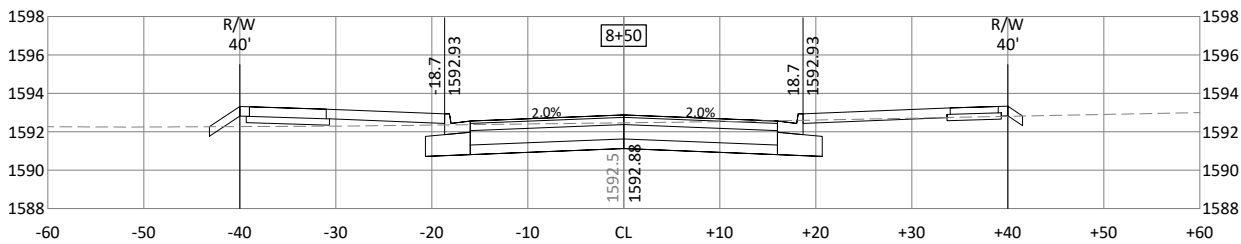
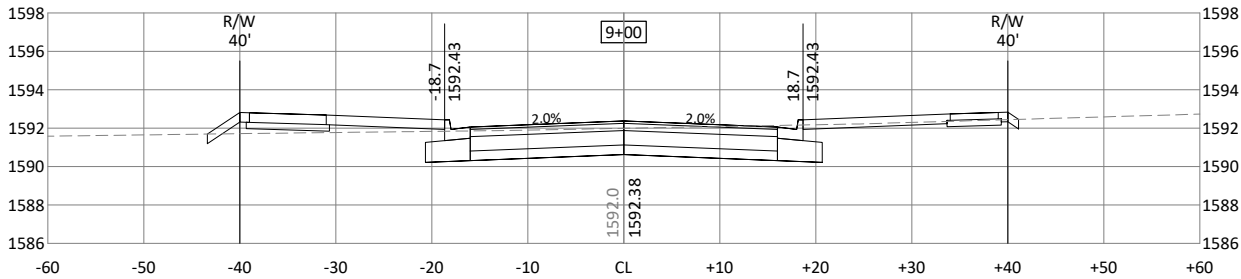


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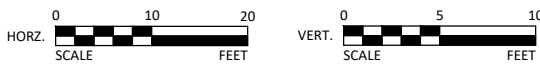


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CITY OF WORTHINGTON, MINNESOTA	SHEET C7.01
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS	
TRAFFIC CONTROL PLAN	



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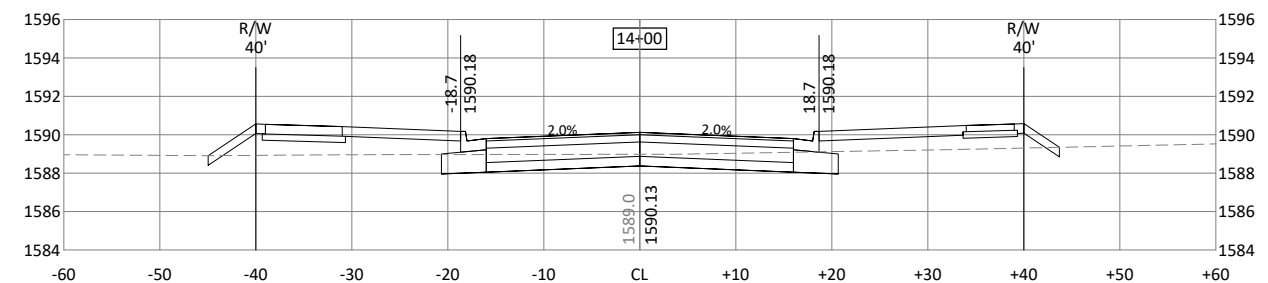
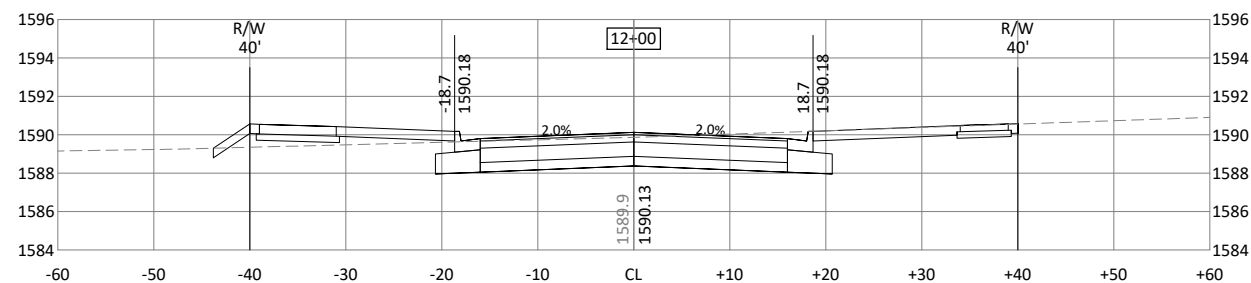


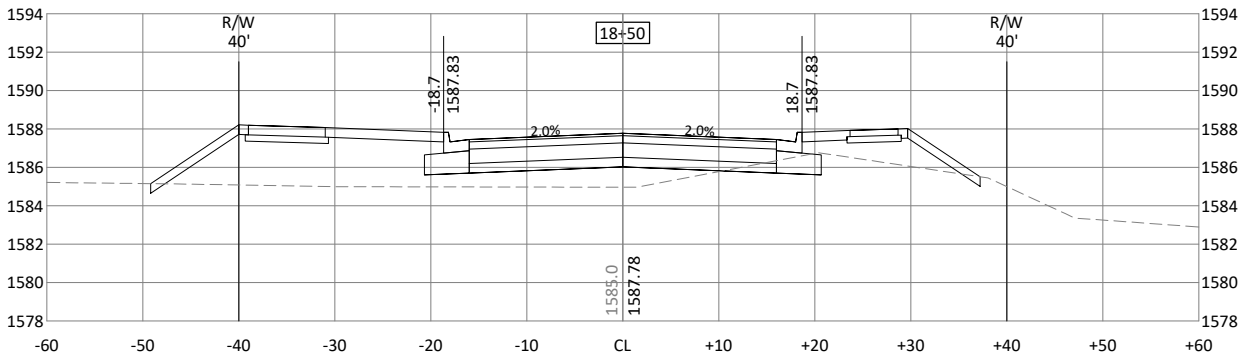
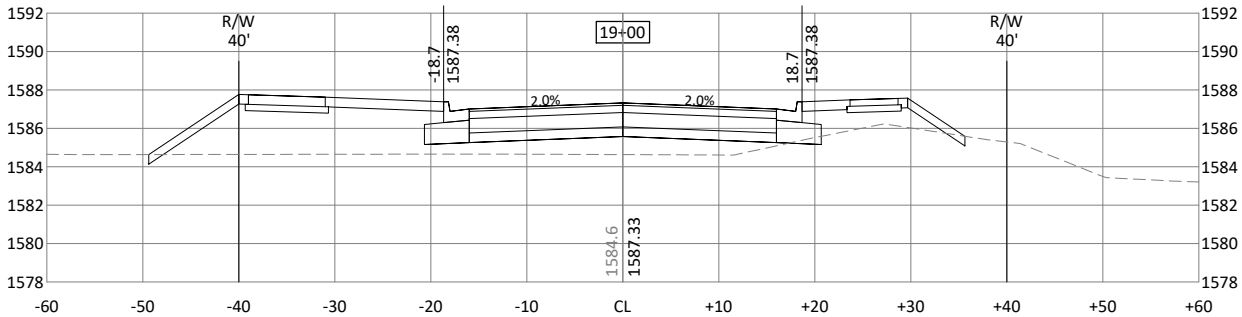
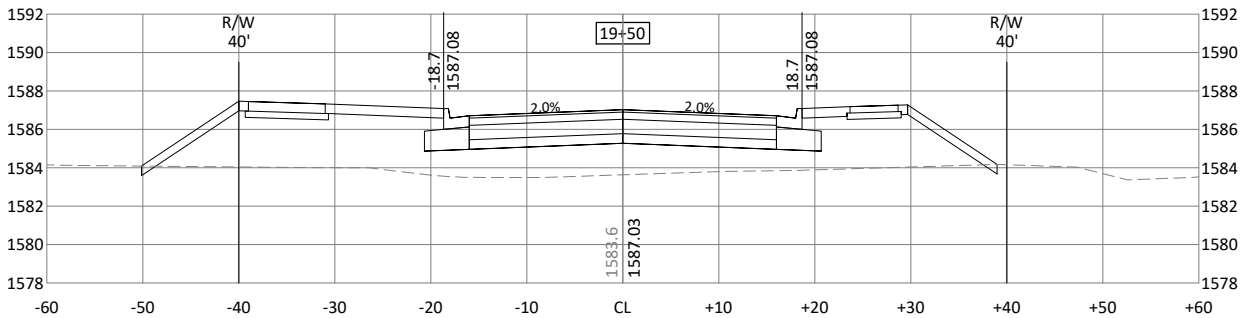
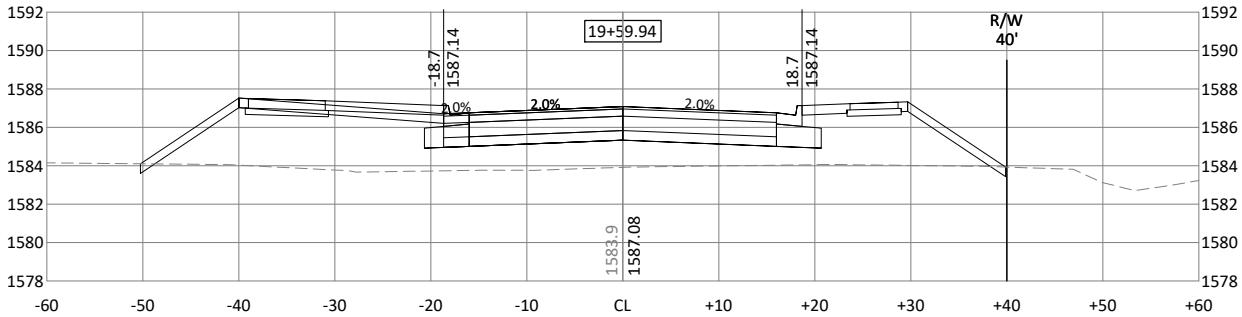
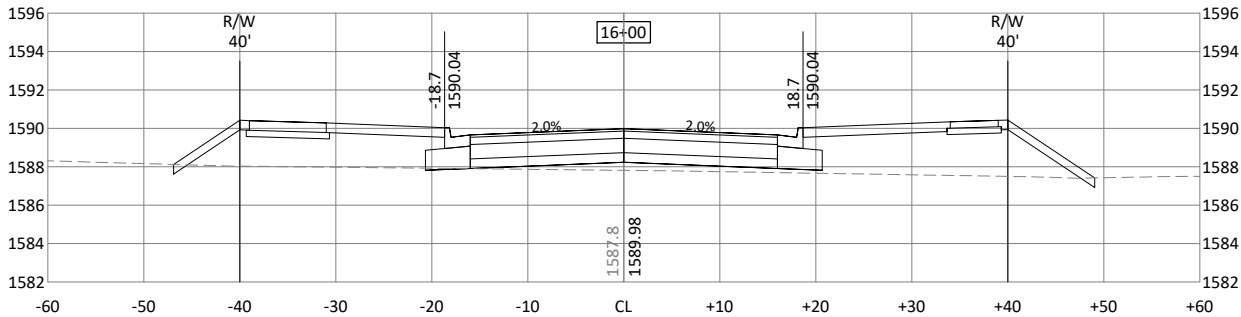
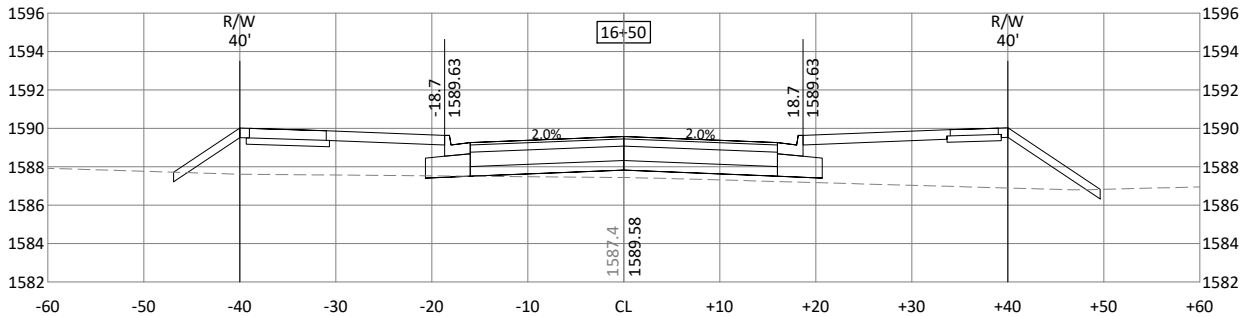
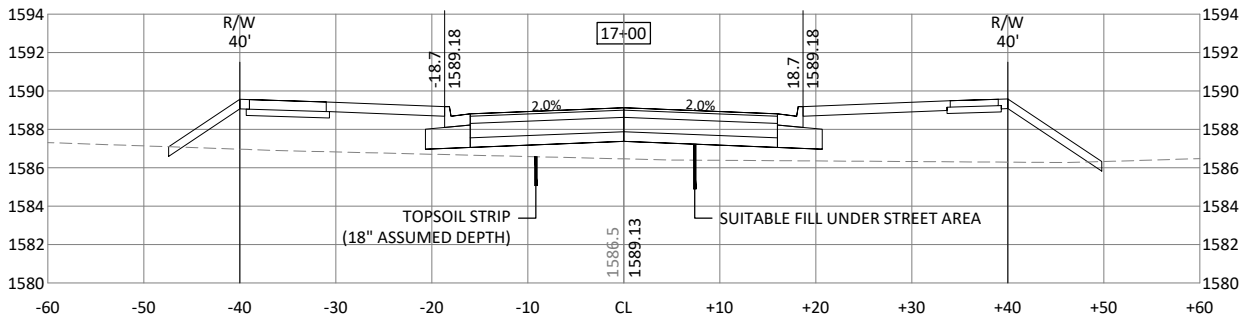
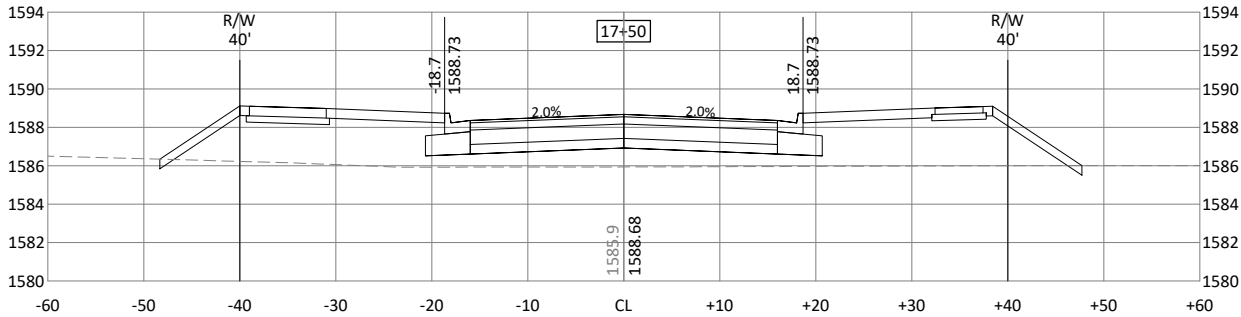
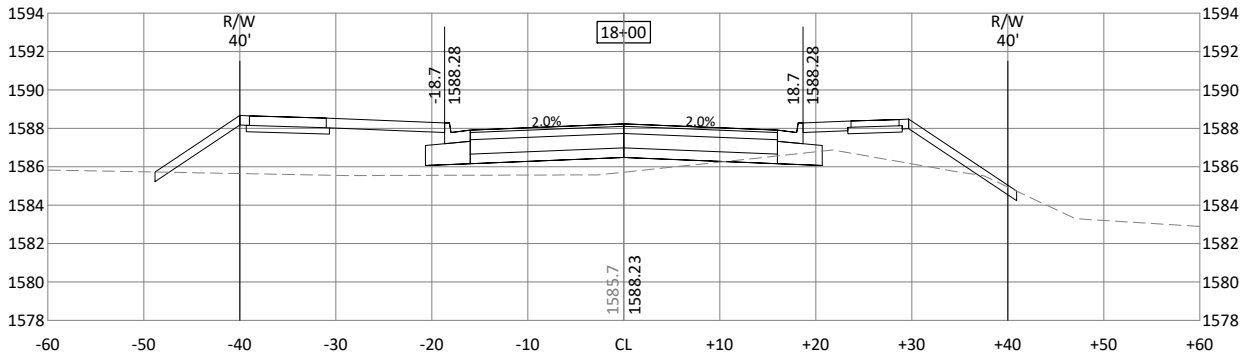
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CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
CROSS SECTIONS
7+25 - 11+50

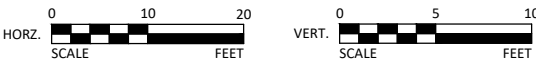
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I HEREBY CERTIFY THAT THIS PLAN, SPECIFICATION, OR REPORT WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT I AM A DULY LICENSED PROFESSIONAL ENGINEER UNDER THE LAWS OF THE STATE OF MINNESOTA.

TRAVIS L. WINTER
LIC. NO. 46649 DATE MAY 27, 2025



1501 SOUTH STATE ST, SUITE 100
FAIRMONT, MN 56031
Phone: (507) 238-4738
Email: Fairmont@bolton-menk.com
www.bolton-menk.com



DESIGNED	JTO	NO.	ISSUED FOR	DATE
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CITY OF WORTHINGTON, MINNESOTA
EAST FLOWER LANE STREET & UTILITY IMPROVEMENTS
CROSS SECTIONS
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C8.03

RESOLUTION NO. 2025-05-_____**APPROVING PLANS AND SPECIFICATIONS AND
ORDERING ADVERTISEMENT FOR BIDS**

WHEREAS, Pursuant to Resolution No. 2025-05-28, passed by the council on **May 12, 2025**, the city engineer has prepared plans and specifications for the improvement of the following described streets by necessary grading and base, and by bituminous surfacing and to assess the benefited property for all or a portion of the cost of the improvement, pursuant to Minnesota Statutes, Chapter 429. Future plan is to tie in with the Railroad Crossing and eliminate the existing Flower Lane from Railroad to East Gateway Drive:

East Flower Lane Extension

Starting at the East Gateway Drive just North of Casey's and extending approximately 800' West.

and

WHEREAS, said plans and specifications for said improvement have been presented to the council for approval.

**NOW THEREFORE, BE IT RESOLVED BY THE CITY COUNCIL OF THE CITY OF
WORTHINGTON, MINNESOTA:**

1. Such plans and specifications for said improvement, a copy of which is attached hereto and made a part hereof, are hereby approved.
2. The city clerk shall prepare and cause to be inserted in the official newspaper and on **QuestCDN.com** an advertisement for bids upon the making of said improvement under such approved plans and specifications. The advertisement shall be published for 21 days, shall specify the work to be done, shall state that bids will be received at the office of the city clerk of the city of Worthington, Minnesota, until 2:00 p.m. on **Wednesday, June 18, 2025**, at which time they will be publicly opened in the council chambers of City Hall, 303 Ninth Street, Worthington, Minnesota, by a council designated committee, and bids and the responsibility of the bidders will be considered by the city council at 5:30 p.m. on **Monday, June 23, 2025**, in the council chambers of City Hall, 303 9th Street. Any bidder whose responsibility is questioned during consideration of the bid will be given an opportunity to address the council on the issue of responsibility. No bids will be considered unless sealed and filed with the clerk and accompanied by a cash deposit, cashier's check, bid bond or certified check payable to the clerk for five percent (5%) of the amount of the bid.

Adopted by the city council of the city of Worthington, Minnesota, this the **27th** day of **May, 2025**.

(SEAL)

Rick Von Holdt, Mayor

Attest: _____
Mindy L. Eggers, City Clerk

LICENSE AGREEMENT

THIS LICENSE AGREEMENT (this "Agreement") is made and entered into this ____ day of _____, 2025, (the "Effective Date"), by and between Minnesota Energy Resources Corporation, hereinafter referred to as "Licensor", and City of Worthington, a municipal corporation, hereinafter referred to as "Licensee". Licensor and Licensee are individually sometimes referred to as a "Party", and collectively as the "Parties".

WITNESSETH:

Licensor, for and in consideration of the covenants, conditions, and agreements hereinafter contained, hereby grants to Licensee a temporary, non-exclusive license and permission to develop, at Licensee's sole cost and expense, and use that portion of Licensor's real property that is described on Exhibit A attached hereto and made a part hereof (the "Premises") as a recreation trail, and to place thereon a trail for hiking, biking, and cross-country skiing and other similar non-motorized recreational uses (the "Trail") for use by the general public and for no other purpose or purposes whatsoever (except those purposes as may be determined by Licensor for its own use).

The License and permission herein granted is subject to the following conditions:

1. **Term:** The term of this Agreement (the "Term") shall commence on the Effective Date and shall continue for an initial a period of twenty (20) years (the "Initial Term"), and thereafter shall continue on a year-to-year basis, subject to termination as hereinafter provided.
2. **Termination:** Upon the expiration of the Initial Term, this Agreement may be terminated at any time by either Party hereto by providing at least ninety (180) days prior written notice to the other Party of such termination. Notwithstanding any of the terms and conditions contained herein, (a) should Licensor require exclusive use of any part of property, including the Premises, for its purposes, or (b) should Licensee be in default under any of the covenants, terms or conditions of this Agreement, then Licensor may, upon written notice to Licensee, terminate the Agreement, in whole or in part on such part or parts of the Premises it requires, and Licensee shall, not later than ninety (180) days after receiving notice of such termination, at its sole cost and expense, remove the Trail from the Premises.
3. **Commencement of Construction:** Licensee shall have one (1) full year, beginning at the Effective Date of this Agreement, to begin construction of the Trail and one (1) full year after the start of construction to complete construction of the Trail, as permitted herein. If Licensee fails to begin construction of the Trail within one (1) year after the Effective Date, or to complete construction of the Trail within one (1) year after commencement of such construction, this Agreement shall terminate immediately without notice from Licensor and Licensee shall, not later than ninety (90) days after such termination, at its sole cost and expense, remove the Trail from the Premises.
4. **Base Rent:** During the Initial Term and extensions thereto, no Base Rent shall be due.
5. **Assignment:** Licensee shall not assign this Agreement nor permit any transfer by operation of law or otherwise of the interest in the herein-described Premises acquired through this Agreement unless otherwise approved by Licensor in writing.
6. **Acceptance of Premises:** Licensee acknowledges and agrees that the Premises are being made available pursuant to the license set forth herein in "as is" condition, and Licensor makes no warranty or representation of any kind as to the condition, quality or suitability of the soil, subsoil or surfacing of the Premises, any other property of Licensor or anything thereon or therein. Licensee has examined the Premises described hereinabove and acknowledges the condition thereof and that no representations or warranties as to the condition or repair thereof and no agreements to make any alterations, repairs or improvements in or about the Premises have been made by Licensor. Licensee's

taking possession of the Premises shall be conclusive evidence as against Licensee that the Premises were in good order and satisfactory condition for use as a Trail and other permitted ancillary uses. Licensor shall not be liable for any damages arising from acts or neglect of Licensee or any of its invitees or users of the Premises, whether authorized to use the Premises or not.

7. **Permitted and Prohibited Uses:** The Premises shall be used for the purpose of constructing, installing, operating, maintaining, using, repairing, and removing the Trail, for hiking, biking, cross-country skiing and other similar non-motorized recreational uses and for no other purpose whatsoever. No vehicles, trucks, cars or equipment are to be parked or materials stored on the Premises at any time without the specific written approval of Licensor. Furthermore, Licensee agrees that no motorized vehicles, including but not limited to cars, trucks, snowmobiles, motor bikes, mini-bikes, motorcycles, mopeds, go-carts and all-terrain vehicles will be used, operated or permitted on the Premises. However, Licensee shall be permitted to use motorized vehicles for the patrol and/or maintenance of the Trail and the Premises, and electric bicycles shall be permitted on the Trail subject to any speed limits that are established by Licensor from time to time. Licensee also agrees that no horses will be used or permitted on the Premises. Licensee agrees that no kites, model airplanes or similar or dissimilar objects that may come in contact with or in close proximity to the facilities of Licensor and its successors and assigns, will be used, operated or permitted on or about the Premises.
8. **Signage:** Licensee shall not place or maintain or allow to be placed or maintained by any person or persons, any signs or advertising billboards upon the Premises at any time, except as required or permitted by this Section. Licensee shall install and maintain signs that are necessary to identify Licensee's Trail and occupancy of the Premises at every road crossing and at least every 2,600 feet along the Trail or more frequently if requested by Licensor. Licensee further agrees to post, maintain at all times, and if necessary, replace signs that expressly state the uses that are permitted and prohibited under Section 7 hereof. In addition, Licensee hereby agrees to post safety and traffic signs along the Trail and at road crossings, railroad crossings, driveways, farm crossings and any other vehicular crossings along the Trail. All signs must be approved by Licensor prior to erection or installation on the Premises.
9. **Zoning and Permits:** Licensee hereby agrees that Licensor has made no representations that the Premises are properly zoned for the proposed use by Licensee, and it is expressly understood that Licensee hereby assumes any and all obligations and responsibilities with respect to compliance with all applicable zoning laws and ordinances of any regulatory bodies which may have jurisdiction. Any change in zoning must be approved by Licensor. This Agreement is conditioned on Licensee's obtaining all necessary permits and authority for the proposed use. All permits required hereunder shall be acquired by Licensee at its sole cost and expense. If permits are required, copies of the final permits must be provided to Licensor prior to the commencement of any work on the Premises by Licensee, and allowing reasonable time for Licensor to review the permits.

Without limiting the generality of the foregoing, Licensee agrees that no temporary or permanent construction may occur in any wetlands. If any work is proposed within wetlands, Licensee must obtain the appropriate permits from the Minnesota Department of Natural Resources ("MNDNR") and the Army Corps of Engineers ("ACOE"). Licensee must provide a copy of the application and final permits to Licensor prior to working within the wetland and provided Licensor has reasonable time to review said permits.

10. **Governmental Jurisdiction:** Licensee shall, in the use and occupancy of the Premises, comply with all laws, ordinances, rules and regulations of the City of Worthington, Nobles County, State of Minnesota and all other governmental bodies having jurisdiction over the operation of Licensee or Licensor's business or occupation of the Premises.

11. **Construction and Other Liens:** Licensee shall have no authority, express or implied, to create or place any lien or encumbrance of any kind or nature whatsoever upon, or in any manner to bind, the interest of Licensor in the Premises, for any claim in favor of any person dealing with Licensee, including those who may furnish materials or perform labor for any construction or repairs. Licensee covenants and agrees that it will pay or cause to be paid all sums legally due and payable by it on account of any labor performed, materials, services or supplies furnished in connection with any work performed on the Premises by or at Licensee's direction on which any lien is or can be validly and legally asserted against its interest in the Premises or the improvements thereon and that it will save and hold Licensor harmless from any and all loss, liability, cost or expense, including costs of suit and reasonable attorney's fees, based on or arising out of asserted claims or liens against the license interest granted hereunder or against the right, title and interest of Licensor in the Premises or any other property of Licensor, or under the terms of this Agreement. Licensee will not permit any construction lien or any other liens which may be imposed by law affecting Licensor's or its mortgagee's interest in the Premises to be placed upon the Premises or any other property of Licensor arising out of any action or claimed action by Licensee, and in case of the filing of any such lien Licensee will promptly pay the same. Licensee shall promptly provide Licensor with lien waivers from all contractors and subcontractors for all work performed and material and services supplied by or on its behalf at the Premises. If any such lien shall remain in force and effect for ten (10) days after written notice thereof from Licensor to Licensee and Licensee has not posted with Licensor a bond, reasonably satisfactory to Licensor, in the amount of at least 125% thereof, Licensor shall have the right and privilege of paying and discharging the same or any portion thereof without inquiry as to the validity thereof, and any amounts so paid, including expenses and interest, shall be reimbursed by Licensee to Licensor immediately on presentation of a bill therefor.
12. **Diggers Hotline:** Licensee shall contact Diggers Hotline at (800) 242-8511 to locate any underground facilities at least five (5) days prior to any work, excavation or construction on the Premises, in order to determine the location of electric, telephone, water, communication, natural gas and other facilities within the Premises and surrounding lands in the vicinity of the contemplated work and the applicable clearance requirements for work performed in the proximity of such facilities.
13. **Plan Review and Approval:** Licensee shall submit to Licensor for its review and written approval, detailed site plans and construction drawings (hereinafter "Plans") showing the proposed location of the Trail with respect to the Premises and any utility facilities that might be affected by the Trail, which Plans shall also include proposed grade changes, Trail cross sections, signs and other improvements to the Premises which Licensee desires to construct or install. If Licensee intends to use any fill on the Premises, Licensee shall include the type and source of any fill material on the Plans and any fill material used shall be subject to inspection and analysis by Licensor for the presence of Hazardous Material as defined in Section 23 below. Licensee will not install or construct or permit to be installed or constructed, any improvements upon, or make any alterations or substantive changes to the approved plans for, the Premises without first submitting plans and specifications to Licensor and receiving Licensor's approval thereof.
14. **Height Restrictions:** Licensee hereby agrees that no vehicles or equipment will be used, stored operated or permitted on the Premises or Licensor's adjacent land having a height in excess of 12 feet above original ground grade level, unless otherwise approved in writing by Licensor as their respective interests lie.
15. **Runoff Control:** Licensee and its contractors shall follow best management practices to prevent or control site runoff and erosion. Following the completion of Licensee's work, all adjoining areas shall be restored.
16. **Drainage and Grade Changes:** Licensee shall be permitted, at its sole cost and expense, to grade, level, and apply crushed stone and/or asphalt paving and plant grass on the Trail and Premises as may be permitted by Licensor, except that the Licensee shall not in any manner alter or change the original ground grade level of the Premises, or alter in any manner the drainage on the Premises without obtaining written

permission from Licensor. Licensor, at its sole discretion, may require Licensee to install such drainage facilities as Licensor may deem necessary to adequately drain the Premises or Licensor's adjacent lands, which facilities are made necessary due to or arising out of any filling, grading, leveling, paving or use by the Licensee hereunder. All such drainage facilities (including culverts, storm sewers, ditches, etc.) shall be installed by and at the expense of Licensee and to the complete satisfaction of Licensor. Any existing culverts that run beneath Licensor's lands must be maintained or enhanced; they cannot be removed, filled or otherwise blocked.

17. **Maintenance and Landscaping:** Licensee shall, at its sole cost and expense, keep the Premises routinely mowed and free of weeds, and shall trim and/or cut down any trees and shrubs to the satisfaction of the local Weed Commissioner and Licensor. Licensor reserves the right to trim and/or cut down any trees and shrubs on the Premises. Licensee further agrees that it shall maintain the entire Premises as a Trail as described hereinabove, and perform such other landscaping maintenance necessary to maintain an appearance suitable to such use as a Trail for biking, hiking and cross-country skiing and other similar non-motorized recreational uses. Licensee shall not plant any trees or shrubs on the Premises without the express written permission of Licensor. Licensee agrees to keep the Premises clean and free from all debris, rubbish, litter and trash. Licensee shall be permitted to, or upon request of Licensor shall, place trash containers at convenient locations on the Premises. Such containers shall be emptied on a regular basis by Licensee, prior to overflowing or creating a nuisance.
18. **Work Standards:** During construction, use of and repairs or maintenance to the Premises pursuant to this Agreement, while in proximity to electrical conductors or gas facilities presently existing or to be installed at some future date, Licensee hereby agrees to conform to all laws, rules, ordinances, acts and regulations such as O.S.H.A. Any work done by Licensee on the Premises shall be performed in such a manner as not to interfere with the use of Licensor's lands for electric lines, gas lines, communication lines and related or unrelated facilities, both overhead and underground, which presently exist or might be installed at a later date.
19. **Damage to Facilities:** Licensee hereby agrees to effectively prevent damage to electrical facilities, communication facilities or related facilities due to or arising out of the construction, installation, operation, maintenance, repairs, removals and use of the Premises by Licensee, or its employees, agents, contractors, customers or invitees. In the event the Premises or adjoining lands or existing electrical and communication facilities or related or unrelated facilities thereon are damaged as a result of activities conducted on or in any way connected with Licensee's construction, installation, operation, maintenance, repairs or removal activity on the Premises, or the use of the Premises by Licensee or any of its employees, agents, contractors, customers or invitees, the repair of such damage shall be completed by or at the direction of Licensor and Licensee shall reimburse Licensor for the cost thereof upon presentation of a bill therefor.
20. **Movement of Licensor Facilities:** In the event it is necessary for Licensor, or existing tenants, permittees or licensees to reconstruct, protect, modify, adjust, replace or relocate its facilities due to the aforementioned use of the Premises and/or the construction, operation, maintenance or existence of Licensee's facilities, Licensee agrees to promptly reimburse Licensor, and/or such affected tenants, permittees or licensees upon presentation of a bill for the costs and expenses incurred by Licensor as a result thereof; provided, however, that Licensee shall be given the option of moving or relocating its material and equipment to reduce or eliminate costs associated herewith.
21. **Proximity to Gas Lines/Facilities:** During construction, installation, operation, maintenance, repairs, removals and use of the Premises pursuant to this License, while in proximity to gas lines and gas facilities presently existing or to be installed at some future date, Licensee hereby agrees to exercise due caution, comply with all applicable safety laws and regulations and take or suffer no action which results in the gas lines or gas facilities being placed in violation of any applicable law or regulation. In the event the Premises, adjoining lands or gas facilities or related facilities thereon are damaged as a result of activities conducted on or in any way connected with Licensee's construction, installation,

operation, maintenance, repairs or removal activity on the Premises, or the use of the Premises by Licensee or any of its employees, agents, contractors, customers or invitees, repairs shall be completed by or at the direction of Licensor and paid for by Licensee upon presentation of a bill therefor.

22. **Solid Waste:** Licensee shall not cause or permit any solid wastes to accumulate or be stored in or about the Premises. All solid wastes shall be properly stored, handled and routinely disposed of off the Premises in a manner that complies with applicable federal, state and local laws, codes and/or regulations. Licensee shall not store, handle or dispose of solid wastes in a manner that will pollute or contaminate the atmosphere, ground or water or which may adversely affect the health, welfare or safety of persons whether located on the Premises or elsewhere.

23. **Environmental:** As used in this Agreement:

"Discharge" means the releasing, spilling, leaking, leaching, disposing, pumping, pouring, emitting, emptying, treating, or dumping of Hazardous Materials at, into, onto, or under or migrating from, under, or onto the Premises or Licensor's adjacent lands or any part thereof (including, without limitation, the soil, subsurface water, or groundwater) or the threat thereof, regardless of whether the result of an intentional or unintentional action or omission.

"Environmental Law" means any federal, state or local law, whether common law, statute, ordinance, rule, regulation, judicial or administrative decision, policy or guideline, pertaining to Hazardous Materials, industrial hygiene, environmental conditions, or the regulation or protection of human health or the environment, and all amendments thereto as of this date and to be added in the future, and any successor statute, ordinance, rule, regulation, or judicial or administrative decision of policy or guideline promulgated thereto.

"Hazardous Materials" means any regulated substance, toxic substance, hazardous substance, hazardous waste, pollution, pollutant, or contaminant, as defined or referred to in any applicable Law, including, without limitation, the Resource Conservation and Recovery Act, as amended, 42 U.S.C. §6901 et seq., the Comprehensive Environmental Response, Compensation and Liability Act, as amended, 42 U.S.C. §9601 et seq. ("CERCLA"); the Water Pollution and Control Act, 33 U.S.C. §1251 et seq., and any and all analogous state laws, together with any and all amendments thereto, regulations promulgated thereunder, and all substitutions thereof, as well as words of similar purport or meaning referred to in any other applicable law, including, without limitation, radon, asbestos, polychlorinated biphenyls, urea formaldehyde products, petroleum products, and petroleum based derivatives. Where a law defines any of these terms more broadly than another, the broader definition shall apply.

Licensee shall not use in any way, or permit or suffer the use of, the Premises or any part thereof (i) in violation of any Environmental Law or (ii) to directly or indirectly prepare, produce, generate, manufacture, refine, treat, transport, store, maintain, handle, dispose of, transfer, process, exacerbate, relocate, or Discharge any Hazardous Material without the prior written consent of Licensor, which consent may be withheld in Licensor's sole discretion.

Licensee shall give Licensor immediate verbal and follow-up written notice of any of the following with respect to the Premises or Licensor's lands, whether actual or threatened, of which Licensee has or obtains actual knowledge: (i) a violation of any Environmental Law; (ii) a Discharge; (iii) an environmental condition requiring responsive or remedial action; or (iv) an emergency environmental condition (each event or circumstance described in the preceding clauses (i) through (iv), an "Environmental Occurrence"). Licensee shall not, without the prior written consent of Licensor, which consent may be withheld in Licensor's sole discretion, communicate with any governmental authorities or other third parties in connection with any Environmental Occurrence, Hazardous Material, or other environmental condition related to the Premises or Licensor's adjacent lands, except to the extent expressly required by Environmental Laws. Licensee shall notify Licensor of any such communication with governmental

authorities or other third parties required by Environmental Laws and shall provide Licensor a reasonable opportunity to coordinate with Licensee regarding such communications.

Licensee shall be solely responsible for and shall indemnify Licensor and all of its agents, contractors, employees, officers, directors, affiliates and parent (collectively, the "Indemnified Parties") for all costs and expenses of any environmental assessment, investigation, sampling, containment, removal, remediation, and/or disposal as Licensor deems appropriate in connection with any Environmental Occurrence on or about the Premises arising out of any act or omission of Licensee or any of Licensee's contractors, subcontractors, agents, employees, and/or guests. Notwithstanding the foregoing, Licensee shall take no such actions in connection with such Environmental Occurrence except pursuant to and in strict accordance with Licensor's written instructions. Licensor reserves the right to conduct, at Licensee's sole cost and expense, any assessment, investigation, sampling, containment, removal, remediation, and/or disposal as may be required in connection with any such Environmental Occurrence.

Notwithstanding anything the contrary set forth herein, LICENSEE SHALL NOT CONDUCT ANY INVASIVE ENVIRONMENTAL OR OTHER STUDIES WITH RESPECT TO CONDITIONS AT THE PREMISES OR LICENSOR'S ADJACENT LANDS WITHOUT LICENSOR'S PRIOR WRITTEN CONSENT (INCLUDING, WITHOUT LIMITATION, AS TO THE PURPOSE, SCOPE, AND TIMING OF SAME), SUCH CONSENT TO BE WITHHELD IN LICENSOR'S SOLE DISCRETION, AND, IN THE EVENT OF LICENSOR'S APPROVAL, WITH THE USE OF LICENSOR'S CONSULTANTS AT LICENSEE'S SOLE COST AND EXPENSE. FURTHERMORE, LICENSEE SHALL, SOLELY IF EXPRESSLY REQUESTED BY LICENSOR, PROVIDE TO LICENSOR (AT LICENSEE'S SOLE COST AND EXPENSE) ALL SAMPLING RESULTS, DATA, REPORTS, AND OTHER INFORMATION RESULTING FROM ANY SUCH STUDIES.

Without limiting the foregoing, if Licensee encounters contamination at, on, or under the Premises or Licensor's adjacent lands, Licensee shall call Marita Stollenwerk at (414) 221-4172 with Stephanie Berti as a backup at (414) 221-4045 before taking any action with respect to same. In the event of a discharge of hazardous materials caused by the Licensee or contractors employed on behalf of the Licensee, please contact Erin Ganzenmuller immediately at (414)221-4045.

Licensee and Developer shall not permit any liens to attach to the Premises or Licensor's adjacent lands by reason of the exercise by Licensee of its rights hereunder.

24. **Notification:** Licensee agree to contact Josh Lowe (507) 848-1050 or such other person or phone number as Licensor may from time to time designate, within the specified time limits to inform him about the following occurrences:

- a) At least seven (7) days prior to the commencement of the project herein permitted.

- a) Within seven (7) calendar days after the termination of the License herein permitted with a plan for restoration.

- b) Within seven (7) calendar days after the restoration has been completed.

- c) Within seven (7) calendar days after a lapse of six (6) months since Licensee accepted this License if the project herein permitted has not been undertaken by such date; within seven (7) calendar days after each six (6) month interval thereafter until the project herein permitted is undertaken, not to exceed two (2) full years from the date hereof.

25. **Insurance:** Licensee shall carry one million dollars (\$1,000,000) per occurrence (or such higher limit as Licensor may request from time to time) General Liability Insurance coverage with Licensor being named as additional insured, as evidenced by certificates to be furnished to Licensor annually during the Term of this License. **A current certificate of such insurance must be furnished at the time this Agreement**

is executed by Licensor. A certificate of insurance must be furnished before this License will be renewed. Prior to construction of any alterations or improvements on the Premises, Licensee's, contractors and subcontractors shall also provide to Licensor proof of insurance.

26. **Waiver of Liability:** Anything in this Agreement to the contrary notwithstanding, Licensor shall not be liable to Licensee or anyone claiming under Licensee for any loss or damage to property or injury to or death of persons occurring on the Premises or Licensor's adjacent lands or in any manner growing out of or in connection with Licensee's use and occupancy of the Premises or the condition thereof. Licensee hereby waives all right of recovery against Licensor, its agents, employees, assignees and invitees for any such loss, damage, injury or death.
27. **Indemnification of Licensor:** To the extent not prohibited by law, Licensee agrees to indemnify, defend and hold harmless Licensor and the Indemnified Parties from and against all claims, demands, suits, causes of action, losses, costs (including reasonable attorneys' fees and expenses) and damages (collectively "Licensor's Losses") incurred by Licensor arising from any breach, violation or default by Licensee under this Agreement or any negligence or willful misconduct of Licensee or any of Licensee's employees, representatives, agents, guests, invitees or contractors. This indemnity and hold harmless provision shall include indemnity from and against any and all liability, fines, suits, demands, costs and expenses of any kind or nature (including, without limitation, attorneys' fees and disbursements) incurred in or in connection with any such Licensor's Losses or proceeding brought thereon, and the defense thereof. Notwithstanding the foregoing, Licensee shall not be required to indemnify Licensor to the extent the indemnified claim or Licensor's Losses are a result of (i) the negligence or willful misconduct of Licensor; or (ii) Licensor's failure to comply with this Agreement or any applicable laws or regulations of any government body that concern this Agreement or Premises. Without limiting the generalities of the foregoing, Licensor shall not be liable for any damages to any property or business operation, or injuries to any person or persons on or about said Premises due to any cause whatsoever.
28. **Safety and Protection:** Licensee hereby agrees, at its sole cost and expense, to erect and maintain any barricades, guard rails, fencing, and/or safety devices for protection as they relate to the protection of the Licensor's City, gas facilities and related construction and operational procedures. Licensee agrees to maintain the same in good condition, repair and appearance at all times. Licensee will submit to Licensor any and all plans and specifications for the installation of barricades, guard rails, fencing and/or safety devices or protection which may be installed on the Premises and such installations shall not be made without the consent and prior written approval of Licensor. No fencing, barricades or other improvements shall be installed or erected for any purpose which will obstruct, interfere with or impede the free access of Licensor to the Premises or Licensor's adjacent land or facilities.
29. **Galvanic/Stray Current/Grounding:** Licensee agrees to release Licensor from any responsibility for damage or personal injury resulting from electromagnetic fields electrolysis due to local galvanic or stray current conditions on or along the Premises. Further, Licensee agrees to assume all costs for electrolysis protection. All improvements, including bridges, shall be grounded unless otherwise approved in writing by Licensor.
30. **Removal of Improvements:** Licensee hereby agrees, upon the expiration or early termination of this Agreement by forfeiture, lapse of time or otherwise, if so requested in writing by Licensor, to remove promptly, at its sole cost and expense, all or part of its improvements including Trail surfaces and drainage structures from the Premises. In the event Licensee cannot or is unable or unwilling to remove said improvements and related facilities as directed by Licensor, Licensee hereby authorizes Licensor to do so, and Licensee hereby agrees to reimburse Licensor for any and all expenses incurred in connection therewith, including restoration as hereinafter required, upon presentation of a bill therefor, and Licensee hereby agrees to indemnify and save harmless Licensor from all liability of any kind whatsoever that Licensor may have incurred by such removal.

31. **Restoration:** Licensee agrees to restore or cause to restore the Premises and Licensor's adjacent lands to the condition existing prior to any disturbance to such lands and Premises. Licensee further agrees that upon the earlier termination or expiration of this Agreement by either Party, the Premises (and, if applicable, Licensor's adjacent lands) shall be restored to the condition existing prior to any disturbance or improvement from the aforementioned use of the Premises. Included, but not limited to, in such restoration, after construction and subsequent to termination of this License, shall be the spreading of topsoil and sowing perennial type grass seed on any disturbed areas, replacement of crushed stone and/or paved surfacing, replanting of shrubs and other ground cover and repair of fences and gates or other damages incurred due to or arising out of the permission herein given.
32. **Snow Plowing:** Licensee shall be permitted to plow, but not pile, the snow on the Premises in the event it desires to do so.
33. **Taxes:** During the Term, Licensee shall be responsible for all taxes on the Premises, such taxes being defined as any and all federal, state and local governmental, quasi-governmental or public authority taxes, assessments and charges of any kind or nature, whether general, special, ordinary or extraordinary (but not including income or franchise taxes or any other taxes imposed upon or measured by Licensor's income or profits, except as provided below), or payments to governmental authorities in lieu thereof, whether or not in contemplation of the parties to this License, which Licensor shall pay or become obligated to pay because of or in connection with the ownership, renting, or operation of the Premises (including but not limited to charges for the installation, maintenance, repair and replacement of sewer/water, curb, gutter and roadway) and of the personal property, fixtures, machinery, equipment, systems and apparatus located thereon or used in connection therewith. Taxes shall include, without limitation, all real and personal property taxes (attributable to the year in which paid), sales taxes, assessments (special or otherwise), fire inspections, transit taxes and ad valorem taxes but shall not include penalties or late fees thereon unless the penalty and/or late fees are directly attributable to Licensee. Taxes shall also include all fees, costs and expenses (including, legal fees and court costs) paid by Licensor in connection with protesting or contesting or seeking a refund or reduction of and/or negotiating with public authorities with respect to any of the aforesaid taxes, regardless of whether Licensor is ultimately successful. If at any time during the term hereof, a tax or excise on rents or other tax however described, other than an income tax, is levied or assessed by the United States or the State of Minnesota, or any political subdivision thereof, on account of the rents hereunder or the interest of Licensor under this License, such tax shall constitute and be included in taxes. Any taxes paid by Licensor hereunder shall be reimbursable to Licensor by Licensee as additional rent.
34. **Breach of License:** In the event Licensee shall breach or violate any of the terms, conditions or provisions of this Agreement, or if any governmental agency having jurisdiction shall serve any demand, order or notice, including violations relating to zoning or municipal ordinances, upon Licensor or Licensee, Licensee shall, at its sole cost and expense, correct said breach or violation and comply with said demand, order or notice within 30 days of its receipt of such written notice or as stated within said demand, order or notice. In the further event that Licensee does not correct said breach or violation or comply with said demand, order or notice within the required time period, it shall be lawful for Licensor, without liability to Licensee and without notice or demand, and in addition to all other rights and remedies available at law or in equity, to declare this Agreement terminated and to re-enter the Premises either with or without process of law and to expel, remove and put out Licensee or any person or persons occupying the Premises, using such force as may be necessary so to do and to repossess and enjoy the Premises again as before the grant of license under this Agreement without prejudice to any remedies which might otherwise be used for the preceding breach of covenants; Licensee hereby expressly waiving all right to any notice or demand under any statute relating to forcible entry and detainer. The decision of Licensor shall be final and binding upon Licensee concerning any breach or default in the covenants and agreements contained in this License. Licensee shall be liable to Licensor for any and all costs incurred, including reasonable attorneys' fees owing to or arising out of any action taken pursuant to this provision in which Licensor prevails.

35. **Licensor Right to Enter:** Licensor reserves unto itself and for their employees, agents and contractors the right, at any time, to enter upon the Premises by any means necessary i) for performing studies, gathering of air, water, soil and other material samples, ii) for inspection of the Premises in order verify Licensee's compliance with the Lease terms, iii) for access to Licensor's lands including the Premises, iv) to inspect, patrol, construct, install, operate, maintain, replace and repair electric lines, gas lines, communication equipment and related and unrelated facilities and equipment, both overhead and underground, upon, over, across, in and beneath the Premises without liability to Licensee, the same as though this Agreement had not been entered into. Licensor through Licensor may, without liability to Licensee, require Licensee to immediately vacate all or part of the Premises upon notice to do so in the event Licensor deems it necessary to make emergency repairs to its facilities. In the event it becomes necessary for Licensor to install or erect additional electric lines, natural gas lines, communication lines and/or related facilities at some future date, Licensee hereby agrees to vacate as much of the Premises as Licensor deems necessary and for such periods of times as may be necessary to install, modify, reconstruct or erect such facilities upon receipt of notice from Licensor to do so. Licensor shall perform and complete all work under this Section as quickly as is reasonable possible to minimize the inconvenience to Licensee.

Licensee further agrees that it shall immediately vacate the Premises and close down the Trail upon notification by Licensor that weather conditions exist or may develop which could cause dangerous conditions such as icing on trees and wires.

36. **Paramount Rights:** The rights of Licensor to utilize the Premises and Licensor's adjacent lands in their utility business will at all times be and remain paramount to the rights herein granted to Licensee and nothing stated herein is to be construed as restricting Licensor from granting rights to other parties or persons in, upon or under the Premises and/or Licensor's adjacent lands, for but not limited to driveways, streets, sidewalks, sewers, water pipes and mains, drainage tiles and pipes, gas mains and pipelines, communication circuits and other allied uses. It is understood and agreed that this Agreement is subject to all existing easements, grants and licenses.

37. **Fees and Charges:** As a condition of this Agreement, Licensee shall not charge at any time fee for the use of the Trail except that Licensee may be permitted to charge a fee for group activities or special events upon written consent of Licensor, which consent shall not be unreasonably withheld.

38. **Alcoholic Beverages Prohibited:** Licensee covenants and agrees that alcoholic liquors or beverages are not permitted on the Premises or Licensor's adjacent lands.

39. **Police Protection:** Licensee shall be permitted to provide or arrange for the provision of all law enforcement and shall be permitted to reasonably require such law enforcement personnel to patrol the Premises as it deems reasonable under this License.

40. **Notices:** All notices and other communications provided for in this Option shall be in writing and sent by e-mail (with a copy by regular mail), sent by reputable overnight courier for next day delivery, or sent by certified mail, or delivered by hand, to each party at the following addresses:

If to Licensee:	City of Worthington
Address:	303 Ninth Street
City, State Zip	Worthington, MN 56187
Attn:	Mindy Eggers
Phone:	507-666-5011
Email:	megggers@ci.worthington.mn.us

If to Licensor:	Minnesota Energy Resources Corporation
	2830 S. Ashland Avenue

Green Bay, WI 54304
Attn: Dawn Van oudenhoven/Real Estate
(920) 433-5767
dawn.vanoudenhoven@wecenergygroup.com

41. **Waiver of Terms and Conditions:** Failure of Licensor or Licensee to enforce or insist upon compliance with any of the terms or conditions of this Agreement shall not constitute a general waiver or relinquishment of any such terms or conditions, but the same shall be and remain at all times in full force and effect.
42. **Costs and Attorney Fees:** Licensee shall pay and discharge all reasonable costs, expenses and attorney fees that may be incurred or paid by Licensor in enforcing the covenants and agreements of this Agreement where litigation is not commenced. In the event litigation is commenced by Licensor or Licensee to enforce any provision of this License, the prevailing Party (as determined by a judgment in favor of one Party or the other) shall be entitled to recover from the other, as additional costs, its reasonable attorney fees and costs incurred in connection with such action.
43. **No Joint Venture:** The agreements contained herein are not intended, nor shall the same be deemed or construed, to create a partnership between Licensor and Licensee, to make them joint ventures, nor to make Licensor in any way responsible for the debts or losses of Licensee.
44. **Obligations Survive:** All obligations of Licensee hereunder not fully performed as of the expiration or earlier termination of the Term of this Agreement shall survive the expiration or earlier termination of the Term hereof, including without limitation, all payment obligations with respect to taxes and all obligations concerning the condition of the Premises and Licensor's adjacent lands.
45. **Binding Effect:** The covenants and agreements herein contained shall inure to the benefit of and be binding upon the Parties hereto and their respective successors and assigns, except as otherwise provided in Section 5 hereof.
46. **Captions:** The captions in this Agreement are inserted only as a matter of convenience and for reference and in no way define, limit, construe or describe the scope or intent of such sections or paragraphs of this Agreement nor in any way affect this Agreement.
47. **Severability of Provisions:** If any term, covenant or condition of the Agreement or the application thereof to any person or Party or circumstance shall, to any extent, be invalid or unenforceable at any time, the remainder of the Agreement, or the application of such term, covenant or condition to persons, Parties, or circumstances other than those as to which it is held invalid or unenforceable, shall not be affected thereby and each term, covenant or condition of this Agreement shall be valid and be enforced to the fullest extent permitted by law.
48. **Interpretation:** The laws of the State of Minnesota shall govern the validity, performance and enforcement of this Agreement. Whenever the singular number is used, the same shall include the plural, and the masculine gender shall include the feminine and neuter genders.
49. **Acceptance:** Licensee hereby accepts this Agreement upon the terms, conditions, restrictions hereinbefore set forth, and do covenant to keep and perform each and every one of said terms conditions and restrictions.

[SIGNATURE PAGE FOLLOWS]

This Agreement may be executed in multiple counterparts, each of which shall be deemed an original agreement and both of which shall constitute one and the same agreement. The counterparts of this Agreement may be executed and delivered by electronic signature (including portable document format) by either of the parties and the receiving party may rely on the receipt of such document so executed and delivered electronically or by facsimile as if the original had been received. [Signature information below]

This License Agreement is hereby granted under the terms and conditions set forth hereinabove.

Licensee:

By: _____ Dated: _____
(Authorized Signature)

(Print Name)

(Title)

By: _____ Dated: _____
(Authorized Signature)

(Print Name)

(Title)

Licensor:

Minnesota Energy Resources Corporation

By: WEC Business Services LLC, Its Affiliate and Agent

Signature: _____ Date: _____
Kory J. Rentmeester, Manager
Property Management

EXHIBIT A

Description of the Premises

PURCHASE AGREEMENT

ROGER BASCHKE AND ANN BASCHKE, OWNERS, (hereinafter referred to as GRANTORS) of the following described property:

LOTS 8 THROUGH 12 AND THE NORTH 10' OF THE EAST 55' OF LOT 7, BLOCK 1, AUDITOR'S PLAT OF BLOCKS 1 THROUGH 5, SUNGOLD HEIGHTS, CITY OF WORTHINGTON, NOBLES COUNTY, MINNESOTA; LYING NORTHERLY OF THE NORTH RIGHT OF WAY LINE OF MINNESOTA HIGHWAY NO. 60

hereby agree to sell to the CITY OF WORTHINGTON, NOBLES COUNTY, MINNESOTA, GRANTEE, a portion of the above-described property located at 2221 OXFORD ST, WORTHINGTON, MINNESOTA; more particularly described as follows:

See Attached Acquisition Plat

upon the following conditions, to-wit:

1. The area disturbed by the project will be graded, shaped and seeded upon completion of the project by THE CITY OF WORTHINGTON.
2. The CITY OF WORTHINGTON agrees to pay the GRANTOR the sum of \$ 50,000.00 for the land by Fee Title, as a requirement for the CITY OF WORTHINGTON HWY 60 SERVICE ROAD IMPROVEMENT project.
3. The compensation and rights herein described shall be binding upon the GRANTOR, its heirs and assigns.
4. That the permission is valid for the area described above.
5. The GRANTOR grants and warrants that there is no known burial site(s), well(s), solid waste disposal site(s), hazardous substance(s), nor underground storage tank(s) on the premises described and sought herein.
6. The GRANTOR shall have five-years from the date of said settlement to renegotiate construction or maintenance damages not apparent at the time of the signing of this contract as required.
7. The GRANTOR agrees to disclose the project details, including timelines, improvements, and impacts, to any and all individuals with a leasehold interest in the subject property.
8. This Agreement may be executed in any number of counterparts as the case may be, each of which shall be deemed a duplicate original and which together shall constitute one and the same instrument. In addition, the parties agree that this Agreement may be executed by electronic, pdf or facsimile signatures by any party and such signature will be deemed binding for all purposes hereof without delivery of an original signature being thereafter required.
Words and phrases herein shall be construed as in the singular or plural number, and as masculine, feminine or neuter gender, according to the context.

Signed this 19 day of May, 2025.

[Signature]
ROGER BASCHKE

[Signature]
ANN BASCHKE

STATE OF MINNESOTA
COUNTY OF NOBLES

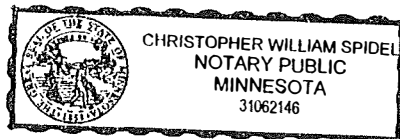
On this 19 day of May, 2025, before me, a Notary Public, personally appeared ROGER BASCHKE to me known to be the person named in and who executed the foregoing instrument and acknowledged that he/she/they executed the same as his/her/their voluntary act and deed.

[Signature]
Notary Public
Print Name: Chris Spidel

(Seal, if any)

My commission expires: 1-31-2028

STATE OF MINNESOTA
COUNTY OF NOBLES

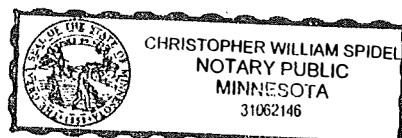


On this 19 day of May, 2025, before me, a Notary Public, personally appeared ANN BASCHKE, to me known to be the person named in and who executed the foregoing instrument and acknowledged that he/she/they executed the same as his/her/their voluntary act and deed.

[Signature]
Notary Public
Print Name: Chris Spidel

(Seal, if any)

My commission expires: 1-31-2028



Signed this ____ day of _____, 2025.

CITY OF WORTHINGTON

BY: _____

Printed Name:

Title:

STATE OF MINNESOTA

COUNTY OF NOBLES

On this ____ day of _____, 2025, before me, a Notary Public, personally appeared _____,
to me known to be the person named in and who executed the foregoing instrument and acknowledged
that he/she/they executed the same as his/her/their voluntary act and deed.

Notary Public
Print Name: _____

(Seal, if any)

My commission expires: _____

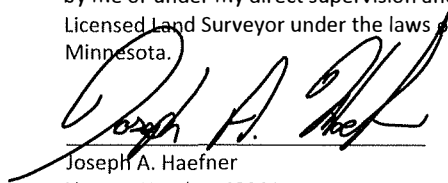
LEGAL DESCRIPTION

All that part of Lots 10, 11 and 12, Block 1, AUDITORS PLAT of BLOCKS 1, 2, 3, 4 & 5, SUNGOLD HEIGHTS in the City of Worthington according to the plat thereof on file and of record in the Office of the County Recorder, Nobles County, Minnesota described as follows:

Beginning at the Northeast corner of Lot 12, Block 1, said AUDITORS PLAT of BLOCKS 1, 2, 3, 4 & 5, SUNGOLD HEIGHTS, thence South 89 degrees 44 minutes 15 seconds West, along the north line of said Block 1, a distance of 148.71 feet; thence South 00 degrees 15 minutes 45 seconds East, a distance of 37.00 feet; thence southwesterly, a distance of 123.66 feet along a tangential circular curve concave northwesterly having a radius of 120.00 feet and a central angle of 59 degrees 02 minutes 30 seconds to the northerly line of a service road (said northerly line being a line lying parallel with and 50.00 feet northwesterly (as measured at right angles) of the northerly highway right of way line of Minnesota Trunk Highway No. 60 as defined by Minnesota Department of Transportation Right of Way Plat No. 53.46); thence northwesterly along said northerly line of Service Road, a distance of 250.2 feet, more or less, to the point of beginning.

SURVEYOR'S CERTIFICATION

I hereby certify that this survey, plan, or report was prepared by me or under my direct supervision and that I am a duly Licensed Land Surveyor under the laws of the State of Minnesota.


Joseph A. Haefner
License Number 43909

11/22/2024
Date

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SHEET 2 OF 2

SKETCH OF DESCRIPTION WORTHINGTON, MINNESOTA



**BOLTON
& MENK**

1501 SOUTH STATE STREET ET
SUITE 100
FAIRMONT, MN 56031
(507) 238-4738

PART LOTS 10, 11 AND 12, BLOCK 1
AUDITOR'S PLAT OF BLOCKS 1-5, SUNGOLD HEIGHTS
CITY OF WORTHINGTON, MINNESOTA

FOR: WORTHINGTON, CITY OF

JOB NUMBER: OF1134589

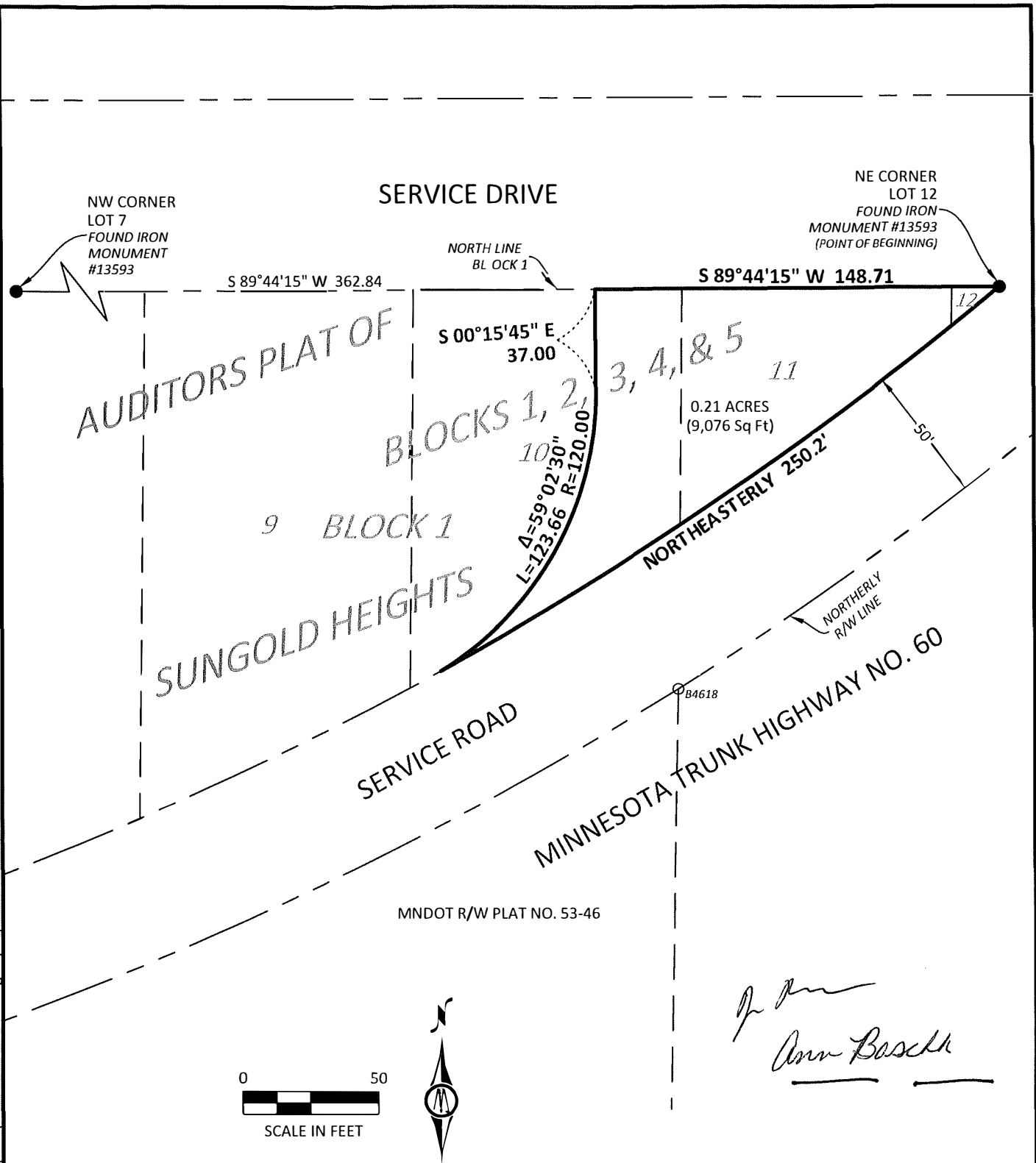
FIELD BOOK: SEE FILE

DRAWN BY: LJJ

Exhibit 5

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SHEET 1 OF 2

SKETCH OF DESCRIPTION
WORTHINGTON, MINNESOTA



**BOLTON
& MENK**

1501 SOUTH STATE STREET
SUITE 100
FAIRMONT, MN 56031
(507) 238-4738

PART LOTS 10, 11 AND 12, BLOCK 1
AUDITOR'S PLAT OF BLOCKS 1-5, SUNGOLD HEIGHTS
CITY OF WORTHINGTON, MINNESOTA

FOR: WORTHINGTON, CITY OF

JOB NUMBER: OF1134589

FIELD BOOK: SEE FILE

DRAWN BY: LIL

Exhibit 5

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
BOLTON & MENK INC	5/16/25	WWT FACILITY IMPROVEMENTS	MUNICIPAL WASTEWAT	FA PURIFY STRUCTURES	<u>13,090.50</u>
				TOTAL:	13,090.50
DEPARTMENT OF FINANCE	5/16/25	1996 HONDA CIVIC DRUG FORF	GENERAL FUND	NON-DEPARTMENTAL	<u>10.50</u>
				TOTAL:	10.50
DEPUTY REGISTER #33	5/16/25	2000 OLDSMOBILE INTRIGUE	GENERAL FUND	POLICE ADMINISTRATION	27.00
	5/16/25	2008 SATURN OUTLOOK 98J139	GENERAL FUND	POLICE ADMINISTRATION	27.00
	5/16/25	2010 AUDI A5	GENERAL FUND	POLICE ADMINISTRATION	<u>27.00</u>
				TOTAL:	81.00
ECHO GROUP INC	5/16/25	SCREW DRIVER	ELECTRIC	O-DISTR MISC	13.97
	5/16/25	#12 STRANDID WIRE	ELECTRIC	M-DISTR UNDERGRND LINE	<u>133.88</u>
				TOTAL:	147.85
FASTENAL COMPANY	5/16/25	RED/WHITE MARKING PAINT	GENERAL FUND	ENGINEERING ADMIN	221.24
	5/16/25	RED/WHITE MARKING PAINT	ELECTRIC	O-DISTR MISC	<u>737.47</u>
				TOTAL:	958.71
FERGUSON ENTERPRISES LLC #1657	5/16/25	PVC 3"	ELECTRIC	M-DISTR UNDERGRND LINE	<u>20.23</u>
				TOTAL:	20.23
FRONTIER COMMUNICATION SERVICES	5/16/25	PHONE SERVICE	WATER	O-PUMPING	24.06
	5/16/25	PHONE SERVICE	WATER	O-PURIFY MISC	32.23
	5/16/25	PHONE SERVICE	WATER	O-PURIFY MISC	19.43
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	27.23
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	27.61
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	32.23
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	32.23
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	19.43
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	24.55
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	32.23
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	32.23
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	31.06
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-PURIFY SUPERVISION	20.20
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-PURIFY MISC	31.22
	5/16/25	PHONE SERVICE	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	20.20
	5/16/25	PHONE SERVICE	ELECTRIC	O-SOURCE MISC	94.51
	5/16/25	PHONE SERVICE	ELECTRIC	O-DISTR STATION EXPENS	165.00
	5/16/25	PHONE SERVICE	ELECTRIC	ACCTS-RECORDS & COLLEC	<u>12.34</u>
				TOTAL:	682.99
GOPHER STATE ONE CALL	5/16/25	MONTHLY LOCATE SERVCIE	WATER	O-DISTR MISC	75.60
	5/16/25	MONTHLY LOCATE SERVCIE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	75.60
	5/16/25	MONTHLY LOCATE SERVCIE	ELECTRIC	O-DISTR MISC	<u>151.20</u>
				TOTAL:	302.40
JT SERVICES	5/16/25	4TH AVE P30 40K LIGHT FIXT	ELECTRIC	FA DISTR ST LITE & SIG	<u>38,760.00</u>
				TOTAL:	38,760.00
LEWIS & CLARK REGIONAL WATER SYSTEM IN	5/16/25	CAPACITY CHARGE 57,120,000	WATER	O-SOURCE MISC	36,556.80
	5/16/25	WATER RATE 45,400,000	WATER	O-SOURCE MISC	<u>33,142.00</u>
				TOTAL:	69,698.80
LOGAN BARBER	5/16/25	UMMA UNDERGROUND SCHOOL	ELECTRIC	O-DISTR MISC	<u>592.65</u>
				TOTAL:	592.65

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
MEDSURETY LLC	5/15/25	5/13/25 FSA-DC	GENERAL FUND	NON-DEPARTMENTAL	538.80
	5/15/25	5/16/25 DCAP-DD	GENERAL FUND	NON-DEPARTMENTAL	<u>624.62</u>
			TOTAL:		1,163.42
MINNESOTA ENERGY RESOURCES CORP	5/16/25	MONTHLY SERVICE	GENERAL FUND	GENERAL GOVT BUILDINGS	419.79
	5/16/25	MONTHLY SERVICE	GENERAL FUND	FIRE ADMINISTRATION	272.54
	5/16/25	MONTHLY SERVICE	GENERAL FUND	CENTER FOR ACTIVE LIVI	244.49
	5/16/25	MONTHLY SERVICE	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	667.07
	5/16/25	MONTHLY SERVICE	RECREATION	FIELD HOUSE	389.64
	5/16/25	MONTHLY SERVICE	WATER	O-DISTR MISC	58.49
	5/16/25	MONTHLY SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	58.49
	5/16/25	MONTHLY SERVICE	ELECTRIC	O-DISTR MISC	<u>116.97</u>
			TOTAL:		2,227.48
MISCELLANEOUS V ANDERSON, AUBREY BANEGAS, CHRISTY A BAUTISTA SURIANO, KANI CASTILLON GARCIA, ROBE CHAVEZ RAMOS, MOISES CHILEL LUCAS, GERMAN U GUIZAR CARDENAS, MITZI IXCOY CHAJ, ALVARO W JEAN FRANCIOS, MICHCA KEOVILAY, JASON KUTONO, SAIRIN MESFUN, FILMON K SIEVE, PRESLEY R SODERSTROM, GYPSY G SOTO, JESUS L WALU, JAMILA S SWIFT PORK COMPANY KREMER, ROB M	5/16/25	ELECTRIC OVERPAYMENT	ELECTRIC	NON-DEPARTMENTAL	197.19
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	19.20
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	56.76
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	65.84
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	292.66
	5/16/25	UTILITIY REFUND	ELECTRIC	NON-DEPARTMENTAL	25.92
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	44.23
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	12.82
	5/16/25	UTILTY REFUND	ELECTRIC	NON-DEPARTMENTAL	21.16
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	69.55
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	44.73
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	182.63
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	27.89
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	88.11
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	50.48
	5/16/25	UTILITY REFUND	ELECTRIC	NON-DEPARTMENTAL	24.02
	5/16/25	COMPRESSED AIR INCENTIVE	ELECTRIC	CUSTOMER INSTALL EXPEN	2,868.54
	5/16/25	GARBAGE REFUND	GARBAGE COLLECTION	NON-DEPARTMENTAL	<u>259.44</u>
			TOTAL:		4,351.17
MISSOURI RIVER ENERGY SERVICES	5/16/25	AMI METER COUNT FEE APRIL	WATER	ACCTS-METER READING	860.20
	5/16/25	AMI METER COUNT FEE APRIL	WATER	ACCTS-METER READING	50.00
	5/16/25	AMI METER COUNT FEE APRIL	ELECTRIC	ACCTS-METER READING	1,114.40
	5/16/25	AMI METER COUNT FEE APRIL	ELECTRIC	ACCTS-METER READING	<u>50.00</u>
			TOTAL:		2,074.60
MORRIS ELECTRONICS INC	5/16/25	TECHNICAL SUPPORT	WATER	ACCTS-RECORDS & COLLEC	393.50
	5/16/25	TECHNICAL SUPPORT	WATER	ACCTS-RECORDS & COLLEC	356.00
	5/16/25	TECHNICAL SUPPORT	WATER	ACCTS-RECORDS & COLLEC	12.50
	5/16/25	TECHNICAL SUPPORT	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	393.50
	5/16/25	TECHNICAL SUPPORT	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	356.00
	5/16/25	TECHNICAL SUPPORT	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	12.50
	5/16/25	TECHNICAL SUPPORT	ELECTRIC	ACCTS-RECORDS & COLLEC	787.00
	5/16/25	TECHNICAL SUPPORT	ELECTRIC	ACCTS-RECORDS & COLLEC	712.00
	5/16/25	TECHNICAL SUPPORT	ELECTRIC	ACCTS-RECORDS & COLLEC	<u>25.00</u>
			TOTAL:		3,048.00
NOBLES COUNTY ATTORNEY	5/16/25	1996 HONDA CIVIC WPD DRUG	GENERAL FUND	NON-DEPARTMENTAL	<u>21.00</u>
			TOTAL:		21.00
NOBLES COUNTY AUDITOR/TREASURER	5/15/25	25 1ST HALF TAXES 31-0486-	GENERAL FUND	ECONOMIC DEVELOPMENT	1,151.11
	5/15/25	25 1ST HALF TAXES 31-0100-	GENERAL FUND	GENERAL GOVT BUILDINGS	24.00

VENDOR	SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
		5/15/25	25 1ST HALF TAXES 31-0685-	GENERAL FUND	OTHER GEN GOVT MISC	48.00
		5/15/25	25 1ST HALF TAXES 31-3973-	GENERAL FUND	OTHER GEN GOVT MISC	24.00
		5/15/25	25 1ST HALF TAXES 31-0001-	GENERAL FUND	FIRE ADMINISTRATION	24.00
		5/15/25	25 1ST HALF TAXES 31-3934-	GENERAL FUND	FIRE ADMINISTRATION	24.00
		5/15/25	25 1ST HALF TAXES 31-3935-	GENERAL FUND	FIRE ADMINISTRATION	24.00
		5/15/25	25 1ST HALF TAXES 31-3936-	GENERAL FUND	FIRE ADMINISTRATION	24.00
		5/15/25	25 1ST HALF TAXES 31-0101-	GENERAL FUND	PAVED STREETS	340.00
		5/15/25	25 1ST HALF TAXES 31-0104-	GENERAL FUND	PAVED STREETS	88.00
		5/15/25	25 1ST HALF TAXES 31-0451-	GENERAL FUND	PAVED STREETS	24.00
		5/15/25	25 1ST HALF TAXES 31-3885-	GENERAL FUND	PAVED STREETS	24.00
		5/15/25	25 1ST HALF TAXES 31-3887-	GENERAL FUND	PAVED STREETS	24.00
		5/15/25	25 1ST HALF TAXES 31-3889-	GENERAL FUND	PAVED STREETS	24.00
		5/15/25	25 1ST HALF TAXES 31-0045-	GENERAL FUND	CENTER FOR ACTIVE LIVI	24.00
		5/15/25	25 1ST HALF TAXES 31-0045-	GENERAL FUND	CENTER FOR ACTIVE LIVI	24.00
		5/15/25	25 1ST HALF TAXES 31-0401-	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	24.00
		5/15/25	25 1ST HALF TAXES 31-3281-	EVENT CENTER	EVENT CENTER	24.00
		5/15/25	25 1ST HALF TAXES 31-0006-	RECREATION	FIELD HOUSE	24.00
		5/15/25	25 1ST HALF TAXES 31-3973-	RECREATION	FIELD HOUSE	24.00
		5/15/25	25 1ST HALF TAXES 31-0011-	RECREATION	FIELD HOUSE	24.00
		5/15/25	25 1ST HALF TAXES 31-0012-	RECREATION	FIELD HOUSE	24.00
		5/15/25	25 1ST HALF TAXES 31-0013-	RECREATION	FIELD HOUSE	24.00
		5/15/25	25 1ST HALF TAXES 31-0015-	RECREATION	FIELD HOUSE	24.00
		5/15/25	25 1ST HALF TAXES 31-1068-	RECREATION	THEATER	47,014.00
		5/15/25	25 1ST HALF TAXES 31-0934-	RECREATION	PARK AREAS	483.03
		5/15/25	25 1ST HALF TAXES 31-0061-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-2139-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-2320-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-2219-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-3576-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-3839-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-3925-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-3925-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-3974-	RECREATION	PARK AREAS	48.00
		5/15/25	25 1ST HALF TAXES 31-3977-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-3978-	RECREATION	PARK AREAS	24.00
		5/15/25	25 1ST HALF TAXES 31-3997-	RECREATION	OLSON PARK CAMPGROUND	24.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	TRAINING/TESTING CENTE	1,587.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	EDA	17,908.00
		5/15/25	25 1ST HALF TAXES 31-3974-	ECONOMIC DEV AUTHO	EDA	1,390.69
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	454.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	24.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	24.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	1,325.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	24.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	3,238.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	166.00
		5/15/25	25 1ST HALF TAXES 31-3786-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	24.00
		5/15/25	25 1ST HALF TAXES 31-3833-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	304.00
		5/15/25	25 1ST HALF TAXES 31-3849-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	24.00
		5/15/25	25 1ST HALF TAXES 31-3974-	ECONOMIC DEV AUTHO	MISC INDUSTRIAL DEVELO	1,761.31
		5/15/25	25 1ST HALF TAXES 31-1068-	ECONOMIC DEV AUTHO	MISC HOUSING DEVELOPME	24.00
		5/15/25	25 1ST HALF TAXES 31-1068-	ECONOMIC DEV AUTHO	MISC HOUSING DEVELOPME	24.00
		5/15/25	25 1ST HALF TAXES 31-1068-	ECONOMIC DEV AUTHO	MISC HOUSING DEVELOPME	24.00
		5/15/25	25 1ST HALF TAXES 31-1068-	ECONOMIC DEV AUTHO	MISC HOUSING DEVELOPME	24.00
		5/15/25	25 1ST HALF TAXES 31-1068-	ECONOMIC DEV AUTHO	MISC HOUSING DEVELOPME	24.00

[illegible]

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	31.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	31.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	30.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	29.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	33.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	35.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	32.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	30.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	866.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	42.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	22.00
	5/15/25	25 1ST HALF TAXES 31-1860-	ECONOMIC	DEV AUTHO MISC HOUSING DEVELOPME	30.00
	5/15/25	25 1ST HALF TAXES 31-3931-	CEMSTONE REDEVEL T	BUSINESS DEVELOPMENT	24.00
	5/15/25	25 1ST HALF TAXES 31-3932-	CEMSTONE REDEVEL T	BUSINESS DEVELOPMENT	24.00
	5/15/25	25 1ST HALF TAXES 31-0686-	WATER	O-PURIFY MISC	24.00
	5/15/25	25 1ST HALF TAXES 01-0239-	WATER	O-DISTR MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-0686-	WATER	O-DISTR MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-0957-	WATER	O-DISTR MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-3786-	WATER	O-DISTR MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-3949-	WATER	O-DISTR MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-1773-	WATER	O-DISTR MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-3821-	MUNICIPAL WASTEWAT	O-PURIFY MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-0045-	ELECTRIC	O-DISTR STATION EXPENS	24.00
	5/15/25	25 1ST HALF TAXES 31-3886-	ELECTRIC	O-DISTR STATION EXPENS	24.00
	5/15/25	25 1ST HALF TAXES 31-3786-	ELECTRIC	O-DISTR MISC	24.00
	5/15/25	25 1ST HALF TAXES 20-0178-	STORM WATER MANAGE	STORM DRAINAGE	29.68
	5/15/25	25 1ST HALF TAXES 31-1068-	STORM WATER MANAGE	STORM DRAINAGE	24.00
	5/15/25	25 1ST HALF TAXES 31-1240-	STORM WATER MANAGE	STORM DRAINAGE	24.00
	5/15/25	25 1ST HALF TAXES 31-1843-	STORM WATER MANAGE	STORM DRAINAGE	24.00
	5/15/25	25 1ST HALF TAXES 31-3669-	STORM WATER MANAGE	STORM DRAINAGE	24.00
	5/15/25	25 1ST HALF TAXES 31-9901-	STORM WATER MANAGE	STORM DRAINAGE	3,436.13
	5/15/25	25 1ST HALF TAXES 31-9901-	STORM WATER MANAGE	STORM DRAINAGE	34,993.65
	5/15/25	25 1ST HALF TAXES 31-9990-	STORM WATER MANAGE	STORM DRAINAGE	599.74
	5/15/25	25 1ST HALF TAXES 31-9990-	STORM WATER MANAGE	STORM DRAINAGE	349.96
	5/15/25	25 1ST HALF TAXES 31-3930-	STORM WATER MANAGE	STORM DRAINAGE	24.00
	5/15/25	25 1ST HALF TAXES 31-2586-	STORM WATER MANAGE	STORM DRAINAGE	63.00
	5/15/25	25 1ST HALF TAXES 31-3928-	STORM WATER MANAGE	STORM DRAINAGE	24.00
	5/15/25	25 1ST HALF TAXES 31-3882-	LIQUOR	O-GEN MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-3786-	AIRPORT	O-GEN MISC	3,301.00
	5/15/25	25 1ST HALF TAXES 31-3786-	AIRPORT	O-GEN MISC	2,194.00
	5/15/25	25 1ST HALF TAXES 31-3786-	AIRPORT	O-GEN MISC	1,065.00
	5/15/25	25 1ST HALF TAXES 31-3786-	AIRPORT	O-GEN MISC	1,439.00
	5/15/25	25 1ST HALF TAXES 31-3786-	AIRPORT	O-GEN MISC	248.00
	5/15/25	25 1ST HALF TAXES 31-3786-	AIRPORT	O-GEN MISC	666.00
	5/15/25	25 1ST HALF TAXES 31-3786-	AIRPORT	O-GEN MISC	1,029.00
	5/15/25	25 1ST HALF TAXES 31-3786-	AIRPORT	O-GEN MISC	666.00
	5/15/25	25 1ST HALF TAXES 31-3803-	AIRPORT	O-GEN MISC	95.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	634.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	1,002.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	860.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	1,152.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	1,451.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	704.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	1,108.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	878.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	364.00

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	1,512.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	220.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	1,739.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	1,048.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	3,445.00
	5/15/25	25 1ST HALF TAXES 31-3825-	AIRPORT	O-GEN MISC	1,774.00
	5/15/25	25 1ST HALF TAXES 31-3827-	AIRPORT	O-GEN MISC	2,339.00
	5/15/25	25 1ST HALF TAXES 31-4007-	AIRPORT	O-GEN MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-4016-	AIRPORT	O-GEN MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-4017-	AIRPORT	O-GEN MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-4018-	AIRPORT	O-GEN MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-4019-	AIRPORT	O-GEN MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-4020-	AIRPORT	O-GEN MISC	24.00
	5/15/25	25 1ST HALF TAXES 31-4021-	AIRPORT	O-GEN MISC	601.00
	5/15/25	25 1ST HALF TAXES 31-4022-	AIRPORT	O-GEN MISC	24.00
				TOTAL:	152,734.30
NOBLES COUNTY PUBLIC WORKS	5/16/25	MARCH FUEL	GENERAL FUND	ENGINEERING ADMIN	28.32
	5/16/25	MARCH FUEL	GENERAL FUND	ECONOMIC DEVELOPMENT	154.64
	5/16/25	MARCH FUEL	GENERAL FUND	POLICE ADMINISTRATION	4,334.26
	5/16/25	MARCH FUEL	GENERAL FUND	FIRE ADMINISTRATION	229.98
	5/16/25	MARCH FUEL	GENERAL FUND	CIVIL DEFENSE ADMIN	43.74
	5/16/25	MARCH FUEL	GENERAL FUND	PAVED STREETS	288.82
	5/16/25	MARCH FUEL	GENERAL FUND	ICE AND SNOW REMOVAL	1,036.27
	5/16/25	MARCH FUEL	RECREATION	SOCCER COMPLEX	91.51
	5/16/25	MARCH FUEL	RECREATION	SOCCER COMPLEX	131.26
	5/16/25	MARCH FUEL	RECREATION	PARK AREAS	799.56
	5/16/25	MARCH FUEL	WATER	O-PUMPING	123.42
	5/16/25	MARCH FUEL	WATER	M-TRANS MAINS	651.27
	5/16/25	MARCH FUEL	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	76.20
	5/16/25	MARCH FUEL	MUNICIPAL WASTEWAT	O-PURIFY SUPERVISION	47.36
	5/16/25	MARCH FUEL	MUNICIPAL WASTEWAT	O-PURIFY MISC	125.45
	5/16/25	MARCH FUEL	MUNICIPAL WASTEWAT	O-PURIFY MISC	52.41
	5/16/25	MARCH FUEL	MUNICIPAL WASTEWAT	O-PURIFY MISC	93.94
	5/16/25	MARCH FUEL	MUNICIPAL WASTEWAT	M-SOURCE MAINS & LIFTS	102.95
	5/16/25	MARCH FUEL	MUNICIPAL WASTEWAT	M-SOURCE MAINS & LIFTS	84.91
	5/16/25	MARCH FUEL	MUNICIPAL WASTEWAT	M-SOURCE MAINS & LIFTS	223.81
	5/16/25	MARCH FUEL	ELECTRIC	O-DISTR UNDERGRND LINE	662.17
	5/16/25	MARCH FUEL	STORM WATER MANAGE	STORM DRAINAGE	76.21
	5/16/25	MARCH FUEL	STORM WATER MANAGE	STREET CLEANING	509.65
				TOTAL:	9,968.11
ONE OFFICE SOLUTION-WOUTIL	5/16/25	MISC SUPPLIES	WATER	ACCTS-RECORDS & COLLEC	7.71
	5/16/25	MISC SUPPLIES	WATER	ACCTS-RECORDS & COLLEC	19.23
	5/16/25	MISC SUPPLIES	WATER	ACCTS-RECORDS & COLLEC	41.50
	5/16/25	MISC SUPPLIES	WATER	ACCTS-RECORDS & COLLEC	202.18
	5/16/25	MISC SUPPLIES	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	7.71
	5/16/25	MISC SUPPLIES	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	19.23
	5/16/25	MISC SUPPLIES	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	41.50
	5/16/25	MISC SUPPLIES	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	202.18
	5/16/25	MISC SUPPLIES	ELECTRIC	ACCTS-RECORDS & COLLEC	15.42
	5/16/25	MISC SUPPLIES	ELECTRIC	ACCTS-RECORDS & COLLEC	38.45
	5/16/25	MISC SUPPLIES	ELECTRIC	ACCTS-RECORDS & COLLEC	82.99
	5/16/25	MISC SUPPLIES	ELECTRIC	ACCTS-RECORDS & COLLEC	404.39
	5/16/25	COPY MACHINE	ELECTRIC	FA OFFICE FURN & EQUIP	9,989.00

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
				TOTAL:	11,071.49
REFSLAND JACOB	5/16/25	BOOTS DANNER	GENERAL FUND	POLICE ADMINISTRATION	198.00
				TOTAL:	198.00
STREICHER'S INC	5/16/25	DETECTIVE HOLSTERS	GENERAL FUND	POLICE ADMINISTRATION	141.00
	5/16/25	PATROL HOLSTERS	GENERAL FUND	POLICE ADMINISTRATION	198.99
				TOTAL:	339.99
TWO WAY SOLUTIONS INC	5/16/25	RADIO INSTALL NEW 100	ELECTRIC	O-DISTR MISC	382.56
				TOTAL:	382.56
VERIZON WIRELESS	5/16/25	CELL PHONE WPD	GENERAL FUND	POLICE ADMINISTRATION	707.88
	5/16/25	LAPTOP AIR CARDS WPD	GENERAL FUND	POLICE ADMINISTRATION	784.21
	5/16/25	CELL PHONE SHARED	GENERAL FUND	SECURITY CENTER	41.64
	5/16/25	CELL PHONE SHARED	GENERAL FUND	SECURITY CENTER	41.64
	5/16/25	CELL PHONE ANIMAL CONTROL	GENERAL FUND	ANIMAL CONTROL ENFORCE	41.64
	5/16/25	LAPTOP AIR CARDS ANIMAL CO	GENERAL FUND	ANIMAL CONTROL ENFORCE	35.01
	5/16/25	CELL PHONE CSO	GENERAL FUND	CODE ENFORCEMENT	41.64
	5/16/25	LAPTOP AIR CARDS CSO	GENERAL FUND	CODE ENFORCEMENT	35.01
				TOTAL:	1,728.67
YMCA	5/16/25	JANUARY AQUATIC DIR REIMBU	RECREATION	AQUATIC CENTER FACILIT	2,268.91
	5/16/25	FEBRUARY AQUATIC DIR REIMB	RECREATION	AQUATIC CENTER FACILIT	2,268.91
	5/16/25	MARCH AQUATIC DIR REIMBURS	RECREATION	AQUATIC CENTER FACILIT	2,256.41
	5/16/25	APRIL AQUATIC DIR REIMBURS	RECREATION	AQUATIC CENTER FACILIT	2,256.18
				TOTAL:	9,050.41

===== FUND TOTALS =====

101	GENERAL FUND	12,731.78
202	MEMORIAL AUDITORIUM	691.07
214	EVENT CENTER	24.00
229	RECREATION	58,415.41
231	ECONOMIC DEV AUTHORITY	31,140.00
437	CEMSTONE REDEVEL TIF#19	48.00
601	WATER	72,794.12
602	MUNICIPAL WASTEWATER	15,423.66
604	ELECTRIC	59,225.33
606	STORM WATER MANAGEMENT	40,202.02
609	LIQUOR	24.00
612	AIRPORT	31,726.00
873	GARBAGE COLLECTION	259.44

GRAND TOTAL: 322,704.83

PACKET: 05503 MEDSURETY
VENDOR SET: 01 CITY OF WORTHINGTON *** DRAFT/OTHER LISTING ***
BANK: 1 FIRST STATE BANK SOUTHWES

VENDOR	I.D.	NAME	ITEM	PAID	DISCOUNT	AMOUNT	ITEM	ITEM
			TYPE	DATE			NO#	AMOUNT

M00512		MEDSURETY LLC	D	5/20/2025			002568	251.89

* * B A N K T O T A L S * *		NO#	DISCOUNTS	CHECK AMT	TOTAL APPLIED
REGULAR CHECKS:		0	0.00	0.00	0.00
HANDWRITTEN CHECKS:		0	0.00	0.00	0.00
PRE-WRITE CHECKS:		0	0.00	0.00	0.00
DRAFTS:		1	0.00	251.89	251.89
VOID CHECKS:		0	0.00	0.00	0.00
NON CHECKS:		0	0.00	0.00	0.00
CORRECTIONS:		0	0.00	0.00	0.00
BANK TOTALS:		1	0.00	251.89	251.89

PACKET: 05509 PAYROLL 5/23/25 - 9

VENDOR SET: 01 CITY OF WORTHINGTON *** DRAFT/OTHER LISTING ***

BANK: 1 FIRST STATE BANK SOUTHWES

VENDOR	I.D.	NAME	ITEM TYPE	PAID DATE	DISCOUNT	AMOUNT	ITEM NO#	ITEM AMOUNT

C00333		COLONIAL LIFE & ACCIDENT INSURAD		5/28/2025			002569	4,385.72
D00173		DEFERRED COMP- MINNESOTA STATE D		5/28/2025			002570	8,372.53
E00088		EFTPS	D	5/28/2025			002571	75,560.43
M00512		MEDSURETY LLC	D	5/28/2025			002572	14,787.03
M00229		MINNESOTA REVENUE	D	5/28/2025			002573	446.39
M00309		MINNESOTA STATE RETIREMENT SYSTD		5/28/2025			002574	2,470.00
N00316		LIFE INSURANCE OF NORTH AMERICAD		5/28/2025			002575	4,004.46
P00039		PUBLIC EMPLOYEES RETIREMENT ASSD		5/28/2025			002576	62,456.80
S00202		STATE OF MINNESOTA DEPT OF REVED		5/28/2025			002577	15,600.41

* * B A N K T O T A L S * *	NO#	DISCOUNTS	CHECK AMT	TOTAL APPLIED
REGULAR CHECKS:	0	0.00	0.00	0.00
HANDWRITTEN CHECKS:	0	0.00	0.00	0.00
PRE-WRITE CHECKS:	0	0.00	0.00	0.00
DRAFTS:	9	0.00	188,083.77	188,083.77
VOID CHECKS:	0	0.00	0.00	0.00
NON CHECKS:	0	0.00	0.00	0.00
CORRECTIONS:	0	0.00	0.00	0.00
BANK TOTALS:	9	0.00	188,083.77	188,083.77

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
A & B BUSINESS SOLUTIONS	5/23/25	COPIER SERVICE	GENERAL FUND	ENGINEERING ADMIN	241.04
	5/23/25	COPIER SERVICE	GENERAL FUND	ECONOMIC DEVELOPMENT	<u>241.04</u>
		TOTAL:			482.08
AARON PAVELKO	5/23/25	REIMBURSEMENT FOR DOT TANK ELECTRIC		O-DISTR MISC	2.50
	5/23/25	REIMBURSEMENT FOR DOT TANK ELECTRIC		O-DISTR MISC	<u>41.00</u>
		TOTAL:			43.50
ACCESS HEALTH WORTHINGTON	5/23/25	CDL DRUG SCREENING	GENERAL FUND	PAVED STREETS	<u>35.00</u>
		TOTAL:			35.00
ALPHA MEDIA LLC	5/23/25	ADVERTISING	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	<u>300.00</u>
		TOTAL:			300.00
AMERITAS	5/23/25	DENTAL INSURANCE-ADJUSTED	GENERAL FUND	NON-DEPARTMENTAL	503.83
	5/23/25	DENTAL INSURANCE	GENERAL FUND	NON-DEPARTMENTAL	496.67
	5/23/25	VISION INSURANCE	GENERAL FUND	NON-DEPARTMENTAL	47.00
	5/23/25	VISION INSURANCE	GENERAL FUND	NON-DEPARTMENTAL	47.04
	5/23/25	DENTAL INSURANCE	MEMORIAL AUDITORIUM	NON-DEPARTMENTAL	18.18
	5/23/25	DENTAL INSURANCE	MEMORIAL AUDITORIUM	NON-DEPARTMENTAL	10.97
	5/23/25	VISION INSURANCE	MEMORIAL AUDITORIUM	NON-DEPARTMENTAL	3.68
	5/23/25	VISION INSURANCE	MEMORIAL AUDITORIUM	NON-DEPARTMENTAL	3.68
	5/23/25	DENTAL INSURANCE	RECREATION	NON-DEPARTMENTAL	18.18
	5/23/25	VISION INSURANCE	RECREATION	NON-DEPARTMENTAL	3.68
	5/23/25	VISION INSURANCE	RECREATION	NON-DEPARTMENTAL	3.68
	5/23/25	VISION INSURANCE	IMPROVEMENT CONST	NON-DEPARTMENTAL	9.66
	5/23/25	VISION INSURANCE	IMPROVEMENT CONST	NON-DEPARTMENTAL	9.26
	5/23/25	DENTAL INSURANCE	WATER	NON-DEPARTMENTAL	84.85
	5/23/25	VISION INSURANCE	WATER	NON-DEPARTMENTAL	0.07
	5/23/25	VISION INSURANCE	WATER	NON-DEPARTMENTAL	0.28
	5/23/25	DENTAL INSURANCE	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	51.61
	5/23/25	VISION INSURANCE	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	6.70
	5/23/25	VISION INSURANCE	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	6.98
	5/23/25	DENTAL INSURANCE	ELECTRIC	NON-DEPARTMENTAL	154.82
	5/23/25	DENTAL INSURANCE	ELECTRIC	NON-DEPARTMENTAL	154.82
	5/23/25	DENTAL INSURANCE	LIQUOR	NON-DEPARTMENTAL	102.94
	5/23/25	DENTAL INSURANCE	LIQUOR	NON-DEPARTMENTAL	102.94
	5/23/25	VISION INSURANCE	LIQUOR	NON-DEPARTMENTAL	7.36
	5/23/25	VISION INSURANCE	LIQUOR	NON-DEPARTMENTAL	7.36
	5/23/25	DENTAL INSURANCE	DATA PROCESSING	NON-DEPARTMENTAL	55.03
	5/23/25	VISION INSURANCE	DATA PROCESSING	NON-DEPARTMENTAL	3.17
	5/23/25	VISION INSURANCE	DATA PROCESSING	NON-DEPARTMENTAL	<u>3.04</u>
		TOTAL:			1,917.48
ARTIC GLACIER U.S.A., INC.	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	332.88
	5/23/25	MIX	LIQUOR	O-SOURCE MISC	<u>17.50</u>
		TOTAL:			350.38
ARTISAN BEER COMPANY	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	333.10
	5/23/25	THC	LIQUOR	NON-DEPARTMENTAL	46.20
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	<u>273.05</u>
		TOTAL:			652.35
BAYCOM, INC	5/23/25	MAGNETIC PHONE HOLDER	GENERAL FUND	POLICE ADMINISTRATION	<u>80.00</u>
		TOTAL:			80.00

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
BEDFORD INDUSTRIES INC	5/23/25	USED FLOOR SCRUBBER	GENERAL FUND	PAVED STREETS	1,500.00
	5/23/25	PITNEY BOWES SUPPLIES	WATER	ACCTS-RECORDS & COLLEC	50.00
	5/23/25	PITNEY BOWES SUPPLIES	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	50.00
	5/23/25	PITNEY BOWES SUPPLIES	ELECTRIC	ACCTS-RECORDS & COLLEC	<u>100.00</u>
				TOTAL:	1,700.00
BENTS TRUCKING	5/23/25	PEA ROCK	ELECTRIC	M-DISTR UNDERGRND LINE	<u>614.07</u>
				TOTAL:	614.07
BEVERAGE WHOLESALERS INC	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	16,118.05
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	19,556.15
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	17,896.75
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	<u>24,496.70</u>
				TOTAL:	78,067.65
BILLION AUTOMOTIVE	5/23/25	SQUAD #18-37 OIL CHANGE	GENERAL FUND	POLICE ADMINISTRATION	50.00
	5/23/25	SQUAD #18-37 OIL CHANGE	GENERAL FUND	POLICE ADMINISTRATION	20.00
	5/23/25	RAM REPAIR	RECREATION	BALLFIELD MAINTENANCE	<u>1,479.74</u>
				TOTAL:	1,549.74
BREAKTHRU BEVERAGE MINNESOTA BEER LLC	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	1,108.00
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	8,516.81
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	264.00
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	72.00
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	2,844.14
	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	325.72
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	1,340.00
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	103.60
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	5.55
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	60.28
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	18.50
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	<u>24.05</u>
				TOTAL:	14,682.65
MARK ROBERT BRODIN	5/23/25	A/V WORK	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	890.00
	5/23/25	A/V WORK	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	710.00
	5/23/25	A/V WORK	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	2,640.00
	5/23/25	A/V WORK	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	<u>2,370.00</u>
				TOTAL:	6,610.00
BUFFALO RIDGE CONCRETE INC	5/23/25	CHATAUQUA BENCHES	RECREATION	PARK AREAS	<u>615.00</u>
				TOTAL:	615.00
CAPITAL ONE	5/23/25	THERMOMETER	GENERAL FUND	POLICE ADMINISTRATION	6.44
	5/23/25	SQUAD CLEANING SUPPLIES	GENERAL FUND	POLICE ADMINISTRATION	57.32
	5/23/25	STICK 4K	GENERAL FUND	CENTER FOR ACTIVE LIVI	49.00
	5/23/25	MISC SUPPLIES	GENERAL FUND	CENTER FOR ACTIVE LIVI	70.44
	5/23/25	MISC SUPPLIES	GENERAL FUND	CENTER FOR ACTIVE LIVI	28.09
	5/23/25	CLEANING SUPPLIES	GENERAL FUND	CENTER FOR ACTIVE LIVI	42.73
	5/23/25	CLEANING SUPPLIES	GENERAL FUND	CENTER FOR ACTIVE LIVI	65.97
	5/23/25	MISC SUPPLIES	WATER	O-PURIFY MISC	64.44
	5/23/25	MISC SUPPLIES	WATER	O-DISTR MISC	<u>21.45</u>
				TOTAL:	405.88
CINTAS CORP	5/23/25	MATS	GENERAL FUND	GENERAL GOVT BUILDINGS	52.00
	5/23/25	MATS	GENERAL FUND	GENERAL GOVT BUILDINGS	52.00

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
				TOTAL:	104.00
CNH INDUSTRIAL ACCOUNTS	5/23/25	SKID LOADER COUPLER	GENERAL FUND	PAVED STREETS	146.91
	5/23/25	SKID LOADER HYDRO LEAK	GENERAL FUND	PAVED STREETS	368.56
	5/23/25	SKID LOADER HYDRO LEAK	GENERAL FUND	PAVED STREETS	914.18
	5/23/25	FINANCE CHARGE	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	12.52
	5/23/25	WEST TEST SHIPPING	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	23.69
	5/23/25	WHOLE EFFLUENT TOXICITY	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	21.56
	5/23/25	WET TEST SHIPPING	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	21.48
	5/23/25	WET TEST SHIPPING	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	22.56
	5/23/25	WINTER TRACTOR RENTAL	MUNICIPAL WASTEWAT	M-PURIFY EQUIPMENT	<u>1,083.50</u>
				TOTAL:	2,614.96
COLUMN SOFTWARE PBC	5/23/25	ORDINANCE #1214	GENERAL FUND	CLERK'S OFFICE	549.76
	5/23/25	PUBLIC MEETING STORM WATER	GENERAL FUND	ENGINEERING ADMIN	68.80
	5/23/25	BID NOTICE RAY DRIVE	GENERAL FUND	ENGINEERING ADMIN	177.52
	5/23/25	JUNE PLANNING COMMISSION	GENERAL FUND	ECONOMIC DEVELOPMENT	94.69
	5/23/25	BUSINESS SUBSIDY POLICY NO	GENERAL FUND	ECONOMIC DEVELOPMENT	<u>52.77</u>
				TOTAL:	943.54
COOPERATIVE ENERGY CO- ACCT # 5910807	5/23/25	SAW GAS	RECREATION	TREE REMOVAL	9.00
	5/23/25	GAS	RECREATION	TREE REMOVAL	42.78
	5/23/25	VACTOR SEWERS	STORM WATER MANAGE	STORM DRAINAGE	220.85
	5/23/25	GAS	STORM WATER MANAGE	STORM DRAINAGE	218.33
	5/23/25	GAS	STORM WATER MANAGE	STORM DRAINAGE	141.01
	5/23/25	GAS	STORM WATER MANAGE	STORM DRAINAGE	219.64
	5/23/25	GAS	STORM WATER MANAGE	STORM DRAINAGE	161.01
	5/23/25	GAS	STORM WATER MANAGE	STORM DRAINAGE	146.49
	5/23/25	GAS	STORM WATER MANAGE	STORM DRAINAGE	211.59
	5/23/25	GAS	STORM WATER MANAGE	STORM DRAINAGE	181.38
	5/23/25	GAS	STORM WATER MANAGE	STORM DRAINAGE	<u>208.29</u>
				TOTAL:	1,760.37
CORE & MAIN LP	5/23/25	SERVICE LINE CURBSTOB REPA	WATER	O-DIST UNDERGRND LINES	<u>195.09</u>
				TOTAL:	195.09
CORE ELECTRIC	5/23/25	INDOOR POOL HANGING LIGHTS	AQUATIC CENTER FAC	AQUATIC CENTER FACILIT	<u>15,683.93</u>
				TOTAL:	15,683.93
CRYE PRECISION LLC	5/23/25	TACTICAL VEST & EQUIPMENT	GENERAL FUND	POLICE ADMINISTRATION	<u>11,711.50</u>
				TOTAL:	11,711.50
CULLIGAN OF WORTHINGTON	5/23/25	MONTHLY SERVICE	ELECTRIC	ACCTS-RECORDS & COLLEC	<u>41.32</u>
				TOTAL:	41.32
CUSTOMIZED FIRE RESCUE TRAININIG INC.	5/23/25	CLASS SCBA	GENERAL FUND	FIRE ADMINISTRATION	<u>1,800.00</u>
				TOTAL:	1,800.00
DAKOTA SUPPLY GROUP INC	5/23/25	CURB BOX REPAIR LTD	WATER	O-DIST UNDERGRND LINES	<u>549.28</u>
				TOTAL:	549.28
DAVID ROSS	5/23/25	CDL CLASS AND TEST	WATER	O-DISTR MISC	<u>529.68</u>
				TOTAL:	529.68
DCL AUTO PARTS INC	5/23/25	ZIP TIES	GENERAL FUND	PAVED STREETS	<u>35.78</u>
				TOTAL:	35.78

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
DEPARTMENT OF FINANCE	5/23/25	2008 HONDA FEL FLEE IN MV	GENERAL FUND	NON-DEPARTMENTAL	<u>93.00</u>
				TOTAL:	93.00
DEPARTMENT OF TRANSPORTATION	5/23/25	HANGER LOAN REPAYMENT	AIRPORT	NON-DEPARTMENTAL	<u>920.00</u>
				TOTAL:	920.00
DEPUTY REGISTER #33	5/23/25	TAB RENEWAL 1C6RR7FT6KS663	GENERAL FUND	POLICE ADMINISTRATION	<u>16.25</u>
				TOTAL:	16.25
DGR ENGINEERING	5/23/25	NEVI- TRANSMISSION RATINGS	ELECTRIC	O-DISTR STATION EXPENS	500.00
	5/23/25	SUB 3	ELECTRIC	O-DISTR STATION EXPENS	105.50
	5/23/25	NEVI- TRANSMISSION RATINGS	ELECTRIC	O-DISTR MISC	603.50
	5/23/25	SUBSTATION #2	ELECTRIC	FA TRANS MAINS	<u>58.50</u>
				TOTAL:	1,267.50
DIAMOND VOGEL, INC	5/23/25	PAINT SUPPLIES	GENERAL FUND	CENTER FOR ACTIVE LIVI	66.75
	5/23/25	TAPE	RECREATION	PARK AREAS	<u>7.95</u>
				TOTAL:	74.70
DISTRICT 518 COMMUNITY EDUCATION	5/23/25	SUMMER CATALOG ADS	GENERAL FUND	CENTER FOR ACTIVE LIVI	<u>300.00</u>
				TOTAL:	300.00
DOLL DISTRIBUTING LLC	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	394.75-
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	698.60
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	189.00
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	16,209.85
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	156.00
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	11,320.25
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	4,159.45
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	51.20
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	188.78-
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	2,161.80
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	728.60
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	12,997.10
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	403.55
	5/23/25	BEER	LIQUOR	NON-DEPARTMENTAL	<u>549.00-</u>
				TOTAL:	47,942.87
ECHO GROUP INC	5/23/25	SCREW DRIVER	ELECTRIC	O-DISTR MISC	<u>16.42</u>
				TOTAL:	16.42
FASTENAL COMPANY	5/23/25	BULK BIN	RECREATION	SOCCER COMPLEX	9.32
	5/23/25	BOLTS	RECREATION	SOCCER COMPLEX	32.34
	5/23/25	BULK BIN	RECREATION	SOCCER COMPLEX	14.60
	5/23/25	SAFETY GLASSES	RECREATION	PARK AREAS	62.52
	5/23/25	GLOVES	RECREATION	PARK AREAS	<u>32.37</u>
				TOTAL:	151.15
FERGUSON ENTERPRISES LLC #1657	5/23/25	PVC CMNT	ELECTRIC	M-DISTR UNDERGRND LINE	<u>247.98</u>
				TOTAL:	247.98
FLAGSHIP RECREATION LLC	5/23/25	WELCOME SIGN	RECREATION	PARK AREAS	<u>1,934.65</u>
				TOTAL:	1,934.65
FORUM COMMUNICATIONS COMPANY	5/23/25	COMMUNITY GUIDE AD	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	<u>1,177.00</u>
				TOTAL:	1,177.00

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
FRONTIER COMMUNICATION SERVICES	5/23/25	PHONE SERVICE	MUNICIPAL WASTEWAT	O-PURIFY MISC	<u>83.22</u>
				TOTAL:	83.22
GREATAMERICA FINANCIAL SERVICES CORPOR	5/23/25	MONTHLY SERVICE	GENERAL FUND	PAVED STREETS	49.45
	5/23/25	MONTHLY SERVICE	RECREATION	PARK AREAS	49.45
	5/23/25	MONTHLY SERVICE	STORM WATER MANAGE	STORM DRAINAGE	<u>49.45</u>
				TOTAL:	148.35
GUARANTEE ROOFING & SHEET METAL OF SD	5/23/25	SERVICE CALL	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	<u>302.87</u>
				TOTAL:	302.87
HARVEYS UPHOLSTERY	5/23/25	SAIL SPLASH PAD	RECREATION	SWIMMING BEACHES	<u>200.00</u>
				TOTAL:	200.00
HEARTLAND TIRE INC	5/23/25	MOWER TIRE	RECREATION	PARK AREAS	40.25
	5/23/25	MOWER TIRE	RECREATION	PARK AREAS	25.00
	5/23/25	MOWER TIRE	RECREATION	PARK AREAS	4.32
	5/23/25	MOWER TIRE	RECREATION	PARK AREAS	<u>32.00</u>
				TOTAL:	101.57
HERITAGE PROFESSIONAL PRODUCTS GROUP	5/23/25	FERTILIZER	RECREATION	SOCCER COMPLEX	<u>1,983.15</u>
				TOTAL:	1,983.15
HILLYARD/SIOUX FALLS	5/23/25	RUG TILES FOR GYM FLOOR	GENERAL FUND	CENTER FOR ACTIVE LIVI	<u>470.00</u>
				TOTAL:	470.00
HOPE HAVEN INC	5/23/25	LATH PAINTED	GENERAL FUND	ENGINEERING ADMIN	447.00
	5/23/25	CLEANING SERVICES	GENERAL FUND	CENTER FOR ACTIVE LIVI	<u>541.67</u>
				TOTAL:	988.67
HY-VEE INC-61609 (UTILITIES)	5/23/25	MISC SUPPLIES	WATER	ACCTS-RECORDS & COLLEC	7.20
	5/23/25	CLEANING SUPPLIES	MUNICIPAL WASTEWAT	O-PURIFY MISC	5.39
	5/23/25	TOLIET PAPER	MUNICIPAL WASTEWAT	O-PURIFY MISC	43.10
	5/23/25	MISC SUPPLIES	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	7.20
	5/23/25	MISC SUPPLIES	ELECTRIC	ACCTS-RECORDS & COLLEC	<u>14.39</u>
				TOTAL:	77.28
HY-VEE INC-61705	5/23/25	DONUTS	GENERAL FUND	ECONOMIC DEVELOPMENT	39.98
	5/23/25	BAKERY ITEMS	GENERAL FUND	ECONOMIC DEVELOPMENT	<u>51.90</u>
				TOTAL:	91.88
HYDRAULIC SOLUTIONS	5/23/25	CASE TRACTOR HYD LINE	RECREATION	PARK AREAS	<u>63.58</u>
				TOTAL:	63.58
INDEPENDENT SCHOOL DISTRICT #518	5/23/25	INDEPENDENT SCHOOL DISTRIC	CABLE TELEVISION	CABLE	<u>2,817.29</u>
				TOTAL:	2,817.29
INTL UNION LOCAL #49	5/23/25	UNION DUES	GENERAL FUND	NON-DEPARTMENTAL	80.19
	5/23/25	UNION DUES	GENERAL FUND	NON-DEPARTMENTAL	106.64
	5/23/25	UNION DUES	RECREATION	NON-DEPARTMENTAL	83.43
	5/23/25	UNION DUES	RECREATION	NON-DEPARTMENTAL	74.05
	5/23/25	UNION DUES	IMPROVEMENT CONST	NON-DEPARTMENTAL	24.37
	5/23/25	UNION DUES	IMPROVEMENT CONST	NON-DEPARTMENTAL	33.68
	5/23/25	UNION DUES	WATER	NON-DEPARTMENTAL	87.68
	5/23/25	UNION DUES	WATER	NON-DEPARTMENTAL	87.50
	5/23/25	UNION DUES	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	118.40

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
	5/23/25	UNION DUES	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	121.02
	5/23/25	UNION DUES	STORM WATER MANAGE	NON-DEPARTMENTAL	43.43
	5/23/25	UNION DUES	STORM WATER MANAGE	NON-DEPARTMENTAL	<u>14.61</u>
			TOTAL:		875.00
IVAN MARTINEZ	5/23/25	TRAINING	GENERAL FUND	POLICE ADMINISTRATION	<u>143.96</u>
			TOTAL:		143.96
JANITOR'S CLOSET	5/23/25	10TH STREET TRASH BAGS	RECREATION	PARK AREAS	<u>240.48</u>
			TOTAL:		240.48
JOHNSON BROTHERS LIQUOR CO	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	4,121.00
	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	77.00
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	2,427.13
	5/23/25	THC	LIQUOR	NON-DEPARTMENTAL	478.75
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	3,116.52
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	2,246.35
	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	103.90
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	85.50
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	66.67
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	44.33
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	<u>74.00</u>
			TOTAL:		12,841.15
JSA SERVICES INC	5/23/25	TOWELS AND CLEANER	GENERAL FUND	CENTER FOR ACTIVE LIVI	<u>263.67</u>
			TOTAL:		263.67
JT SERVICES	5/23/25	4TH AVE ITEM 1-LIGHT	ELECTRIC	FA DISTR ST LITE & SIG	69,570.00
	5/23/25	P40 FIXTURES	ELECTRIC	FA DISTR ST LITE & SIG	<u>13,640.00</u>
			TOTAL:		83,210.00
KARL'S TV & APPLIANCE INC	5/23/25	REFRIDGERATOR	DATA PROCESSING	DATA PROCESSING	<u>625.49</u>
			TOTAL:		625.49
SIDDNEY KOUNLABOUT	5/23/25	TRAINING	GENERAL FUND	POLICE ADMINISTRATION	<u>104.27</u>
			TOTAL:		104.27
LAMPERTS YARDS INC-2600013	5/23/25	TAPE MEASURE	GENERAL FUND	ECONOMIC DEVELOPMENT	16.99
	5/23/25	DECKING WATER WORLD	RECREATION	AQUATIC CENTER FACILIT	155.56
	5/23/25	CEDAR PLANTS	RECREATION	AQUATIC CENTER FACILIT	<u>123.76</u>
			TOTAL:		296.31
LAW ENF LABOR SERV INC #4	5/23/25	UNION DUES LICENSED	GENERAL FUND	NON-DEPARTMENTAL	620.50
	5/23/25	UNION DUES LICENSED	GENERAL FUND	NON-DEPARTMENTAL	620.50
	5/23/25	UNION DUES NON-LICENSED	GENERAL FUND	NON-DEPARTMENTAL	193.98
	5/23/25	UNION DUES NON-LICENSED	GENERAL FUND	NON-DEPARTMENTAL	<u>193.98</u>
			TOTAL:		1,628.96
LOGAN BARBER	5/23/25	REIMBURSEMENT FOR DOT TEST	ELECTRIC	O-DISTR MISC	2.50
	5/23/25	REIMBURSEMENT FOR DOT TANK	ELECTRIC	O-DISTR MISC	<u>46.00</u>
			TOTAL:		48.50
MC GLASS & KEY INC	5/23/25	OLSON LOCK	RECREATION	OLSON PARK CAMPGROUND	<u>21.04</u>
			TOTAL:		21.04
MIDWAY FORD	5/23/25	2025 FORD F250 XL	WATER	FA TRANSPORTATION EQUI	50,734.85

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
				TOTAL:	50,734.85
MIDWEST ALARM COMPANY INC	5/23/25	SERVICE CALL	GENERAL FUND	GENERAL GOVT BUILDINGS	561.25
				TOTAL:	561.25
MIDWEST CONCRETE CUTTING INC	5/23/25	CONCRETE GRINDING SEALING	RECREATION	PARK AREAS	16,120.00
				TOTAL:	16,120.00
MINNESOTA BENEFIT ASSOCIATION	5/23/25	MN BENEFITS	GENERAL FUND	NON-DEPARTMENTAL	18.17
	5/23/25	MN BENEFITS	GENERAL FUND	NON-DEPARTMENTAL	14.06
	5/23/25	MN BENEFITS	GENERAL FUND	NON-DEPARTMENTAL	62.84
	5/23/25	MN BENEFITS	GENERAL FUND	NON-DEPARTMENTAL	58.03
	5/23/25	INSURANCE	GENERAL FUND	POLICE ADMINISTRATION	0.01
	5/23/25	INSURANCE	GENERAL FUND	PAVED STREETS	114.80
	5/23/25	INSURANCE	GENERAL FUND	PUBLIC WORK SHOP	3.63
	5/23/25	MN BENEFITS	RECREATION	NON-DEPARTMENTAL	33.25
	5/23/25	MN BENEFITS	RECREATION	NON-DEPARTMENTAL	37.36
	5/23/25	MN BENEFITS	RECREATION	NON-DEPARTMENTAL	8.87
	5/23/25	MN BENEFITS	RECREATION	NON-DEPARTMENTAL	13.68
	5/23/25	INSURANCE	RECREATION	SOCCER COMPLEX	0.01
	5/23/25	INSURANCE	RECREATION	PARK AREAS	0.01
	5/23/25	MN BENEFITS	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	96.92
	5/23/25	MN BENEFITS	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	96.92
	5/23/25	MN BENEFITS	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	9.21
	5/23/25	MN BENEFITS	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	9.21
	5/23/25	INSURANCE	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	0.01
	5/23/25	MN BENEFITS	ELECTRIC	NON-DEPARTMENTAL	13.92
	5/23/25	MN BENEFITS	ELECTRIC	NON-DEPARTMENTAL	13.92
	5/23/25	INSURANCE	STORM WATER MANAGE	STORM DRAINAGE	74.91
	5/23/25	MN BENEFITS	DATA PROCESSING	NON-DEPARTMENTAL	7.87
	5/23/25	MN BENEFITS	DATA PROCESSING	NON-DEPARTMENTAL	7.87
				TOTAL:	695.48
MINNESOTA CHILD SUPPORT PAYMENT CTR	5/23/25	SUPPORT ORDER	GENERAL FUND	NON-DEPARTMENTAL	146.28
				TOTAL:	146.28
MINNESOTA ENERGY RESOURCES CORP	5/23/25	MONTHLY SERVICE	WATER	O-PURIFY MISC	19.95
	5/23/25	MONTHLY SERVICE	WATER	O-DISTR MISC	87.84
	5/23/25	MONTHLY SERVICE	WATER	O-DISTR MISC	60.78
	5/23/25	MONTHLY SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	60.78
	5/23/25	MONTHLY SERVICE	ELECTRIC	O-DISTR MISC	87.53
	5/23/25	MONTHLY SERVICE	ELECTRIC	O-DISTR MISC	121.55
				TOTAL:	438.43
MINNESOTA POLLUTION CONTROL	5/23/25	GENERAL PERMIT	GENERAL FUND	ENGINEERING ADMIN	400.00
				TOTAL:	400.00
MINNESOTA VALLEY TESTING LABS INC	5/23/25	SEMI ANNUAL MERCURY	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	335.25
	5/23/25	TOTAL KJELDAHL NITROGEN	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	154.59
	5/23/25	TOTAL KJELDAHL NITROGEN	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	192.38
				TOTAL:	682.22
MISCELLANEOUS V ELEVA CARE	5/23/25	SHELTER CANCELLATION	RECREATION	NON-DEPARTMENTAL	85.00
HAYENGA, SARAH	5/23/25	QUALITY CENTRAL AIR INSTAL	ELECTRIC	CUSTOMER INSTALL EXPEN	375.00
FIRST LUTHERN CHURCH	5/23/25	CENTRAL AIR REBATE	ELECTRIC	CUSTOMER INSTALL EXPEN	350.00
MIRANDA, ANDRES	5/23/25	CENTRAL AIR REBATE	ELECTRIC	CUSTOMER INSTALL EXPEN	350.00

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
HOFFMAN, MARY ANN	5/23/25	CENTRAL AIR REBATE	ELECTRIC	CUSTOMER INSTALL EXPEN	350.00
INNER BEAUTY SALON LLC	5/23/25	AIR CONDITION REBATE	ELECTRIC	CUSTOMER INSTALL EXPEN	<u>420.00</u>
				TOTAL:	1,930.00
NCPERS GROUP LIFE INS	5/23/25	LIFE INS	GENERAL FUND	NON-DEPARTMENTAL	138.98
	5/23/25	LIFE INS	GENERAL FUND	NON-DEPARTMENTAL	143.40
	5/23/25	LIFE INS	MEMORIAL AUDITORIUM	NON-DEPARTMENTAL	8.00
	5/23/25	LIFE INS	MEMORIAL AUDITORIUM	NON-DEPARTMENTAL	8.00
	5/23/25	LIFE INS	RECREATION	NON-DEPARTMENTAL	35.95
	5/23/25	LIFE INS	RECREATION	NON-DEPARTMENTAL	33.05
	5/23/25	LIFE INS	IMPROVEMENT CONST	NON-DEPARTMENTAL	7.10
	5/23/25	LIFE INS	IMPROVEMENT CONST	NON-DEPARTMENTAL	7.55
	5/23/25	LIFE INS	WATER	NON-DEPARTMENTAL	38.58
	5/23/25	LIFE INS	WATER	NON-DEPARTMENTAL	38.57
	5/23/25	LIFE INS	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	44.98
	5/23/25	LIFE INS	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	45.18
	5/23/25	LIFE INS	ELECTRIC	NON-DEPARTMENTAL	43.72
	5/23/25	LIFE INS	ELECTRIC	NON-DEPARTMENTAL	43.57
	5/23/25	BYRNE-MAY FOR JUNE	ELECTRIC	NON-DEPARTMENTAL	16.00
	5/23/25	LIFE INS	STORM WATER MANAGE	NON-DEPARTMENTAL	2.69
	5/23/25	LIFE INS	STORM WATER MANAGE	NON-DEPARTMENTAL	0.68
	5/23/25	LIFE INS	LIQUOR	NON-DEPARTMENTAL	8.00
	5/23/25	LIFE INS	LIQUOR	NON-DEPARTMENTAL	8.00
	5/23/25	LIFE INS	DATA PROCESSING	NON-DEPARTMENTAL	16.00
	5/23/25	LIFE INS	DATA PROCESSING	NON-DEPARTMENTAL	<u>16.00</u>
				TOTAL:	704.00
NICOLE R KEMPEMA	5/23/25	CLEANING LAUREL CANYON	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	194.18
	5/23/25	CLEANING SHINING FAME	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	420.71
	5/23/25	CLEANING WHOSE	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	113.28
	5/23/25	WMS CHOIR CLEANING	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	194.18
	5/23/25	CHOIR CLEANING	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	302.05
	5/23/25	CLEANING MUYEPHEN	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	172.60
	5/23/25	CLEANING WMS MOVIE	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	302.05
	5/23/25	WMS CLEANING	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	<u>323.63</u>
				TOTAL:	2,022.68
NOBLES COOPERATIVE ELECTRIC	5/23/25	MONTHLY SERVICE	GENERAL FUND	SECURITY CENTER	21.89
	5/23/25	MONTHLY SERVICE	GENERAL FUND	SECURITY CENTER	21.88
	5/23/25	MONTHLY SERVICE	GENERAL FUND	SIGNS AND SIGNALS	46.53
	5/23/25	MONTHLY SERVICE	GENERAL FUND	SIGNS AND SIGNALS	50.43
	5/23/25	NOBLES COOPERATIVE ELECTRIC	RECREATION	ICE ARENA	493.13
	5/23/25	MONTHLY SERVICE	INDUSTRIAL WASTEWA	O-PURIFY MISC	<u>100.00</u>
				TOTAL:	733.86
NOBLES COUNTY ATTORNEY	5/23/25	2008 HONDA FEL FLEE IN MV	GENERAL FUND	NON-DEPARTMENTAL	<u>186.00</u>
				TOTAL:	186.00
NOBLES COUNTY AUDITOR/TREASURER	5/23/25	2024 YEAR END FINANCIAL RE	GENERAL FUND	AUDITS AND BUDGETS	300.00
	5/23/25	MAY LEGAL SERVICES	GENERAL FUND	PROSECUTION	<u>31,514.00</u>
				TOTAL:	31,814.00
NOTHING BUT HEMP	5/23/25	THC	LIQUOR	NON-DEPARTMENTAL	1,611.00
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	<u>45.00</u>
				TOTAL:	1,656.00

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
OFFICE OF MNIT SERVICES	5/23/25	VOICE SERVICES	GENERAL FUND	ADMINISTRATION	31.64
	5/23/25	VOICE SERVICES	GENERAL FUND	ADMINISTRATION	30.97
	5/23/25	VOICE SERVICES	GENERAL FUND	CLERK'S OFFICE	32.96
	5/23/25	VOICE SERVICES	GENERAL FUND	CLERK'S OFFICE	32.42
	5/23/25	VOICE SERVICES	GENERAL FUND	ACCOUNTING	31.49
	5/23/25	VOICE SERVICES	GENERAL FUND	ACCOUNTING	30.72
	5/23/25	VOICE SERVICES	GENERAL FUND	ENGINEERING ADMIN	22.37
	5/23/25	VOICE SERVICES	GENERAL FUND	ENGINEERING ADMIN	138.68
	5/23/25	VOICE SERVICES	GENERAL FUND	ENGINEERING ADMIN	23.11
	5/23/25	VOICE SERVICES	GENERAL FUND	ENGINEERING ADMIN	138.85
	5/23/25	VOICE SERVICES	GENERAL FUND	ECONOMIC DEVELOPMENT	22.37
	5/23/25	VOICE SERVICES	GENERAL FUND	ECONOMIC DEVELOPMENT	31.54
	5/23/25	VOICE SERVICES	GENERAL FUND	ECONOMIC DEVELOPMENT	23.11
	5/23/25	VOICE SERVICES	GENERAL FUND	ECONOMIC DEVELOPMENT	31.70
	5/23/25	VOICE SERVICES	RECREATION	FIELD HOUSE	46.51
	5/23/25	VOICE SERVICES	RECREATION	FIELD HOUSE	46.66
	5/23/25	APRIL VOICE SERVICES	WATER	O-PUMPING	16.13
	5/23/25	APRIL VOICE SERVICES	WATER	O-DISTR MISC	46.07
	5/23/25	APRIL VOICE SERVICES	WATER	ADMIN OFFICE SUPPLIES	14.88
	5/23/25	APRIL VOICE SERVICES	WATER	ACCTS-RECORDS & COLLEC	28.91
	5/23/25	APRIL VOICE SERVICES	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	14.88
	5/23/25	APRIL VOICE SERVICES	MUNICIPAL WASTEWAT	O-PURIFY MISC	32.21
	5/23/25	APRIL VOICE SERVICES	MUNICIPAL WASTEWAT	ADMIN OFFICE SUPPLIES	14.88
	5/23/25	APRIL VOICE SERVICES	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	32.21
	5/23/25	APRIL VOICE SERVICES	ELECTRIC	O-SOURCE MISC	14.88
	5/23/25	APRIL VOICE SERVICES	ELECTRIC	O-DISTR SUPER & ENG	14.88
	5/23/25	APRIL VOICE SERVICES	ELECTRIC	O-DISTR STATION EXPENS	44.85
	5/23/25	APRIL VOICE SERVICES	ELECTRIC	O-DISTR MISC	14.88
	5/23/25	APRIL VOICE SERVICES	ELECTRIC	ADMIN OFFICE SUPPLIES	15.49
	5/23/25	APRIL VOICE SERVICES	ELECTRIC	ACCTS-RECORDS & COLLEC	86.06
	5/23/25	APRIL VOICE SERVICES	ELECTRIC	ACCTS-ASSISTANCE	16.49
	5/23/25	VOICE DIRECTORY	DATA PROCESSING	DATA PROCESSING	53.72
	5/23/25	VOICE SERVICES	DATA PROCESSING	DATA PROCESSING	31.21
	5/23/25	VOICE SERVICES	DATA PROCESSING	DATA PROCESSING	31.23
	5/23/25	VOICE DIRECTORY	DATA PROCESSING	DATA PROCESSING	<u>53.72</u>
	TOTAL:				1,292.68
ON DECK SPORTS	5/23/25	NEW BASEBALLS	RECREATION	FIELD HOUSE	<u>584.98</u>
	TOTAL:				584.98
ONE OFFICE SOLUTION-WOCITY	5/23/25	PAPER	GENERAL FUND	CLERK'S OFFICE	<u>40.94</u>
	TOTAL:				40.94
ONE OFFICE SOLUTION-NCLAW	5/23/25	NOTES AND TAPE	GENERAL FUND	SECURITY CENTER	13.98
	5/23/25	NOTES AND TAPE	GENERAL FUND	SECURITY CENTER	13.99
	5/23/25	PENS	GENERAL FUND	SECURITY CENTER	9.90
	5/23/25	PENS	GENERAL FUND	SECURITY CENTER	<u>9.90</u>
	TOTAL:				47.77
ONE OFFICE SOLUTION-WOCITY	5/23/25	BATHROOM TOWELS	GENERAL FUND	FIRE ADMINISTRATION	57.59
	5/23/25	PAPERS	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	184.80
	5/23/25	TOWELS	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	165.68
	5/23/25	TAPE AND DIVIDERS	RECREATION	FIELD HOUSE	<u>56.00</u>
	TOTAL:				464.07
ONE OFFICE SOLUTION-WOUTIL	5/23/25	MISC SUPPLIES	WATER	ACCTS-RECORDS & COLLEC	4.60

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
	5/23/25	COPIER SERVICE AGREEMENT	WATER	ACCTS-RECORDS & COLLEC	88.91
	5/23/25	MISC SUPPLIES	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	4.60
	5/23/25	COPIER SERVICE AGREEMENT	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	88.90
	5/23/25	MISC SUPPLIES	ELECTRIC	ACCTS-RECORDS & COLLEC	9.21
	5/23/25	COPIER SERVICE AGREEMENT	ELECTRIC	ACCTS-RECORDS & COLLEC	<u>177.81</u>
				TOTAL:	374.03
PALMA CUSTOMS & AUTO SALES LLC	5/23/25	25-1781 DURANGO TOW	GENERAL FUND	POLICE ADMINISTRATION	68.00
	5/23/25	WGTN FOOTBALL VAN RENTAL	RECREATION	RECREATION PROGRAMS	251.38
	5/23/25	WGTN FOOTBALL VAN RENTAL	RECREATION	RECREATION PROGRAMS	<u>497.46</u>
				TOTAL:	816.84
PEIL BRANDON	5/23/25	PARTIAL TUTION REIMBURSEME	GENERAL FUND	POLICE ADMINISTRATION	<u>500.00</u>
				TOTAL:	500.00
MN PEIP	5/23/25	HEALTH INS PREMIUM	GENERAL FUND	NON-DEPARTMENTAL	5,469.60
	5/23/25	HEALTH INS PREMIUM	GENERAL FUND	NON-DEPARTMENTAL	5,500.96
	5/23/25	HEALTH INS-MAY FOR JUNE	GENERAL FUND	NON-DEPARTMENTAL	2,188.28
	5/23/25	BOMGAARS INS-MAY FOR JUNE	GENERAL FUND	NON-DEPARTMENTAL	510.04
	5/23/25	HEALTH INS PREM	GENERAL FUND	MAYOR AND COUNCIL	460.54
	5/23/25	HEALTH INS PREM	GENERAL FUND	MAYOR AND COUNCIL	460.54
	5/23/25	HEALTH INS PREM	GENERAL FUND	ADMINISTRATION	1,786.74
	5/23/25	HEALTH INS PREM	GENERAL FUND	ADMINISTRATION	1,786.74
	5/23/25	HEALTH INS PREM	GENERAL FUND	CLERK'S OFFICE	1,031.59
	5/23/25	HEALTH INS PREM	GENERAL FUND	CLERK'S OFFICE	1,022.96
	5/23/25	HEALTH INS PREM	GENERAL FUND	ACCOUNTING	1,072.59
	5/23/25	HEALTH INS PREM	GENERAL FUND	ACCOUNTING	1,069.71
	5/23/25	HEALTH INS PREM	GENERAL FUND	ENGINEERING ADMIN	1,158.24
	5/23/25	HEALTH INS PREM	GENERAL FUND	ENGINEERING ADMIN	1,215.81
	5/23/25	HEALTH INS PREM	GENERAL FUND	ECONOMIC DEVELOPMENT	2,041.82
	5/23/25	HEALTH INS PREM	GENERAL FUND	ECONOMIC DEVELOPMENT	1,020.08
	5/23/25	HEALTH INS PREM	GENERAL FUND	ECONOMIC DEVELOPMENT	12.75
	5/23/25	HEALTH INS PREM	GENERAL FUND	ECONOMIC DEVELOPMENT	2,054.57
	5/23/25	HEALTH INS PREM	GENERAL FUND	ECONOMIC DEVELOPMENT	1,020.08
	5/23/25	HEALTH INS PREM	GENERAL FUND	POLICE ADMINISTRATION	15,873.89
	5/23/25	HEALTH INS PREM	GENERAL FUND	POLICE ADMINISTRATION	366.63
	5/23/25	HEALTH INS PREM	GENERAL FUND	POLICE ADMINISTRATION	16,086.20
	5/23/25	HEALTH INS PREM	GENERAL FUND	POLICE ADMINISTRATION	110.82
	5/23/25	HEALTH INS PREM	GENERAL FUND	POLICE ADMINISTRATION	43.50
	5/23/25	BOMGAARS INS-MAY FOR JUNE	GENERAL FUND	POLICE ADMINISTRATION	2,040.16
	5/23/25	HEALTH INS PREM	GENERAL FUND	SECURITY CENTER	2,734.17
	5/23/25	HEALTH INS PREM	GENERAL FUND	SECURITY CENTER	2,734.17
	5/23/25	HEALTH INS PREM	GENERAL FUND	SECURITY CENTER	2,734.17
	5/23/25	HEALTH INS PREM	GENERAL FUND	SECURITY CENTER	2,734.17
	5/23/25	HEALTH INS PREM	GENERAL FUND	ANIMAL CONTROL ENFORCE	255.02
	5/23/25	HEALTH INS PREM	GENERAL FUND	ANIMAL CONTROL ENFORCE	255.02
	5/23/25	HEALTH INS PREM	GENERAL FUND	PAVED STREETS	688.55
	5/23/25	HEALTH INS PREM	GENERAL FUND	PAVED STREETS	510.04
	5/23/25	HEALTH INS PREM	GENERAL FUND	PUBLIC WORK SHOP	51.00
	5/23/25	HEALTH INS PREM	GENERAL FUND	TRASH PICKUP	510.04
	5/23/25	HEALTH INS PREM	GENERAL FUND	CODE ENFORCEMENT	1,785.14
	5/23/25	HEALTH INS PREM	GENERAL FUND	CODE ENFORCEMENT	1,785.14
	5/23/25	HEALTH INS PREM	GENERAL FUND	CENTER FOR ACTIVE LIVI	1,020.08
	5/23/25	HEALTH INS PREM	GENERAL FUND	CENTER FOR ACTIVE LIVI	1,020.08
	5/23/25	HEALTH INS PREM	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	460.54
	5/23/25	HEALTH INS PREM	MEMORIAL AUDITORIUM	MEMORIAL AUDITORIUM	460.54

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
	5/23/25	HEALTH INS PREMIUM	RECREATION	NON-DEPARTMENTAL	446.69
	5/23/25	HEALTH INS PREMIUM	RECREATION	NON-DEPARTMENTAL	446.69
	5/23/25	HEALTH INS PREM	RECREATION	FIELD HOUSE	1,480.62
	5/23/25	HEALTH INS PREM	RECREATION	FIELD HOUSE	1,480.62
	5/23/25	HEALTH INS PREM	RECREATION	PARK AREAS	594.16
	5/23/25	HEALTH INS PREM	RECREATION	PARK AREAS	766.66
	5/23/25	HEALTH INS PREM	RECREATION	TREE REMOVAL	172.50
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	SP ASSESS-ADMIN ESCROW	51.81
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	SP ASSESS-ADMIN ESCROW	80.59
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	MURRAY AVE-NOBLES-E DE	46.05
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	MURRAY AVE-NOBLES-E DE	23.03
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	TRAIL-BEACH NOOK TO CY	46.05
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	TRAIL-BEACH NOOK TO CY	11.51
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	RAY DRIVE N EXTENSION	276.32
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	RAY DRIVE N EXTENSION	184.22
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	OVERLAY PROGRAM	46.07
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	OVERLAY PROGRAM	23.03
	5/23/25	HEALTH INS PREM	IMPROVEMENT CONST	E 9TH AVE SWR EXT-10-1	46.05
	5/23/25	HEALTH INS PREMIUM	WATER	NON-DEPARTMENTAL	353.32
	5/23/25	HEALTH INS PREMIUM	WATER	NON-DEPARTMENTAL	350.77
	5/23/25	HEALTH INS PREM	WATER	O-DISTR SUPER AND ENG	1,020.08
	5/23/25	HEALTH INS PREM	WATER	O-DISTR SUPER AND ENG	1,020.08
	5/23/25	HEALTH INS PREM	WATER	O-DISTR MISC	23.03
	5/23/25	HEALTH INS PREM	WATER	O-DISTR MISC	23.03
	5/23/25	HEALTH INS PREM	WATER	GENERAL ADMIN	299.22
	5/23/25	HEALTH INS PREM	WATER	GENERAL ADMIN	293.68
	5/23/25	HEALTH INS PREM	WATER	ADMIN OFFICE SUPPLIES	9.24
	5/23/25	HEALTH INS PREM	WATER	ACCTS-METER READING	163.21
	5/23/25	HEALTH INS PREM	WATER	ACCTS-METER READING	153.01
	5/23/25	HEALTH INS PREM	WATER	ACCTS-RECORDS & COLLEC	143.78
	5/23/25	HEALTH INS PREM	WATER	ACCTS-RECORDS & COLLEC	143.78
	5/23/25	HEALTH INS PREM	WATER	PROJECT #11	34.54
	5/23/25	HEALTH INS PREMIUM	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	301.02
	5/23/25	HEALTH INS PREMIUM	MUNICIPAL WASTEWAT	NON-DEPARTMENTAL	301.02
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	O-SOURCE SUPERVISION	306.02
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	O-SOURCE SUPERVISION	306.02
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	O-PURIFY SUPERVISION	714.06
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	O-PURIFY SUPERVISION	714.06
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	O-PURIFY MISC	23.03
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	O-PURIFY MISC	23.03
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	GENERAL ADMIN	262.40
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	GENERAL ADMIN	257.97
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	ADMIN OFFICE SUPPLIES	9.24
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	120.78
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	ACCT-RECORDS & COLLECT	120.78
	5/23/25	HEALTH INS PREM	MUNICIPAL WASTEWAT	PROJECT #7	34.54
	5/23/25	HEALTH INS PREMIUM	ELECTRIC	NON-DEPARTMENTAL	1,450.79
	5/23/25	HEALTH INS PREMIUM	ELECTRIC	NON-DEPARTMENTAL	1,453.34
	5/23/25	HEALTH INS-MAY FOR JUNE	ELECTRIC	NON-DEPARTMENTAL	510.04
	5/23/25	HEALTH INS PREM	ELECTRIC	O-SOURCE SUPER & ENG	51.00
	5/23/25	HEALTH INS PREM	ELECTRIC	O-SOURCE SUPER & ENG	51.00
	5/23/25	HEALTH INS PREM	ELECTRIC	O-SOURCE GENERATION	72.28
	5/23/25	HEALTH INS PREM	ELECTRIC	O-DISTR SUPER & ENG	918.07
	5/23/25	HEALTH INS PREM	ELECTRIC	O-DISTR SUPER & ENG	918.07
	5/23/25	HEALTH INS PREM	ELECTRIC	O-DISTR UNDERGRND LINE	570.65
	5/23/25	HEALTH INS PREM	ELECTRIC	O-DISTR UNDERGRND LINE	466.78

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
	5/23/25	HEALTH INS PREM	ELECTRIC	O-DISTR MISC	698.44
	5/23/25	HEALTH INS PREM	ELECTRIC	O-DISTR MISC	784.54
	5/23/25	HEALTH INS PREM	ELECTRIC	M-SOURCE SUPER & ENF	51.01
	5/23/25	HEALTH INS PREM	ELECTRIC	M-SOURCE SUPER & ENF	51.01
	5/23/25	HEALTH INS PREM	ELECTRIC	M-CISTR SUPER & ENG	28.05
	5/23/25	HEALTH INS PREM	ELECTRIC	M-CISTR SUPER & ENG	57.50
	5/23/25	HEALTH INS PREM	ELECTRIC	M-DISTR UNDERGRND LINE	1,017.12
	5/23/25	HEALTH INS PREM	ELECTRIC	M-DISTR UNDERGRND LINE	817.90
	5/23/25	HEALTH INS PREM	ELECTRIC	M-DISTR LINE TRANSFRMR	295.02
	5/23/25	HEALTH INS PREM	ELECTRIC	M-DISTR ST LITE & SIG	386.91
	5/23/25	HEALTH INS PREM	ELECTRIC	M-DISTR ST LITE & SIG	412.19
	5/23/25	HEALTH INS PREM	ELECTRIC	M-DISTR PLANT MISC	56.10
	5/23/25	HEALTH INS PREM	ELECTRIC	M-DISTR PLANT MISC	42.55
	5/23/25	HEALTH INS PREM	ELECTRIC	GENERAL ADMIN	1,126.12
	5/23/25	HEALTH INS PREM	ELECTRIC	GENERAL ADMIN	1,099.12
	5/23/25	HEALTH INS PREM	ELECTRIC	ADMIN OFFICE SUPPLIES	18.49
	5/23/25	HEALTH INS PREM	ELECTRIC	ACCTS-METER READING	163.21
	5/23/25	HEALTH INS PREM	ELECTRIC	ACCTS-METER READING	191.27
	5/23/25	HEALTH INS PREM	ELECTRIC	ACCTS-RECORDS & COLLEC	605.71
	5/23/25	HEALTH INS PREM	ELECTRIC	ACCTS-RECORDS & COLLEC	605.71
	5/23/25	HEALTH INS PREM	ELECTRIC	ACCTS-ASSISTANCE	230.28
	5/23/25	HEALTH INS PREM	ELECTRIC	ACCTS-ASSISTANCE	230.28
	5/23/25	HEALTH INS PREM	ELECTRIC	FA DISTR UNDRGRND COND	1,252.60
	5/23/25	HEALTH INS PREM	ELECTRIC	FA DISTR UNDRGRND COND	2,009.40
	5/23/25	HEALTH INS PREM	ELECTRIC	FA DISTR UNDRGRND COND	942.31
	5/23/25	HEALTH INS PREM	ELECTRIC	FA DISTR ST LITE & SIG	195.18
	5/23/25	HEALTH INS PREMIUM	LIQUOR	NON-DEPARTMENTAL	255.02
	5/23/25	HEALTH INS PREMIUM	LIQUOR	NON-DEPARTMENTAL	255.02
	5/23/25	HEALTH INS PREM	LIQUOR	O-GEN MISC	1,935.65
	5/23/25	HEALTH INS PREM	LIQUOR	O-GEN MISC	5.51
	5/23/25	HEALTH INS PREM	LIQUOR	O-GEN MISC	1,941.16
	5/23/25	HEALTH INS PREMIUM	AIRPORT	NON-DEPARTMENTAL	31.36
	5/23/25	HEALTH INS PREM	AIRPORT	O-GEN MISC	280.53
	5/23/25	HEALTH INS PREMIUM	DATA PROCESSING	NON-DEPARTMENTAL	357.03
	5/23/25	HEALTH INS PREMIUM	DATA PROCESSING	NON-DEPARTMENTAL	357.03
	5/23/25	HEALTH INS PREM	DATA PROCESSING	DATA PROCESSING	2,285.87
	5/23/25	HEALTH INS PREM	DATA PROCESSING	DATA PROCESSING	<u>2,268.60</u>
				TOTAL:	128,692.94
PELLEGRINO FIRE EXTINGUISHERS	5/23/25	FIRE EXTINGUISHERS	GENERAL FUND	PAVED STREETS	777.00
	5/23/25	FIRE EXTINGUISHERS	GENERAL FUND	CENTER FOR ACTIVE LIVI	82.50
	5/23/25	FIRE EXTINGUISHERS	RECREATION	PARK AREAS	<u>422.00</u>
				TOTAL:	1,281.50
PEPSI COLA BOTTLING CO OF PIPESTONE, M	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	<u>47.00</u>
				TOTAL:	47.00
PHILLIPS WINE & SPIRITS INC	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	4,815.54
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	4,188.93
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	1,287.51
	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	34.00
	5/23/25	THC	LIQUOR	NON-DEPARTMENTAL	979.00
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	3,186.50
	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	203.65
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	1,413.05
	5/23/25	LIQUOR	LIQUOR	O-SOURCE MISC	100.98

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	41.50
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	54.00
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	80.16
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	<u>62.17</u>
				TOTAL:	16,446.99
QUARNSTROM & DOERING P A	5/23/25	SPECIAL ASSESSMENT ANALYSI	GENERAL FUND	ENGINEERING ADMIN	<u>937.50</u>
				TOTAL:	937.50
RADIO WORKS LLC	5/23/25	APRIL ADS	GENERAL FUND	CENTER FOR ACTIVE LIVI	<u>200.00</u>
				TOTAL:	200.00
RICKERS STUDIO	5/23/25	POLICE ID PICTURE	GENERAL FUND	POLICE ADMINISTRATION	<u>92.99</u>
				TOTAL:	92.99
RILEY MARK	5/23/25	K9 NARC TRIALS	GENERAL FUND	POLICE ADMINISTRATION	<u>307.74</u>
				TOTAL:	307.74
RMB ENVIRONMENTAL LABORATORIES INC	5/23/25	ACUTE TOXICITY EVALUATION	MUNICIPAL WASTEWAT	O-PURIFY LABORATORY	<u>1,075.00</u>
				TOTAL:	1,075.00
ROOS ERIC	5/23/25	ANNUAL MEETING	ELECTRIC	ADMIN OFFICE SUPPLIES	<u>182.00</u>
				TOTAL:	182.00
ROUND LAKE VINEYARDS & WINERY LLC	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	<u>375.00</u>
				TOTAL:	375.00
RUNNINGS SUPPLY INC-ACCT#9502440	5/23/25	SMALL TOOLS #209	WATER	O-DIST UNDERGRND LINES	95.97
	5/23/25	SMALL TOOLS #206	WATER	O-DIST UNDERGRND LINES	28.99
	5/23/25	SHOP SUPPLIES	MUNICIPAL WASTEWAT	M-PURIFY EQUIPMENT	107.94
	5/23/25	PLUMBING PARTS	MUNICIPAL WASTEWAT	M-PURIFY EQUIPMENT	29.55
	5/23/25	SQUEEGEE	MUNICIPAL WASTEWAT	M-PURIFY EQUIPMENT	20.98
	5/23/25	MARKING PAINT	MUNICIPAL WASTEWAT	M-PURIFY EQUIPMENT	5.99
	5/23/25	NUTS AND BOLTS	MUNICIPAL WASTEWAT	M-PURIFY EQUIPMENT	<u>3.12</u>
				TOTAL:	292.54
RUNNINGS SUPPLY INC-ACCT#9502485	5/23/25	SQUAD #08-51 BATTERY CHARG	GENERAL FUND	POLICE ADMINISTRATION	44.99
	5/23/25	EXTENSION CORD	GENERAL FUND	POLICE ADMINISTRATION	39.99
	5/23/25	HAMMER FOR TRUCK	GENERAL FUND	FIRE ADMINISTRATION	31.99
	5/23/25	GLOVES	GENERAL FUND	PAVED STREETS	68.97
	5/23/25	BOOTS	GENERAL FUND	PAVED STREETS	175.95
	5/23/25	FORKS	RECREATION	BALLFIELD MAINTENANCE	114.98
	5/23/25	GLOVES	RECREATION	SOCCER COMPLEX	16.99
	5/23/25	TOTES	RECREATION	SOCCER COMPLEX	19.48
	5/23/25	GREASE	RECREATION	AQUATIC CENTER FACILIT	15.98
	5/23/25	STEEL GRINDING WHEEL	RECREATION	PARK AREAS	22.16
	5/23/25	BEACH RESTROOM	RECREATION	PARK AREAS	7.29
	5/23/25	MILLARD RESTROOM	RECREATION	PARK AREAS	67.99
	5/23/25	GLOVES	RECREATION	PARK AREAS	7.00
	5/23/25	PLAYGROUND BOLTS	RECREATION	PARK AREAS	11.50
	5/23/25	GRASS SEEDER	RECREATION	PARK AREAS	21.99
	5/23/25	GLOVES	RECREATION	PARK AREAS	14.99
	5/23/25	MULCH CHATUQUA	RECREATION	PARK AREAS	11.97
	5/23/25	OLSON ICE CHEST	RECREATION	OLSON PARK CAMPGROUND	<u>7.99</u>
				TOTAL:	702.20

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
SANFORD HEALTH-OCC MED	5/23/25	PRE-EMPLOYMENT TESTING EXA	GENERAL FUND	FIRE ADMINISTRATION	<u>770.00</u>
				TOTAL:	770.00
SCHAAP SANITATION	5/23/25	MONTHLY SERVICE	GENERAL FUND	CENTER FOR ACTIVE LIVI	43.09
	5/23/25	MONTHLY SERVICE	RECREATION	10TH STREET PAVILION	182.16
	5/23/25	MONTHLY SERVICE	MUNICIPAL WASTEWAT	O-PURIFY MISC	355.53
	5/23/25	MONTHLY SERVICE	LIQUOR	O-GEN MISC	<u>279.44</u>
				TOTAL:	860.22
TODD SCHROEDER	5/23/25	MOWING	GENERAL FUND	GENERAL GOVT BUILDINGS	80.00
	5/23/25	MOWING AND CLEANING	GENERAL FUND	GENERAL GOVT BUILDINGS	534.38
	5/23/25	MOWING	LIQUOR	O-GEN MISC	<u>534.53</u>
				TOTAL:	1,148.91
SCHWALBACH ACE 5930	5/23/25	WAND	GENERAL FUND	CENTER FOR ACTIVE LIVI	31.99
	5/23/25	SCRUB BRUSH	RECREATION	BALLFIELD MAINTENANCE	17.98
	5/23/25	FUSE	RECREATION	BALLFIELD MAINTENANCE	6.99
	5/23/25	TANK LEVEL	RECREATION	BALLFIELD MAINTENANCE	14.99
	5/23/25	SCREWS	RECREATION	ICE ARENA	36.99
	5/23/25	SILICONE LOCKS	RECREATION	AQUATIC CENTER FACILIT	29.76
	5/23/25	TIE DOWN, GLOVES, BITS	RECREATION	AQUATIC CENTER FACILIT	102.54
	5/23/25	TOLIET REPAIR	RECREATION	AQUATIC CENTER FACILIT	159.96
	5/23/25	WRENCH	RECREATION	AQUATIC CENTER FACILIT	20.15
	5/23/25	ROLLER COVERS	RECREATION	PARK AREAS	9.89
	5/23/25	VALVES	RECREATION	PARK AREAS	14.99
	5/23/25	OLSON PARK RESTROOM	RECREATION	OLSON PARK CAMPGROUND	9.99
	5/23/25	OLSON TOLIET	RECREATION	OLSON PARK CAMPGROUND	<u>13.58</u>
				TOTAL:	469.80
SCHWALBACH ACE #6067	5/23/25	FLAT IRON FOR MAKING LID L	MUNICIPAL WASTEWAT	M-SOURCE MAINS & LIFTS	<u>9.99</u>
				TOTAL:	9.99
SCOTT, BEAU	5/23/25	CDL CLASS AND TEST	WATER	O-DISTR MISC	<u>42.32</u>
				TOTAL:	42.32
SEW UNIQUE INC	5/23/25	T SHIRT MRWA POSTER	WATER	ACCTS-SERV & INFORMATI	<u>21.75</u>
				TOTAL:	21.75
SOUTHERN GLAZER'S WINE AND SPIRITS LL	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	64.00
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	1,399.62
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	2,916.72
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	91.54
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	7,554.82
	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	27.00
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	1,256.80
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	718.04
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	104.46
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	3.70
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	0.62
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	41.93
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	12.02
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	45.29
	5/23/25	FREIGH	LIQUOR	O-SOURCE MISC	0.31
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	120.37
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	1.85
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	46.56

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	3.55
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	<u>2.16</u>
			TOTAL:		14,411.36
SOUTHWESTERN MENTAL HEALTH CENTER INC	5/23/25	EAP SESSION	SAFETY PROMO/LOSS	HEALTH/SAFETY/FITNESS	<u>65.00</u>
			TOTAL:		65.00
STREICHER'S INC	5/23/25	40 MM SLINGS	GENERAL FUND	POLICE ADMINISTRATION	<u>177.95</u>
			TOTAL:		177.95
US POSTAL SERVICE	5/23/25	POSTAGE REFILL	ELECTRIC	ADMIN OFFICE SUPPLIES	<u>44.10</u>
			TOTAL:		44.10
USA BLUE BOOK	5/23/25	5 ML PIPET	WATER	O-PURIFY MISC	<u>44.87</u>
			TOTAL:		44.87
VERIZON WIRELESS	5/23/25	MONTHLY WIRELESS SERVICE	WATER	O-DISTR MISC	40.01
	5/23/25	MONTHLY WIRELESS SERVICE	WATER	O-DISTR MISC	41.39
	5/23/25	MONTHLY WIRELESS SERVICE	WATER	O-DISTR MISC	41.39
	5/23/25	MONTHLY WIRELESS SERVICE	WATER	ADMIN OFFICE SUPPLIES	11.60
	5/23/25	MONTHLY WIRELESS SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	40.01
	5/23/25	MONTHLY WIRELESS SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	41.39
	5/23/25	MONTHLY WIRELESS SERVICE	MUNICIPAL WASTEWAT	O-SOURCE MAINS & LIFTS	41.39
	5/23/25	MONTHLY WIRELESS SERVICE	MUNICIPAL WASTEWAT	O-PURIFY SUPERVISION	41.39
	5/23/25	MONTHLY WIRELESS SERVICE	MUNICIPAL WASTEWAT	ADMIN OFFICE SUPPLIES	11.60
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	O-DISTR SUPER & ENG	40.01
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	O-DISTR SUPER & ENG	40.01
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	O-DISTR SUPER & ENG	40.01
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	O-DISTR SUPER & ENG	41.39
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	O-DISTR SUPER & ENG	41.39
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	O-DISTR MISC	41.39
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	O-DISTR MISC	40.01
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	O-DISTR MISC	46.39
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	ADMIN OFFICE SUPPLIES	23.19
	5/23/25	MONTHLY WIRELESS SERVICE	ELECTRIC	ACCTS-RECORDS & COLLEC	<u>46.39</u>
			TOTAL:		710.35
VETERINARY MEDICAL CTR PA	5/23/25	SKOL EXAM	GENERAL FUND	POLICE ADMINISTRATION	<u>559.20</u>
			TOTAL:		559.20
VINOPIA INC	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	203.00
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	300.00
	5/23/25	LIQUOR	LIQUOR	NON-DEPARTMENTAL	679.25
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	2,459.50
	5/23/25	MIX	LIQUOR	NON-DEPARTMENTAL	240.00
	5/23/25	FREGHT	LIQUOR	O-SOURCE MISC	2.50
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	<u>50.00</u>
			TOTAL:		3,934.25
WELL TECH SOLUTIONS	5/23/25	SPRAY	RECREATION	PARK AREAS	<u>104.00</u>
			TOTAL:		104.00
WESTMOR INDUSTRIES LLC	5/23/25	FUEL SYSTEMS	AIRPORT	O-GEN MISC	5,179.90
	5/23/25	MONITORS	AIRPORT	O-GEN MISC	<u>144.87</u>
			TOTAL:		5,324.77

VENDOR SORT KEY	DATE	DESCRIPTION	FUND	DEPARTMENT	AMOUNT
WINE MERCHANTS	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	3,184.50
	5/23/25	WINE	LIQUOR	NON-DEPARTMENTAL	1,943.00
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	96.00
	5/23/25	FREIGHT	LIQUOR	O-SOURCE MISC	<u>34.00</u>
				TOTAL:	5,257.50
WOODS, FULLER, SHULTZ & SMITH P.C.	5/23/25	GENERAL BUSINESS	GENERAL FUND	ENGINEERING ADMIN	4,462.50
	5/23/25	ECONOMIC DEVELOP	GENERAL FUND	ECONOMIC DEVELOPMENT	<u>714.00</u>
				TOTAL:	5,176.50
WORTHINGTON AREA UNITED WAY	5/23/25	PAYROLL WITHHOLDING	GENERAL FUND	NON-DEPARTMENTAL	6.00
	5/23/25	PAYROLL WITHHOLDING	GENERAL FUND	NON-DEPARTMENTAL	6.00
	5/23/25	PAYROLL WITHHOLDING	MEMORIAL AUDITORIUM	NON-DEPARTMENTAL	10.00
	5/23/25	PAYROLL WITHHOLDING	MEMORIAL AUDITORIUM	NON-DEPARTMENTAL	10.00
	5/23/25	PAYROLL WITHHOLDING	DATA PROCESSING	NON-DEPARTMENTAL	19.00
	5/23/25	PAYROLL WITHHOLDING	DATA PROCESSING	NON-DEPARTMENTAL	<u>19.00</u>
				TOTAL:	70.00
WORTHINGTON ELECTRIC INC	5/23/25	CHAUTAUQUA SHELTER	RECREATION	PARK AREAS	43.97
	5/23/25	CHAUTAUQUA SHELTER	RECREATION	PARK AREAS	80.00
	5/23/25	SERVICE CALL	AIRPORT	O-GEN MISC	<u>80.00</u>
				TOTAL:	203.97

===== FUND TOTALS =====

101	GENERAL FUND	153,531.53
202	MEMORIAL AUDITORIUM	11,756.62
229	RECREATION	32,703.37
401	IMPROVEMENT CONST	926.35
431	AQUATIC CENTER FACILITY	15,683.93
601	WATER	57,256.65
602	MUNICIPAL WASTEWATER	8,189.89
604	ELECTRIC	108,949.40
605	INDUSTRIAL WASTEWATER	100.00
606	STORM WATER MANAGEMENT	1,894.36
609	LIQUOR	202,108.08
612	AIRPORT	6,636.66
702	DATA PROCESSING	6,210.88
703	SAFETY PROMO/LOSS CTRL	65.00
872	CABLE TELEVISION	2,817.29

GRAND TOTAL: 608,830.01
